



**Planning Division**

201 N. Stone, 2<sup>nd</sup> Floor, Tucson, Arizona 85701

Phone: 520-724-9000

**DESIGN REVIEW AND TECHNICAL REVIEW COMMITTEE APPLICATION**

Owner RMG Vail II LLC c/o David Mandell Phone 773.289.4403  
 Applicant The Planning Center c/o Lexy Wellott Phone 520.623.6146  
 Applicant's Address 2 East Congress Street, Suite 600 Tucson, AZ Zip 85701  
305-15-002B, 305-14-008A, 305-14-008D, 305-14-007A, 305-14-0060,  
 Tax Code number(s) 305-14-0050, 305-14-0040, 305-14-0100, 305-19-002A Fax# \_\_\_\_\_  
 Email address lwellott@azplanningcenter.com  
 Site/Project Address: 14200 East Success Drive, 13805 East Benson Highway, 14105 East Benson Highway, 14001 East Benson Highway, 14200 East Sundown Boulevard Existing Zoning: CI-1  
 Project Name and Cross-reference case and permit activity numbers P20SC00068 and P20SC00067-01

<b><u>Check 1 request per application:</u></b>	<b>Section:</b>	<b>Action requested by applicant:</b>
<input type="checkbox"/> Buffer Overlay Zone	18.67.060	Grant exception(s)
<input type="checkbox"/> Buffer Overlay Zone	18.67.070B	Site analysis review
<input type="checkbox"/> Campus Park Industrial Zone	18.49.050B2	Conformance review
<input type="checkbox"/> Cluster Development Option	18.09.040	Conformance review
<input type="checkbox"/> Major Resort Zone	18.40.040	Architectural design review (see also 18.71.060)
<input type="checkbox"/> Historic Zone	18.63.030	Compliance review
<input type="checkbox"/> Hillside Development Zone	18.61.080	Grant exception(s)
<input type="checkbox"/> Landscape Plan Appeal	18.73.070D	Grant appeal to county decision(s)
<input type="checkbox"/> Native Plant Preservation	18.72.080D	Review of variance request appeal(s)
<input type="checkbox"/> Rural Village Center	18.71.060	Architectural design review (also see chapter 18.41 RVC)
<input type="checkbox"/> Gateway Overlay Zone	18.78.040	Site planning review
<input type="checkbox"/> Signs within bufferyards	18.79.030E4	Compatibility review
<input type="checkbox"/> Specific Plan	18.90.060B	Variance(s) to chapter 's 18.73; .75; .77; & .79.
<input type="checkbox"/> Mitigation Requirement	18.95.030C2	Hearing Officer Request for Determination
<input type="checkbox"/> Appeal	18.95.030E	Review of Appeal of Hearing Officer Action
<input type="checkbox"/> Technical Review Committee	18.99.040	Waiver or Review of Interpretation/Amend Grading Standards
<input type="checkbox"/> Supervisors rezoning requirement	Refer to rezoning condition: # _____ Case # _____	
x Other: <u>CI-1 Mixed Use Option - Design Review</u>		

**\*\*Refer to the appropriate Chapters within the Pima County Zoning Code and applicable guidelines for details on procedures and requirements. All DRC/TRC requests require: 1) a pre-application meeting; 2) a first submittal; 3) a complete submittal required for all DRC requests. A request is not placed on a meeting agenda until Staff confirms it to be a complete submittal.**

Applicant signature  Date 08/10/22

RMG Vail II LLC  
2441 North Leavitt Street  
Chicago, Illinois 60647  
(773) 384-2800

August 9, 2022

Pima County  
Development Services Department  
201 N. Stone Ave, 2<sup>nd</sup> Floor  
Tucson, AZ 85701

Subject: **Vail Crossings Mixed Use Design Guidelines – Design Review Application**  
for Tax Parcels: 305-15-002B, 305-14-008A, 305-14-008D, 305-14-007A, 305-14-0060, 305-14-0050, 305-14-0040, 305-14-0100, 305-19-002A

Pima County Development Services Department:

As the Manager of the owner of the above-referenced tax parcels, I hereby authorize The Planning Center and Bourn Companies to act as the owner's agents throughout the Design Review application process. This authorization shall remain in effect unless and until it is revoked in writing by the owner.

Sincerely,

  
Signature \_\_\_\_\_ Date 8-9-22

  
Title \_\_\_\_\_



# VAIL CROSSINGS

## DESIGN GUIDELINES





# VAIL CROSSINGS

## DESIGN GUIDELINES

*Prepared for:*

RMG VAIL II LLC  
2441 North Leavitt Street  
Chicago, IL 60647  
(520) 777-4949

*Prepared by:*

THE PLANNING CENTER  
2 East Congress Street, Suite 600  
Tucson, AZ 85701  
(520) 623-6146

*Assisted by:*

BOURN COMPANIES  
20 East Congress Street, Suite 300  
Tucson, AZ 85701  
(520) 323-1005

*June 2022*

# TABLE OF CONTENTS

PROJECT OVERVIEW .....	1
A. Introduction .....	2
B. Purpose .....	3
PART I – BACKGROUND.....	7
A. History .....	8
B. Mixed Use Option.....	9
1. Landscaping.....	10
2. Site Design.....	10
PART II – DEVELOPMENT AREAS.....	12
A. Northern Development Area .....	13
B. Southern Development Area .....	16
PART III – DEVELOPMENT PROPOSAL.....	19
A. Northern Development Area Development Plan.....	20
B. Southern Development Area Development Plan .....	25
PART IV – DESIGN STANDARDS.....	29
A. Introduction .....	30
1. Purpose and Intent .....	30
2. Design Goals.....	30
B. General Site Design Standards .....	32
C. Residential Architectural Design Standards .....	34
1. Residential Architectural Style.....	34
2. Residential Architectural Criteria .....	36
D. Nonresidential Architectural Design Standards .....	46
1. Nonresidential Architectural Style.....	46
2. Nonresidential Architectural Criteria .....	47
E. Community Entries, Signage, Theme Walls, and Public Art Design Standards.....	51
1. Entries and Theme Walls.....	51
2. Site Signage .....	55
3. Public Art.....	57

F. Circulation and Streetscape Standards .....	61
1. Collector Roads .....	62
2. Collector Road Streetscape Design Criteria .....	64
3. Local Roads .....	66
4. Local Road Streetscape Design Criteria .....	67
G. Landscape Design Standards .....	70
1. Residential Landscape .....	73
2. Nonresidential Landscape .....	74
3. Vail Crossings Specimen Plant List .....	76
4. Site Furnishings .....	80
5. Hardscape .....	81
H. Recreation and Open Space .....	83
1. Parks and Open Space .....	83
2. Neighborhood Pocket Parks .....	86
3. Community Park .....	87
4. Open Space (Paths, Trails, and Greenbelts) .....	88
I. Views and Lighting .....	89
1. Views .....	89
2. Lighting .....	90
<b>PART V – IMPLEMENTATION &amp; ADMINISTRATION .....</b>	<b>91</b>
A. Purpose .....	92
1. General Implementation .....	92
2. Interpretation .....	92
3. Development Review Procedures .....	92
<b>PART VI – CONFORMANCE MATRIX .....</b>	<b>93</b>
A. Layout Guidelines .....	94
B. Parking, Vehicular, And Pedestrian Access Guidelines .....	95
C. Landscaping Guidelines .....	96
D. Signage Guidelines .....	97
E. Safety Guidelines .....	97
F. Context Guidelines .....	98
<b>APPENDIX – VAIL CROSSINGS LETTER OF UNDERSTANDING .....</b>	<b>99</b>

