



## Audit and certification for sustainable fuel and low-carbon product pathways in Australia

Local delivery partner working on behalf of TUV NORD

Audit

Verification

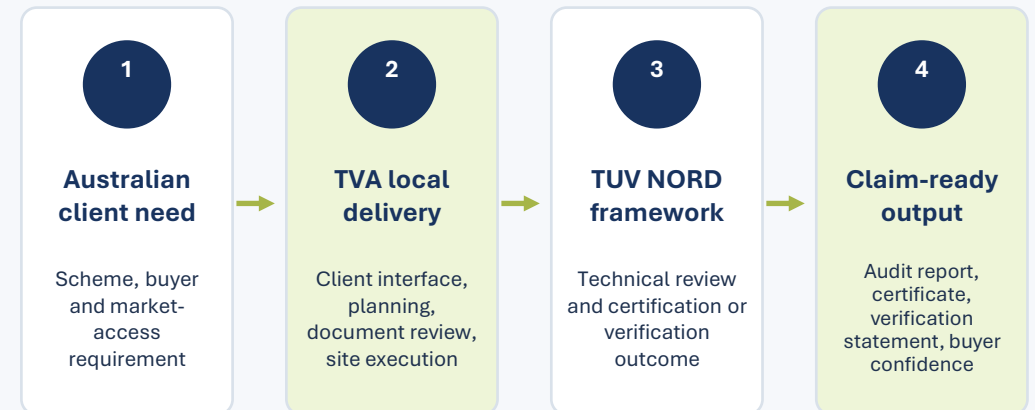
Certification

Pathways in scope: biodiesel, bioethanol, SAF, RFNBO (hydrogen, ammonia, methanol), biogas to bio-CNG / bio-LNG

### Positioning for this deck

For companies, TVA is positioned first as an audit, verification and certification delivery partner under the TUV NORD framework. Consulting is reserved for public organisations.

### Where TVA sits in the client journey



**Audience: Australian companies and Australian public organisations**

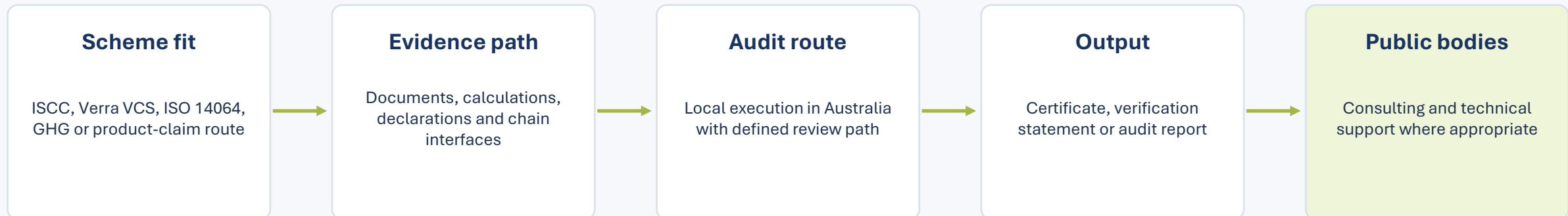
# TVA + TUV NORD operating model



Clear separation between company offer and public-sector consulting



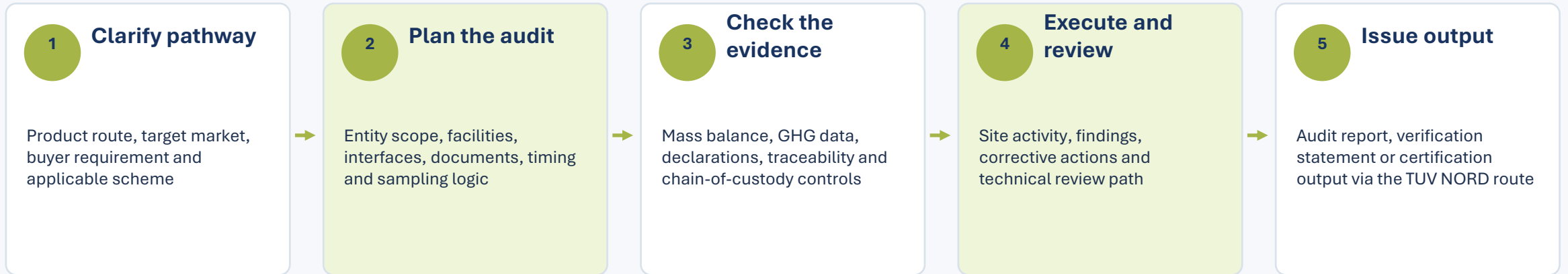
## What this means commercially



# Company pathway: from scope to certificate or verification output



Flow-chart view for private-sector clients



## Typical services for companies



Key message: this is not sold as a broad private-sector consulting offer. The commercial path for companies is structured audit, verification and certification delivery.