---

## TABLE OF EXHIBITS

Exhibit II.A.1: Northern Development Area .....	13
Exhibit II.A.2: Northern Development Area Surrounding Land Uses.....	14
Exhibit II.B.1: Southern Development Area .....	16
Exhibit II.B.2: Southern Development Area Surrounding Land Uses .....	17
Exhibit III.A.1: Northern Development Area Development Plan .....	23
Exhibit III.B.1: Southern Development Area Development Plan.....	27
Exhibit IV.C.1: Residential Architectural Styles .....	35
Exhibit IV.C.2.n: Residential Color Palette .....	42
Exhibit IV.E.2: Examples of Site Signage .....	56
Exhibit IV.E.4: Illustrative Public Art Imagery .....	58
Exhibit IV.E.1.a: Northern Development Area Entries and Theme Walls.....	59
Exhibit IV.E.1.b: Southern Development Area Entries and Theme Walls.....	60
Exhibit IV.F.i: Northern Development Area Circulation.....	68
Exhibit IV.F.ii: Southern Development Area Circulation.....	69
Exhibit IV.H.1: Northern Development Area Recreation and Open Space .....	84
Exhibit IV.H.2: Southern Development Area Recreation and Open Space .....	85



# VAIL CROSSINGS

## PROJECT OVERVIEW



# PROJECT OVERVIEW

## A. Introduction

RMG Vail II, in partnership with Bourn Companies, a local Tucson land developer who has a vested interest in expanding and improving the Tucson-metro region, is pleased to present Vail Crossings, a master-planned community nestled south of the western limits of the Cienega Creek Natural Preserve. Located in the high-performing Vail Unified School District (VUSD) and near Interstate 10 (I-10) and Union Pacific Railroad lines, Vail Crossings aims to establish a vibrant mixed use community that creates opportunities for families to live, work, and play in a highly desirable area while serving as the eastern gateway for interstate travelers into and out of Pima County.

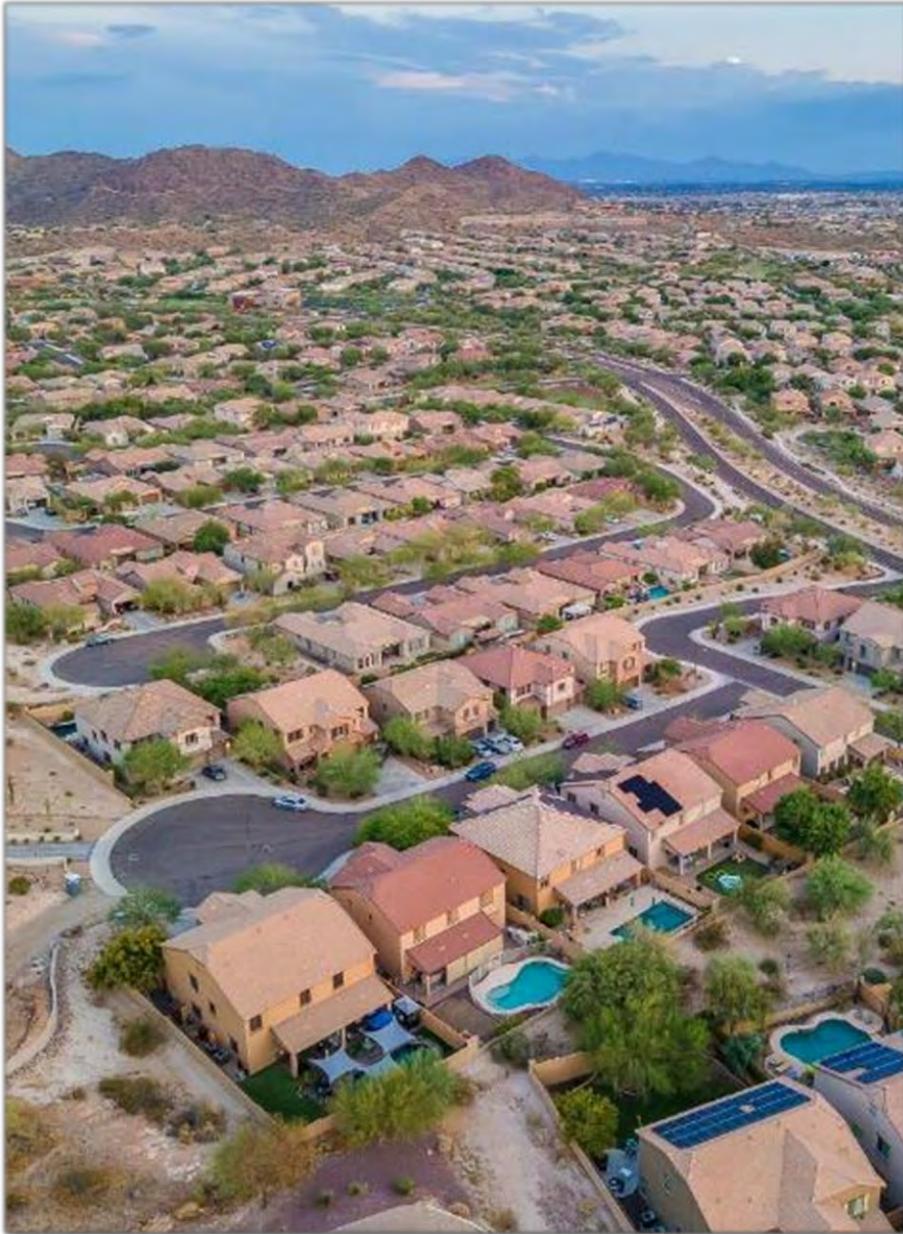
In March 2022, Vail, Arizona was rated as the #2 best place to raise a family in the west by Dwellics.com (*Dwellics.com, 2022*). In addition to providing opportunities to support the continued success of Vail and the Vail Unified School District, Vail Crossings includes housing and commercial opportunities for the 3,000+ anticipated employees of the Rosemont Copper Mine (*Hudbay Minerals, Inc.*), which is located less than 13-miles south of Vail Crossings on State Route 83 (SR-83).

Vail Crossings will convert vacant industrial property that has remained unused for decades into a neighborhood that celebrates the built and natural environment in an area largely devoid of new development yet burgeoning with demand. It also enhances Pima County's ability to

address housing supply shortages and generate tax revenues by increasing Vail's housing availability for families seeking to purchase their first homes or major employers that require direct freeway or rail access, frontage, or visibility. Furthermore, Vail Crossings expands Pima County's economic base with high-quality opportunities for a mix of housing and employment while preserving the surrounding area's character.

By implementing carefully crafted design standards established herein, Vail Crossings will create a community that emphasizes design excellence and affordability while fostering community identity and a sense of place. Furthermore, the design standards also assure that nonresidential uses (commercial, office, industrial, etc.) and residential uses are integrated spatially and visually through thoughtful land use configurations, vehicular and pedestrian connectivity, common landscape and architectural themes, signage, and interconnected open spaces.





## B. Purpose

For several years now, communities nationwide have struggled to provide an adequate and affordable housing stock for people living in urban and rural neighborhoods. Population increases coupled with the impacts of the COVID-19 Pandemic (rising home prices, labor shortages, and the rising cost of building materials) have made it increasingly challenging to address housing shortages and affordability. Many communities have begun to enact policy changes or zoning tools that stimulate residential development to alleviate these challenges.

In Arizona specifically, a bipartisan bill (HB 2674) was recently introduced to the Arizona House of Representatives with the intent of providing a mechanism to combat the housing crisis local governments are facing across the state. In essence, HB 2674 would allow for the by-right development of higher-density residential uses regardless of guiding General Plan / Comprehensive Plan land use designations as long as the development complies with local zoning code. Although the bill did not advance, HB 2674 demonstrates a concerted effort on the State Legislature's part to address housing supply and affordability deficiencies.

Over the years here in Pima County, local jurisdictions have been actively revising their codes and policies to encourage innovative procedures to address inventory shortages while encouraging development of various housing types. In the City of Tucson, recent

policy adoptions have allowed for accessory dwelling units on properties where the existing zoning would otherwise prohibit a secondary dwelling unit. Pima County allows secondary dwellings in the GR-1 and RH zones and guesthouses on larger lots, but those provisions are limited to family members and guests of whoever is occupying the main home. In the Town of Marana, parking requirements have been modified to allow the market to dictate parking needs, allowing for more developable space and ultimately more housing units. In unincorporated Pima County, the zoning code provides an opportunity for limited residential development in industrial zones. These code modifications and policies have opened the door for the region to begin addressing housing challenges.

In unincorporated Pima County specifically, there have been challenges providing an adequate housing supply to accommodate the growing demand for residential properties on the County's eastern side, particularly in the Vail area due to its majestic scenery, improved roadway infrastructure, and the highly performing Vail Unified School District. Extraordinary challenges in providing an additional housing stock can be attributed to a diminishing supply of entitled land to support master-planned communities and development limitations due to a lack of utility infrastructure and the proximity to environmentally sensitive resources like the Cienega Creek Natural Preserve (see map to the right of unentitled State Land holdings and preservation land that make up most of the remaining vacant property available for development in the Vail area). As a result, a focus has



shifted to re-envisioning industrially zoned properties located in the Vail area, particularly those that have remained vacant since becoming industrially zoned and are adjacent to existing homes and residentially zoned properties.

Two industrially zoned sites totaling approximately 391 acres and generally located north of I-10 at the SR-83 interchange have been of particular interest to the development community, mainly residential homebuilders. The two sites are non-contiguous and



known as Vail Crossings. For clarity, the two sites are further identified as the Northern Development Area and Southern Development Area. The current CI-1 (Light Industrial) zoning of the two sites allows for residential as a mixed use development with approval from Pima County's Design Review Committee (DRC) and demonstrating compliance with the parameters established by the Mixed Use Option and associated design guidelines of the CI-1 zone. The Vail Crossings Design Guidelines presented herein provide an innovative and comprehensively planned framework for developing Vail Crossings as two separate mixed use communities that effectively bring together housing, commerce, and industry through appropriate transitions, common design themes, and connected open spaces on each site.

The following document has been prepared in accordance with the parameters set forth in Section 18.51.050 of the *Pima County Code* and the associated *Mixed Use Option Design Guidelines* for the purpose of establishing a comprehensive framework that informs the overall design of Vail Crossings. The Vail Crossings Design Guidelines intend to ensure that Vail Crossings develops as a harmonious mixed use community in which nonresidential uses and residential uses are integrated spatially and visually through thoughtful land use configurations, vehicular and pedestrian locomotion, common landscape and architectural themes, signage, and interconnected open spaces.

In recognition that Vail Crossings is non-contiguous, and the opportunities and constraints presented for each development area are unique, design guidelines have

been formulated for each site based specifically on land use plans. While the overall configuration and inspiration for each development area may differ due to the unique characteristics of each site, use-specific standards have been crafted to ensure that both Vail Crossings development areas develop with consistent guidelines implemented to enhance the attributes and unique characteristics of each site.

This document has been structured to present the overall history and vision of Vail Crossings, how it is anticipated to develop based on an analysis of each development area and its surroundings, and how it conforms to the overall objectives of the *Mixed Use Option*. It includes the following six parts:

*Part I – Background* provides an overview of the history of Vail Crossings and the rationale for pursuing the Mixed Use Option.

*Part II – Development Areas* provides a brief analysis of the physical characteristics of each development area within

Vail Crossings as well as the existing conditions of the surrounding area.

*Part III – Development Proposal* outlines the overall vision of Vail Crossings as it relates to proposed land uses, circulation patterns, landscaping, and the character of development.

*Part IV – Design Guidelines* inform the overall design to ensure that Vail Crossings develops as a cohesive and compatible mixed use community for each site and the anticipated residents and end-users.

*Part V – Implementation and Administration* guides the implementation and general administration of the design guidelines and outlines interpretation and amendment procedures.

*Part VI – Conformance Matrix* demonstrates compliance with the Mixed Use Option Design Guidelines.



# VAIL CROSSINGS

## PART I - BACKGROUND



# I. BACKGROUND

## A. History

In 2018, RMG Vail II LLC purchased two sites (the Northern Development Area and Southern Development Area). Since the 1950s/early 1960s, these properties have remained vacant with little to no interest from the development community. The lack of interest is primarily due to a lack of infrastructure, lack of a dense enough populous to support commercial services, and the limitations of the existing CI-1 entitlements (which prohibit residential land uses). In 2005, a development team led by Bob Hoffman, a principal in the development of the Mall of America, envisioned an ambitious plan to develop the site for an urban, mixed-use development known as *The Passages of Tucson* that would include 6.2 million square feet of retail and commercial space and 2,400 high density housing units. The proposal spurred a code amendment that created a mixed use option for industrially zoned properties in Pima County. However, the ambitious project never became a reality, ultimately creating this opportunity for a local Tucson developer to craft an innovative approach to bringing a successful new project to this end of the metropolitan area.

Immediately upon purchasing the sites, RMG Vail began strategizing with Pima County to formulate a sewer extension agreement to extend sewer infrastructure to both areas while working with site-selector agencies like Sun Corridor Inc. to market them to nonresidential users. An extension of the sewer line is critical to spurring



development in this portion of Pima County. Although a sewer extension agreement with Pima County has been secured, RMG Vail continues to seek a nonresidential user mainly because of the same limiting factors that have kept the sites vacant since receiving their industrial zoning status.