# Which supply chains are most affected?

Practical view of product pathways, interface points and sectors



| Pathway                                | Typical supply-chain pattern   | Where evidence usually matters  | Main sectors  |
|--|--|---|---|
| <b>Biodiesel</b>                       | Feedstock collection -> pre-treatment -> refining / esterification -> blending / distribution              | Feedstock origin, no-deforestation logic, GHG savings, blend and dispatch records       | Road transport<br>Mining<br>Agriculture             |
| <b>Bioethanol</b>                      | Crop or waste feedstock -> fermentation / distillation -> storage -> blending / export                     | Feedstock proof, process yields, transport records and product-carbon data              | Road transport<br>Chemicals<br>Exporters            |
| <b>SAF</b>                             | Waste, biogenic, RFNBO or alcohol-to-jet route -> upgrading -> fuel logistics -> airport supply            | Approved sustainability route, batch integrity, chain-of-custody, airport handover      | Aviation<br>Airports<br>Airlines                    |
| <b>Biogas to bio-CNG / bio-LNG</b>     | Organic waste / manure / landfill gas -> digestion -> upgrading -> compression or liquefaction -> delivery | Waste origin, upgrading logic, dispatch evidence and use-phase claims                   | Heavy fleets<br>Industrial users<br>Maritime pilots |
| <b>RFNBO (H2 / ammonia / methanol)</b> | Renewable electricity -> electrolysis / synthesis -> storage / shipping -> industrial or transport use     | Electricity linkage, emissions intensity, synthesis balance, delivery and export claims | Industry<br>Maritime<br>Aviation e-fuels<br>Export  |

**Best fit where the product story must stay consistent from origin to final claim across production, storage, transfer, blending, export and end use.**

# Industries and stakeholders in scope

Where the need for auditable product claims becomes operational



## Road and heavy transport

Fuel claims, blending chains, depot interfaces and low-carbon fuel procurement.

## Aviation

SAF pathway readiness, airport chain integrity and airline-facing sustainability evidence.

## Maritime and ports

Ammonia, methanol, bio-LNG and port-based storage, transfer and bunkering interfaces.

## Industrial energy and chemicals

Hydrogen, ammonia, biomethane, process fuels and downstream industrial offtake claims.

## Exporters and traders

Buyer templates, certification needs, cross-border documentation and claim consistency.

## Public organisations

Programme design, scheme review, consultation support and market-readiness input.

## Where TVA usually adds the most value

- Projects moving from concept into audited execution
- Organisations with multiple interfaces, facilities or logistics handovers
- Buyers or programme owners asking for traceable product claims
- Teams that prefer a practical audit route rather than a theory-heavy memo

## Typical touchpoints

- Producers
- Refineries
- Warehouses
- Transporters
- Blenders
- Airports
- Ports
- Exporters
- Industrial users

# Public organisations: consulting and technical support only



Separate track from the private-sector audit and certification offer

**Public bodies can be supported with practical, technical and market-facing input. This is the consulting part of the TVA offer.**

## Scheme and method input

Translate market practice into programme rules, data requirements and workable process logic.

## Independent technical review

Review pilots, evidence models, consultation drafts or pathway proposals before launch.

## Training and stakeholder engagement

Support workshops, webinars, briefings and targeted market-readiness sessions.

## Programme and pilot support

Useful for departments, regulators, grant administrators, trade bodies and working groups.