Although they are still awaiting nonresidential users, RMG Vail has received significant interest from the housing industry, particularly single-family homebuilders. In recognition that this area is burgeoning with demand for single-family homes, RMG Vail has been exploring options that would allow them to be developed here. In most settings, the logical choice would be to process a rezoning to a less intensive zoning category that allows for residential uses. However, in Pima County, the rezoning process presents a nexus that triggers compliance with the *Maeveen Marie Behan Conservation Lands System (MMBCLS)*.

The MMBCLS “*categorizes and identifies locations of priority biological resources within Pima County and provides policy guidelines for the conservation of these resources*” (Pima County). Because of the Biological Core designation assigned by the MMBCLS, the majority of both sites (80%) would have to remain as natural undisturbed open space if rezoned, despite the fact that their CI-1 zoning entitlements currently allow for nearly complete disturbance. In other words, processing a rezoning would render most of Vail Crossings undevelopable, thus limiting RMG Vail’s ability to effectively contribute to the region’s housing supply shortage and further making the infrastructure extensions for any new development cost-prohibitive. Since rezoning is not feasible or conducive to alleviating housing supply shortages under these circumstances, the alternative for Vail Crossings is to pursue the Mixed Use Option as permitted in the CI-1 zone.

In December 2021, RMG Vail and Pima County created a Letter of Understanding (LOU) for the Southern

Development Area setting forth expectations for Vail Crossings and the application requirements for pursuing the Mixed Use Option under the CI-1 zone (refer to *Appendix – Vail Crossings Letter of Understanding*). Since establishing the LOU, RMG Vail has formed a partnership with Bourn Companies to craft design guidelines for Vail Crossings that promote a horizontally mixed use community. Bourn Companies and Pima County recently established an LOU for the Northern Development Area outlining similar expectations and application requirements (submitted under separate cover).

## B. Mixed Use Option

The Mixed Use Option is a tool that the Pima County Board of Supervisors enacted in 2006 to:

1. “*Promote and facilitate innovative and comprehensively planned mixed use developments which are compatible with surrounding uses; and*
2. *Allow residential uses in the CI-1 zone where appropriately mixed with nonresidential” (Pima County Zoning Code).*

In essence, the Mixed Use Option allows for the development of communities that provide a mix of residential and nonresidential land uses to reduce the distance between where people live, work, and play. This is done while ensuring high-quality design and compatibility internally and externally through conformance with specific design guidelines that emphasize overall site layout, parking and circulation,

landscaping, signage, and safety. The Mixed Use Option affords Bourn Companies and RMG Vail the opportunity to develop Vail Crossings with an interconnected residential and nonresidential mix of uses subject to approval from the Design Review Committee; thus, the impetus for establishing these design guidelines.

In addition to design guideline conformance, the Mixed Use Option also requires adherence to the following development standards per Section 18.51.070 of the Pima County Code.

### 1. Landscaping

- a. Mixed use developments are exempt from Section 18.73.040 Screening and Bufferyard Requirements and Sections 18.78.030(B)(2)(d), 18.78.030(C)(2)(a)&(d), and 18.78.030(F)(2)(c) Gateway Development Standards.
- b. Native Plant Preservations Plans shall be submitted in accordance with Chapter 18.72.

### 2. Site Design

- a. Permitted Coverage: a minimum of 20% of the total site development and a maximum of 50% of the total site development shall be residential use. Residential development constructed over or under other uses shall count towards permitted coverage.
- b. Maximum building height: 90 feet.

- c. All nonresidential uses within 2,500 feet of a residential structure shall adhere to the performance standards set forth in Section 18.43.020(B).
- d. For residential structures, or mixed use structures containing residential uses: A minimum distance of 14 feet shall separate exterior walls of separate buildings containing dwelling units on the same lot except for multi-family developments consisting of detached units (casita-style or build-for-rent). The windows or window/doors of any one dwelling unit may not face the windows or window/doors of any other dwelling unit unless separated by a distance of 14 or more feet except where the angle between the wall of the separate dwelling units is 90 degrees or more. Walls parallel to each other shall be considered to be at a zero degree angle.



Per Pima County code section 18.51.70, mixed use is defined as “the combining of retail/commercial and/or service uses with residential or office use in the same building or on the same site in one or more of the following ways:

1. **Vertical Mixed Use.** A single structure with the above floors generally used for residential or office use and with all or a portion of the ground floor for retail/commercial or service uses.
2. **Horizontal Mixed Use — Attached.** A single structure which provides retail/commercial or service use in the portion fronting the public or private street with attached residential or office uses behind.

3. **Horizontal Mixed Use — Detached.** Two or more structures on one site which provide retail/commercial, or service uses in the structure(s) fronting the public or private street, and residential or office uses in separate structure(s) behind or to the side.”

Due to surrounding development patterns and the costs and conflicts associated with vertical mixed use structures and single-story buildings that combine residential and nonresidential uses, demand for these types of mixed use is not anticipated within Vail Crossings. Instead, Vail Crossings will consist of thoughtfully designed detached horizontal mixed use that forms a cohesive development through unified signage, streetscapes, pedestrian pathways, landscaping, materials, colors, etc.





# VAIL CROSSINGS

## PART II - DEVELOPMENT AREAS

## II. DEVELOPMENT AREAS

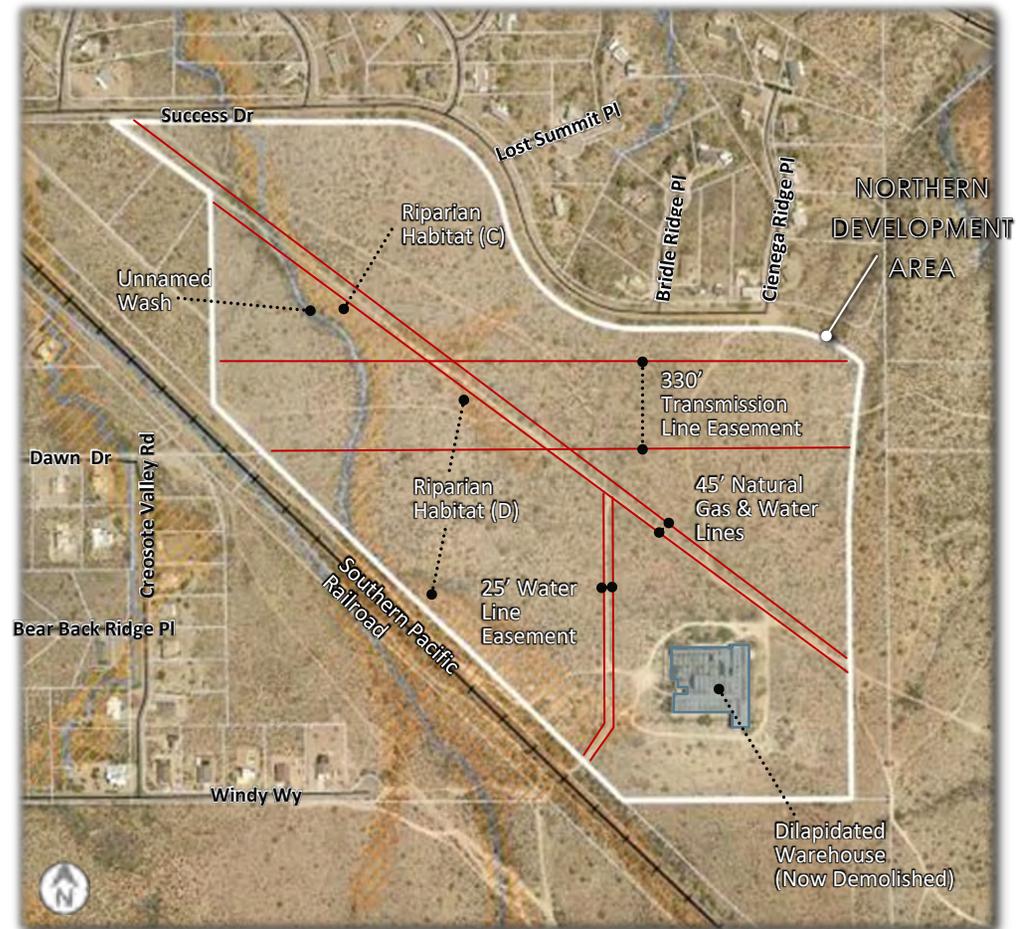
As previously stated, Vail Crossings consists of two non-contiguous development areas (Northern and Southern) located on approximately 391 acres on the north side of I-10 at the I-10/ SR-83 interchange in eastern unincorporated Pima County, Township 16 South, Range 16 East, Sections 15, 22, and 27 East. The following provides a brief analysis of the physical characteristics of each development area and the existing conditions of the surrounding area.

### A. Northern Development Area

As shown in *Exhibit II.A.1*, the Northern Development Area of Vail Crossings consists of an approximately 97.5-acre parcel zoned CI-1 (light industrial/warehousing) and located about 1.5 miles north of the I-10 and SR-83 / Marsh Station Road interchange at 14200 E Success Drive.

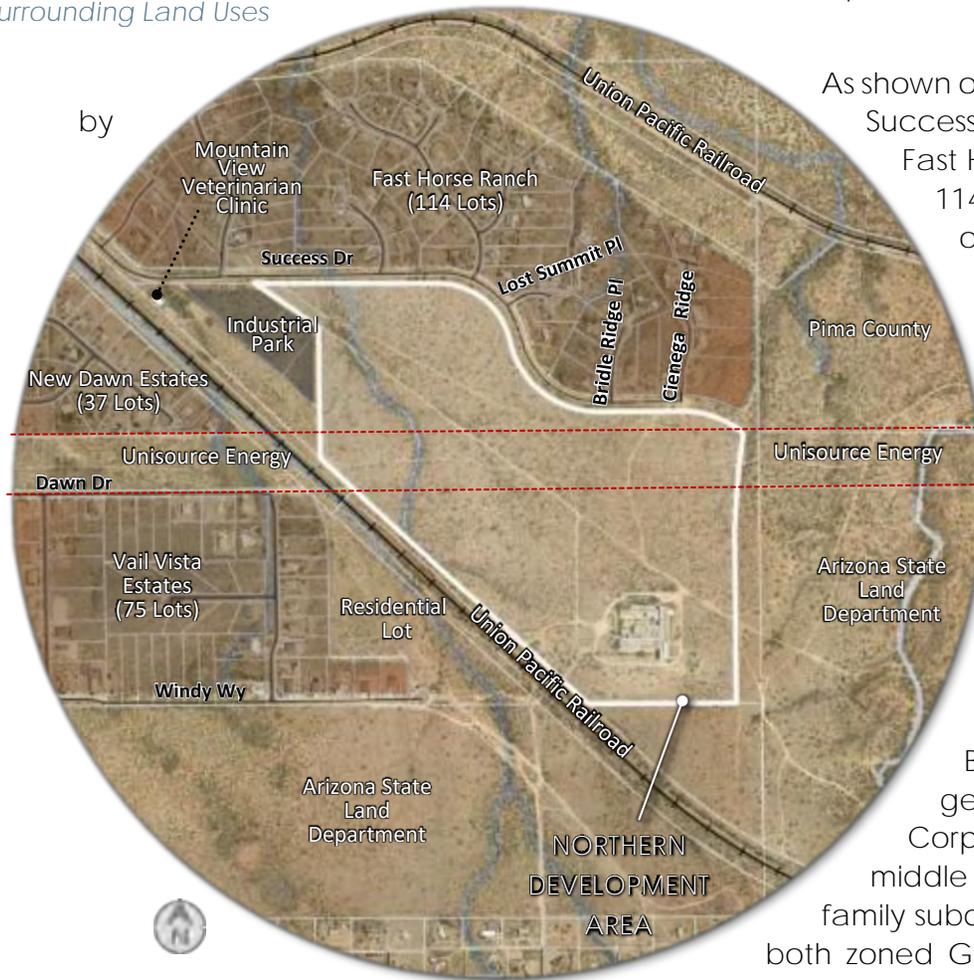
The parcel is currently vacant with several easements crossing it. The easements include a 30-foot El Paso Natural Gas easement and a 15-foot water line easement that bisect the site diagonally in the northwest to southeast direction, a 330-foot major transmission line easement that bisects the central portion in the east-west direction, and a 25-foot water line easement that generally bisects the southern half of the development area in the north-south direction. The easements, particularly the 330-foot transmission line easement, greatly divide the parcel yet offer opportunities to provide a comprehensive approach to mixing land uses. Access to the Northern Development Area is via Success Drive, a paved local road off East Colossal Cave Road that can be accessed from I-10 or Mary Ann Cleveland Way. Informal dirt roads/trails

Exhibit II.A.1: Northern Development Area



also provide access from the east. While not constructed, right-of-way has been dedicated along the eastern boundary for the future extension of Camino Loma Alta. Aside from disturbances caused by the former use, utility infrastructure installation and maintenance, and informal trail/road networks established by nearby residents, the Northern Development area is characterized as desert scrubland with some regulated riparian habitat (Xeroriparian Classes C and D) adjacent to the wash that runs along the western/southern portion of the site. The unnamed wash is estimated to produce flows of less than 500 cubic feet per second (cfs).

Exhibit II.A.2: Northern Development Area Surrounding Land Uses



As shown on *Exhibit II.A.2*, the Northern Development Area is bounded by Success Drive and the Fast Horse Ranch subdivision to the north. Fast Horse Ranch, zoned SH (Suburban Homestead), consists of 114 single-story, single-family residences with a typical lot size of one acre or more, and an overall density of 0.82 residences per acre (RAC). Immediately east of the Northern Development Area is largely vacant land owned by Pima County, Unisource Energy Corp., and the Arizona State Land Department (ASLD). The Pima County and Unisource Energy Corp. properties are zoned RH (Rural Homestead) while the ASLD property to the east and immediately to the south is zoned GR-1 (Rural Residential). The Northern Development Area is bordered on the west by a vacant industrial park (northern third), Mountain View Veterinarian Clinic (northern third) and a Union Pacific Railroad (UPRR) line (southern two-thirds).