## Typical use cases

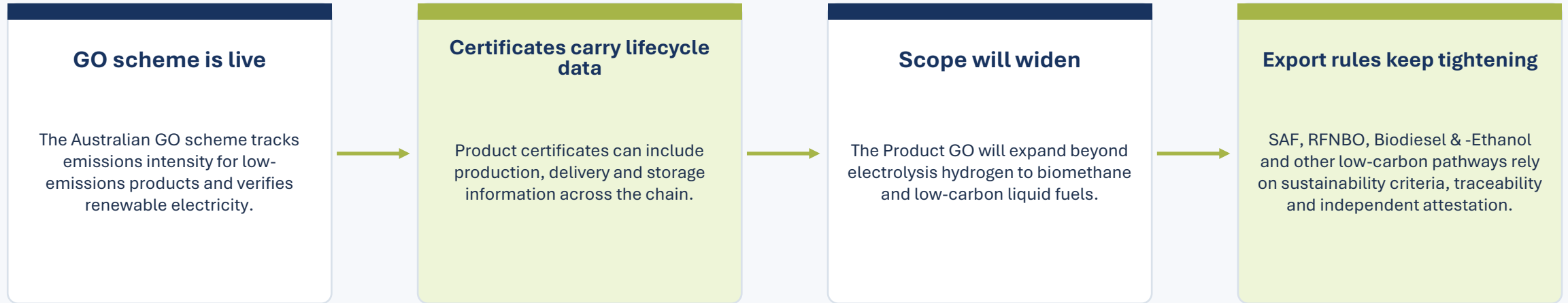
- GO and hydrogen pathway readiness
- Biomethane and low-carbon fuel programme design
- Technical scrutiny of supply-chain claims
- Consultation support and industry engagement

## Who this is for

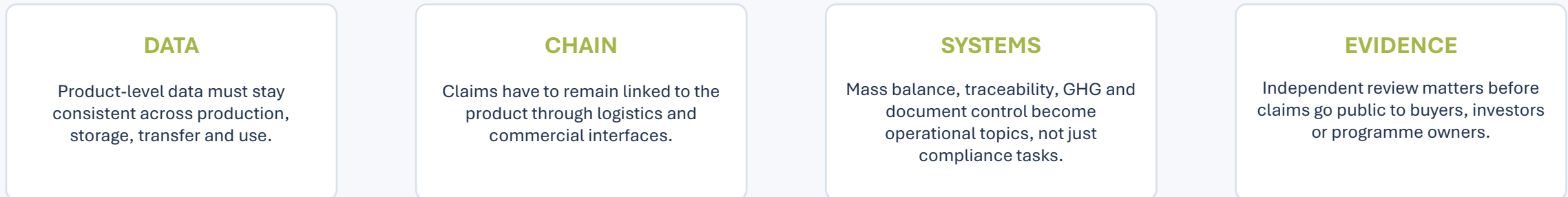
Departments  
Regulators  
Programme teams  
Trade bodies  
Public funding or pilot administrators

# Why this matters now in Australia

Market architecture is moving toward traceable, auditable product claims



## What this means operationally



TVA helps convert scheme language into an auditable operating model that works in real supply chains.

# CBAM, EU ETS and EUDR: what Australian exporters should watch



High-level regulatory snapshot for carbon-intensive goods, transport-linked emissions and deforestation-free supply chains

## Key timing points



### CBAM

EU rule

**Where exposure comes from**

Mainly metals, cement, fertilisers, hydrogen and other CBAM-listed goods moving into the EU.

**What evidence matters**

Embedded emissions data, product carbon footprint logic, importer-ready evidence and annual updates.

**Practical TVA angle**

Best TVA route: evidence-pack build, audit preparation and coordination into the TUV NORD verification path.

### EU ETS

Carbon market

**Where exposure comes from**

Relevant where aviation, maritime freight or EU industrial buyers are exposed to carbon pricing and MRV duties.

**What evidence matters**

Verified emissions data, fuel or product intensity figures and transport-chain transparency.

**Practical TVA angle**

Best TVA route: carbon-data readiness, interfaces to verifiers and support for market-facing evidence packs.

### EUDR

EU rule

**Where exposure comes from**

Deforestation-risk commodities and derived products such as cattle / leather, timber / wood, cocoa, coffee, soy, palm oil and rubber.

**What evidence matters**

Geolocation, traceability, due diligence statements and proof of no deforestation after 31 Dec 2020.