Beyond the UPRR line to the west is a 330-foot wide strip of generally vacant RH zoned land owned by Unisource Energy Corp. (extension of the transmission line easement through the middle of the development area) that sits between two single-family subdivisions, New Dawn Estates and Vail Vista Estates, that are both zoned GR-1. New Dawn Estates consists of 37 single-story, single-

family residences with typical lot sizes of less than one acre and an overall density of one RAC. Vail Vista Estates consists of 75 lots, generally less than one acre in size, and an overall density of 0.89 RAC. A vacant triangular parcel with GR-1 zoning sits between the UPRR line and Vail Vista Estates. The Unisource Energy Corp. parcel located on either side of the Northern Development Area contains major electrical transmission lines that span east-west through the property within the 330-foot easement. These physical characteristics of the site, particularly the surrounding residential subdivisions and major transmission lines essentially divide the Northern Development Area into two pieces. Because of the inherent division in the site and the adjacent land uses, land uses anticipated within the Northern Development Area will likely mimic the intensity of the adjacent land use to ensure compatibility and meet user preference to avoid nuisance claims.

Per an agreement with Pima County and RMG Vail II, sewer services will be extended **at Developer's expense** to the parcel from an existing line located northwest of the site in East Mary Ann Cleveland Way. The Northern Development Area is located within the Tucson Electric Power and Vail Water Company obligated service areas. Developer has to expand the existing water infrastructure to provide additional capacity.



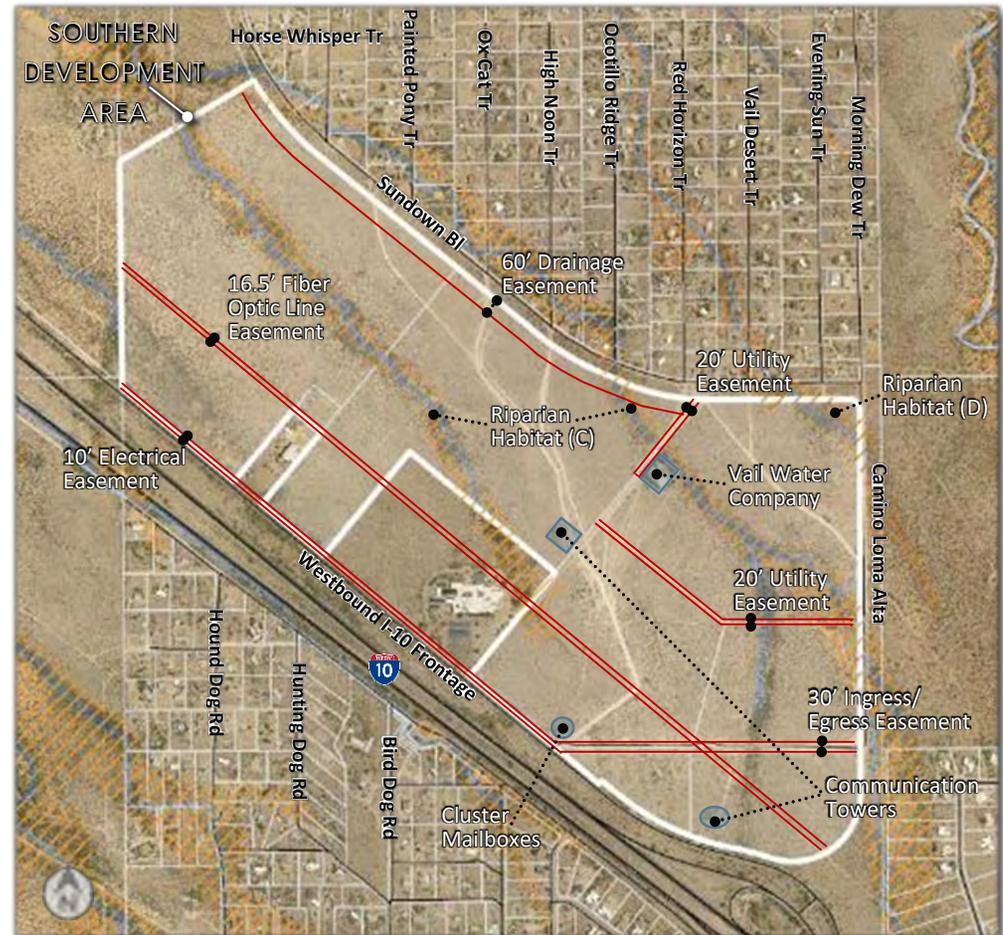
*Oblique aerial image of central portion of Northern Development Area*

## B. Southern Development Area

As shown on *Exhibit II.B.1*, the Southern Development Area of Vail Crossings consists of eight contiguous parcels totaling approximately 293 acres, zoned CI-1, and located along the north side of I-10 at the I-10 and SR-83 / Marsh Station Road interchange at 13805-14105 E Benson Highway.

The site is currently vacant with communication towers located on the southernmost parcel closest to the I-10/SR-83 interchange (Parcel No. 305-14-0100) and the parcel located north of the Vail Unified School District offices (Parcel No. 305-14-007A). Cluster mailboxes are also located on-site near the intersection of the Westbound (WB) I-10 Frontage Road and the informal Camino Loma Alta alignment on Parcel No. 305-14-0060. A separate parcel at 14100 East Sundown Boulevard with water distribution and storage facilities owned by Vail Water Company is wholly contained within Vail Crossings and is surrounded by the northeastern most parcel (Parcel No. 305-14-0050). A number of easements cross the site including: a 60-foot drainage easement, two 20-foot utility easements, a

Exhibit II.B.1: Southern Development Area



30-foot ingress/egress easement, a 16.5-foot fiber optic line easement and a 10-foot electrical easement (see *Exhibit II.B.1 for easement locations*).

*Exhibit II.B.2: Southern Development Area Surrounding Land Uses*

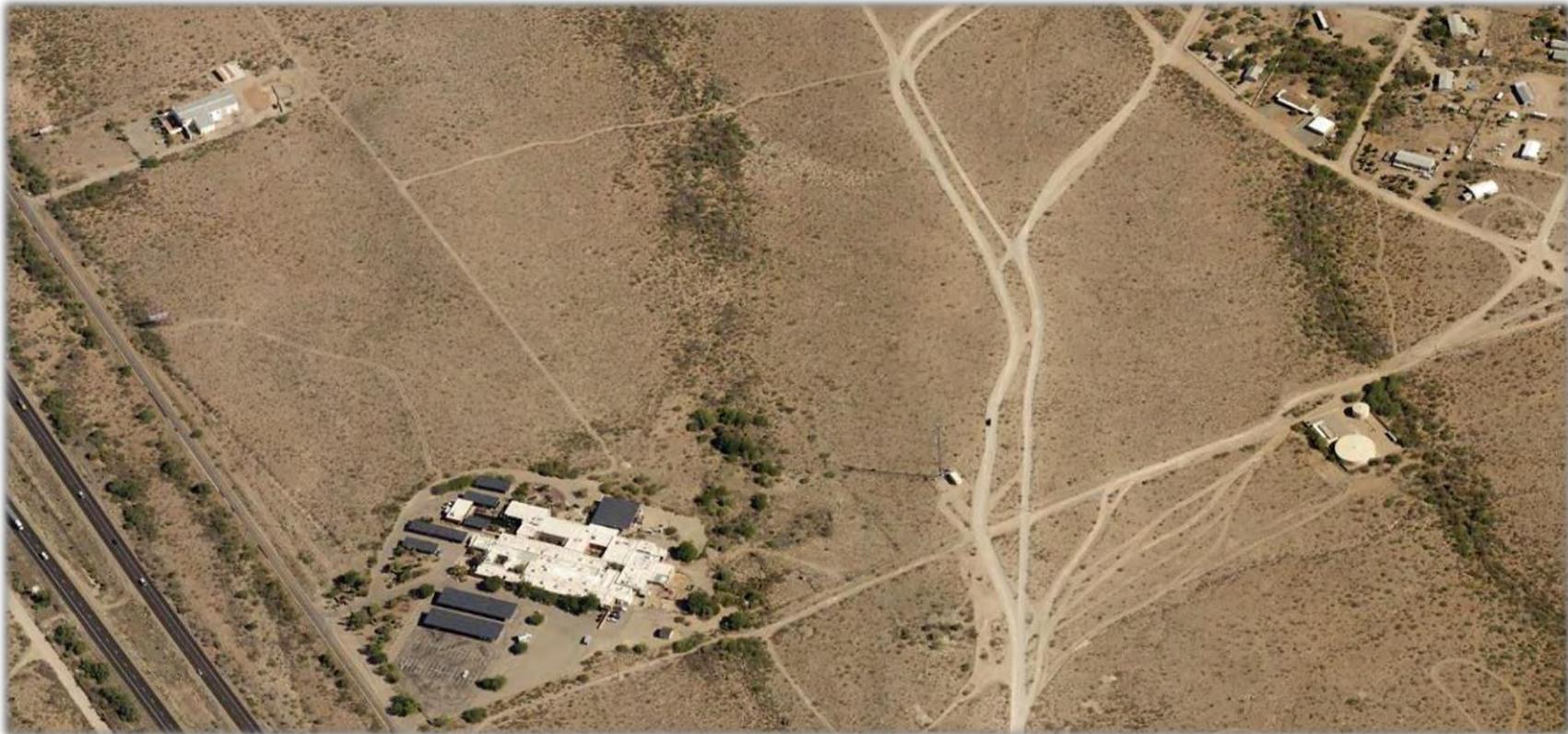


Access to the Southern Development Area is via the WB I-10 Frontage Road along the southern boundary. While not constructed, right-of-way has been dedicated along the eastern boundary for future extension of Camino Loma Alta. Aside from disturbances caused by maintaining the communication towers and offsite Vail Water Company water facilities, and the informal roads/trail networks established by nearby residents, the southern development area is generally characterized as desert scrubland with some regulated riparian habitats (Classes C and D) adjacent to the washes that cut through the central and northeastern portions of the development area in the northwest to southeast general direction. All washes onsite are unnamed and are estimated to produce flows less than 500 cfs (wash traversing central portion and northeast corner) or between 500 cfs and 1,000 cfs (wash traversing the northeast portion).

The Southern Development Area is bounded by Sundown Boulevard, a non-paved local road, an unregulated subdivision (zoned GR-1), and vacant industrially zoned (CI-1) lots located in 'Tucson Park' to the north. Camino Loma Alta borders the eastern side of the development area. Beyond the dedicated right-of-way is a privately-owned vacant parcel, zoned GR-1 and the Mountain View Ranch subdivision. The Mountain View Ranch subdivision consists of 362 lots, generally one acre in size, with an overall density of 0.77 RAC. The Southern Development Area is bounded by I-10 and the WB 1-10 Frontage Road, the Vail Unified School District offices and an abandoned winery to the south, and vacant ASLD land to the west (refer to *Exhibit II.B.2: Southern Development Area*

*Surrounding Land Uses*). South of I-10 are a number of unregulated residential lots and the Sonoita Ranch subdivision which consist of 50 lots, generally over an acre in size, with an overall density of 0.88 RAC. Because of the adjacent land uses and the constraints present, it is likely that the Southern Development Area will feature some nonresidential development in its eastern portion yet most of it will be located on the west side.

Per an agreement with Pima County and RMG Vail II, sewer services will be extended **at Developer's expense** to the site from an existing line located northwest of the site in East Mary Ann Cleveland Way. The Southern Development Area is located within the Tucson Electric Power and Vail Water Company obligated service areas. Developer has to expand the existing water infrastructure to provide additional capacity.



*Oblique aerial image looking northeast at the central portion of Southern Development Area*



# VAIL CROSSINGS

## PART III - DEVELOPMENT PROPOSAL

### III. DEVELOPMENT PROPOSAL

Vail Crossings is designed to provide a harmonious mix of uses that effectively blend housing, commerce, and industry through appropriate transitions, common design themes, and connected open spaces with the goal of creating a vibrant community where people have the ability to interconnect the live, work, and play aspects of their lifestyle. Recognizing that Vail Crossings is physically separated, and each development area presents unique opportunities, development proposals have been prepared for each development area in response to their characteristics and the surrounding land uses.

Despite the two sites being non-contiguous, the standards presented in the next section ensure that each development area features high quality and visually appealing designs that are appropriately scaled and activate the pedestrian realm while celebrating the natural beauty of the area through the use of native plant materials and earth-toned color palettes. The proposal for each development area emphasizes:

- 1) the importance of site configuration to create a sense of place grounded in the context of the surrounding area,
- 2) current development trends and user preferences,
- 3) interconnectivity to create safe and efficient spaces, and
- 4) community identity.

While each development proposal presents a unique approach to integrate uses, the plans presented for each development area are conceptual in nature and are intended to serve as illustrations that demonstrate how Vail Crossings is envisioned to develop. Final site configurations and uses, including various residential housing types, will be determined at the time of development and will generally be consistent with the overarching design elements showcased in each development plan. Currently, the number of residential units planned within Vail Crossings is a combined 1,065 units.

Per the LOU established between Pima County and the property owner, and Pima County Code Section 18.51.070.E.3, final site configurations may be modified up to 25% subject to approval by the Planning Official. The LOU also establishes an overall density cap for Vail Crossings which may not exceed more than 10% of the 1,065 planned units. The design standards presented in the next section, and the Implementation and Administration portion of this document, ensure Vail Crossings adheres to the Mixed Use Option Design Guidelines with an interconnected and harmonious design aesthetic that creates a comprehensive approach to mixing uses.

#### A. Northern Development Area Development Plan

As discussed in *Section II.A* and displayed in *Exhibit III.A.1: Northern Development Area Development Plan*, the Northern Development Area is highly constrained with

physical impediments that create clear divides in the site. These divisions offer an exciting opportunity to develop Vail Crossings with unifying design elements in keeping with the intent of the mixed-use design guidelines. In addition to its physical characteristics, the existing land uses and zoning designations adjacent to the site present compatibility considerations. Recognizing that compatibility is at the forefront of creating vibrant communities, the Northern Development Area positions similar development types next to one another and utilizes



the major transmission lines, roadways (existing and proposed), and open spaces to transition between uses. Due to the presence of the UPRR line and the vacant industrial park that border the Northern Development Area on the west, opportunities for logistics and manufacturing, and other nonresidential uses that provide

jobs and commercial services are anticipated to develop along the western boundary. These uses will likely span northeasterly toward the end of Success Drive, with residential uses located on the northwest and southeast ends of the development area. Nonresidential buildings will be oriented away from residences where possible, and toward public streets with logical site transitions (i.e. varied setbacks, landscaping, screening, parking area siting, etc.) to ensure compatibility, activate streets, and create a sense of arrival into the nonresidential areas.