**Practical TVA angle**

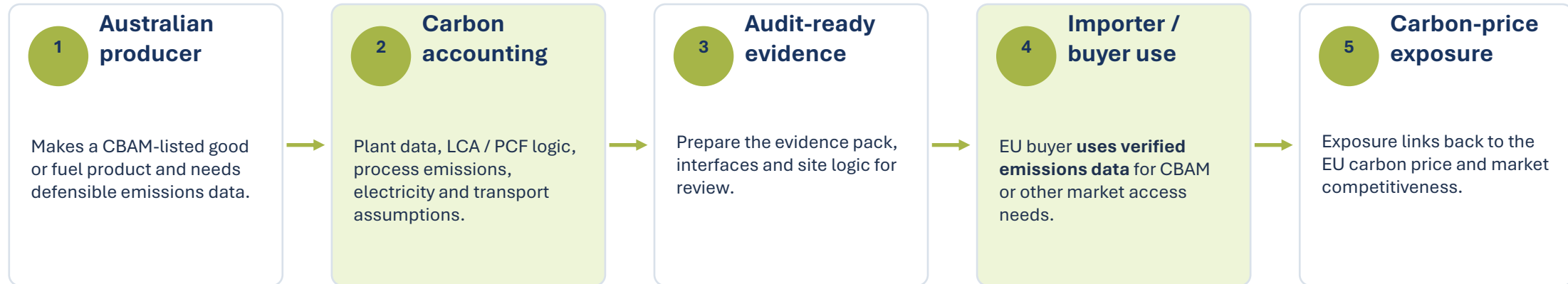
Best TVA route: traceability review, supply-chain audit planning and support into the relevant TUV NORD audit route.

**Australian sectors most likely to feel these rules first: metals, alumina / aluminium, hydrogen derivatives, fertiliser chains, aviation fuels, maritime fuel pathways, beef / leather, timber and other export-facing agricultural chains.**

# How CBAM and EU ETS move through the value chain

Carbon data, verification and commercial exposure connect

## Carbon-cost pathway for an Australian exporter



### CBAM lane

- Goods currently covered include cement, iron and steel, aluminium, fertilisers, electricity and hydrogen.
- The definitive CBAM regime started on 1 January 2026.
- Importers above the 50-tonne threshold need authorised declarant status and will buy certificates via the EU system.
- For Australian producers, the commercial issue is data quality: if emissions are not robustly evidenced, market access and cost position both suffer.

### EU ETS lane

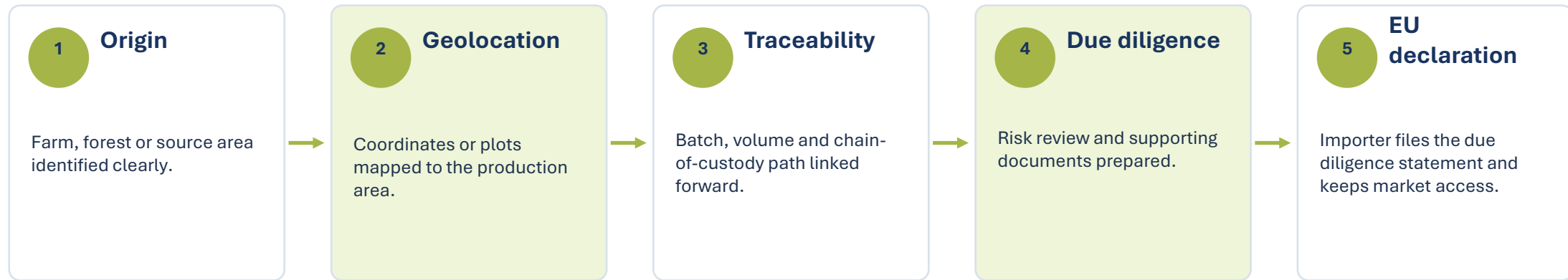
- The EU ETS covers power, industrial manufacturing and aviation, and it started covering maritime transport in 2024.
- For maritime transport, 40% of emissions were covered for 2024 and 70% for 2025 as the phase-in progresses.
- For aviation, free allocation is removed as of 2026.
- This matters to Australian supply chains where freight, aviation fuel, hydrogen or industrial exports are being compared in EU carbon-costed markets.

# EUDR: what has to travel with the product

Traceability, geolocation and due-diligence evidence for deforestation-free claims



## Evidence chain for EUDR-relevant products



### Typical products to watch

- Cattle / beef / leather
- Timber, wood and paper chains
- Rubber, cocoa, coffee, soy and palm-linked materials where relevant
- Derived products where the Annex I code brings them into scope

### What the buyer will ask for

- Proof the product is deforestation-free
- Evidence of legal production in the country of origin
- Traceability strong enough to support the due diligence statement
- Consistent records across producer, trader, exporter and importer

### Practical TVA / TUV NORD route

Map the chain first.  
Confirm where geolocation and legal-source evidence sit.  
Build the audit trail before the buyer asks for it.  
Then move into the relevant audit / verification route under the TUV NORD framework.

# ReFuelEU Aviation for Australian SAF and e-fuel chains

Rule, the hand-offs and the TVA / TUV NORD route

## How the obligation moves through the chain



### Key ReFuelEU points

- Minimum SAF share starts at 2% from 2025 and rises to 6% in 2030, then 20% in 2035.
- Synthetic aviation fuels have a sub-target of 1.2% from 2030 and 35% by 2050.
- The rule links three parties: aviation fuel suppliers, Union airports and aircraft operators.
- It is designed to curb tankering and push SAF availability across EU airports.

### Why this matters in Australia

- Relevant for SAF producers, alcohol-to-jet projects, e-fuel pathways and airport supply interfaces. Export conversations quickly turn into evidence
- conversations: feedstock, chain-of-custody, CI and dispatch records.
- Airlines and airport partners want a route that lines up sustainability claims with operational hand-off data.
- Best fit for TVA where product claims have to remain defensible from origin to airport delivery.

### TVA / TUV NORD route

- Use TVA for local audit planning, evidence-pack structure and Australian delivery coordination.
- Move through the TUV NORD framework for the relevant certification, verification or audit programme.
- Keep the message practical: pathway, evidence, hand-off points and final claim.

**Full TUV NORD certification programmes**

[tuev-nord.de/de/dienstleistungen/auditierung-und-zertifizierung/](https://tuev-nord.de/de/dienstleistungen/auditierung-und-zertifizierung/)

# Why TVA

Local delivery plus a certification-focused operating model



## Australia-facing execution

Client coordination, site access, logistics and communication handled close to the project.

## TUV NORD route

Private-sector offer is tied to audit, verification and certification delivery under the partner framework.

## Supply-chain focus

Best fit where traceability, mass balance, GHG data and claim integrity have to line up.

## Practical format

Flow-chart first, evidence-focused and adapted for teams that want a usable route, not a long memo.

## Best-fit client profiles

**Producers and refiners** Fuel, hydrogen, biomethane and low-carbon product producers

**Traders and exporters** Where documentation has to travel cleanly with the product

**Infrastructure and logistics operators** Storage, terminals, depots, port and airport interfaces

**Public-sector programmes** Scheme design, review and stakeholder engagement support

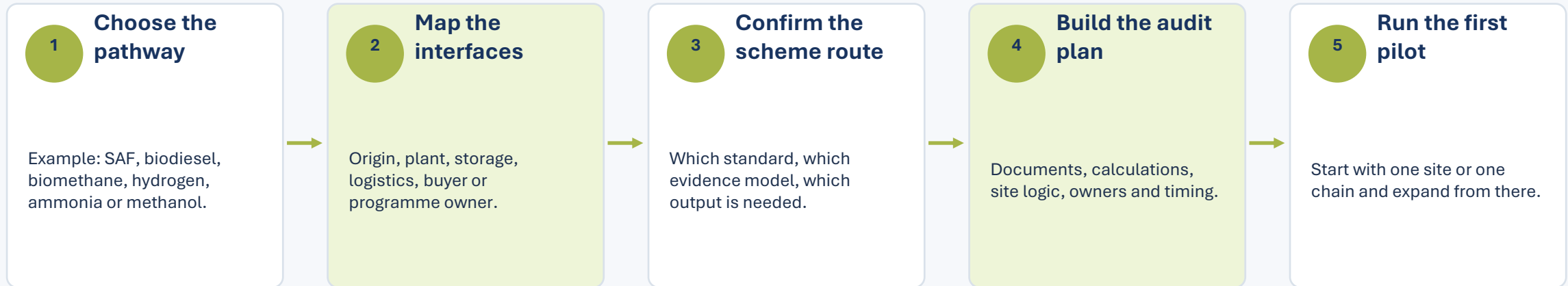
## TUV NORD pathway

Offers international accreditation not only on standard schemes but also on requirements demanded by legislation such as the EU Renewable Energy Directive III, FuelEU Maritime, EU Emission Trading System, EU Deforestation Regulation, ReFuelEU Aviation and others.

# Suggested first step for an Australian client or programme owner



Start with one pathway, one scope and one practical audit route



## Contact:

Oliver Glatow  
glatow@tva-global.au

## Details

Website: [www.tva-global.au](http://www.tva-global.au)  
TVA Technical Verification Alliance PTY. LTD