Two subdivisions consistent with the TR (Transitional) Zone are proposed within the Northern Development Area to complement and buffer the adjacent residentially zoned properties from nonresidential land uses. Lots within the Northern Development Area are intended to be sized to provide opportunities for first-time homebuyers, working professionals, and families, and may consist of one- and two-story homes. Since the exact residential housing layout and mix has not been determined yet, some of the single-family residential depicted in [Exhibit III.A.1](#) could develop as multifamily housing.

As shown in [Exhibit III.A.1](#), the northern subdivision will be accessed at two locations via local roads that connect to

Success Drive, with the primary entrance being the westernmost access point. Entry monumentation featuring common materials and colors as both the residential and nonresidential uses within Vail Crossings will be provided at this entrance to signify a sense of arrival and establish a similar yet unique identity. See *Exhibit IV.E.1.a: Northern Development Area Entries and Theme Walls* for an illustrative example of the entry monumentation at this location.

To ensure uses are horizontally mixed, a new collector road with bicycle lanes, sidewalk, and trail connectivity, as depicted in the *Secondary Collector Road* cross-section and *Exhibit IV.F.i: Northern Development Area Circulation*, will connect the southern subdivision to Success Drive with nonresidential uses in between, located on one or both sides of the street. This allows for the co-mingling of multimodal residential and nonresidential traffic moving north-south through the site. Monumentation will be provided at the entry of the subdivision to signify a transition from the nonresidential areas to the residential community. The nonresidential area will also feature an entry monument at Success Drive.

Since the site is bisected with easements or natural areas that render portions of it unsuitable for vertical development, a variety of connected common areas are envisioned to be provided throughout the Northern Development Area. Not only does this allow for native plant material to be reintroduced on the site, but it also provides opportunities for recreation, and further

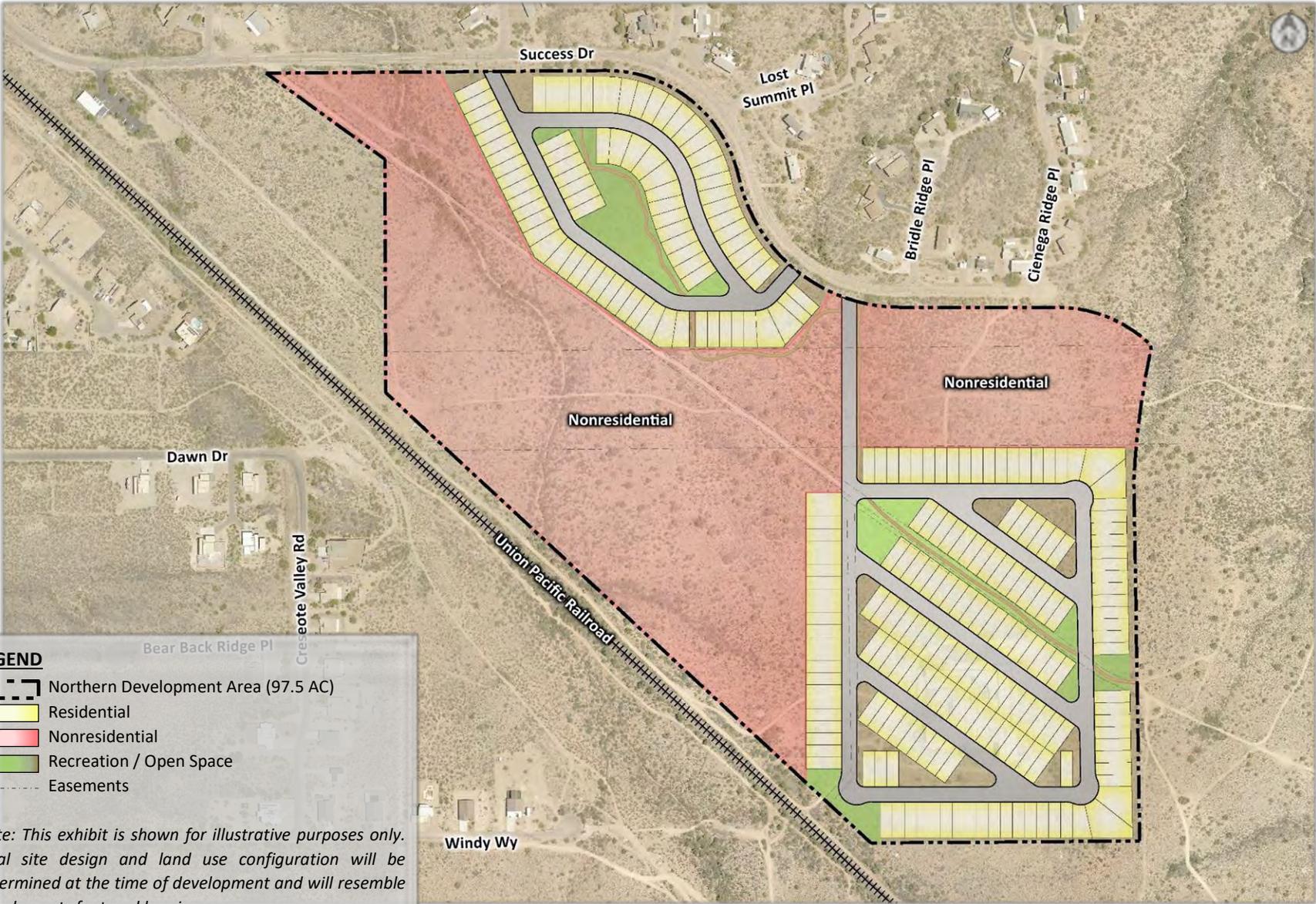
enhances the overall walkability of the community. Multimodal connections between the proposed residential communities and the nonresidential areas will be provided via collector roadways, pedestrian accessways/gates, sidewalks and trails along streets, and walking paths proposed within the communities. No buildings or other structures will be erected within the easements affecting Vail Crossings, yet use of them is expected throughout.

Landscaping within the Northern Development Area will be provided in accordance with Section 18.73 of the Pima County Code and will be utilized to beautify the site, frame and soften uses where needed, and enhance the visual aesthetic of the site and the surrounding natural environment.

In keeping with the 50/50 split required by the Mixed Use Option, 50% of the Northern Development Area is residential and 50% is nonresidential as shown in *Exhibit III.A.2: Northern Development Area Land Use Breakdown*.

Areas left as natural open space or acreage set aside for public utilities (i.e Vail Water Company facilities) shall be removed from the overall acreage of the development area and the 50/50 split shall be applied to the net acreage without such natural open space/utility. If serving as the recreational requirement for a particular land use, open space must be included as part of the overall acreage listed for that land use category.

Exhibit III.A.1: Northern Development Area Development Plan

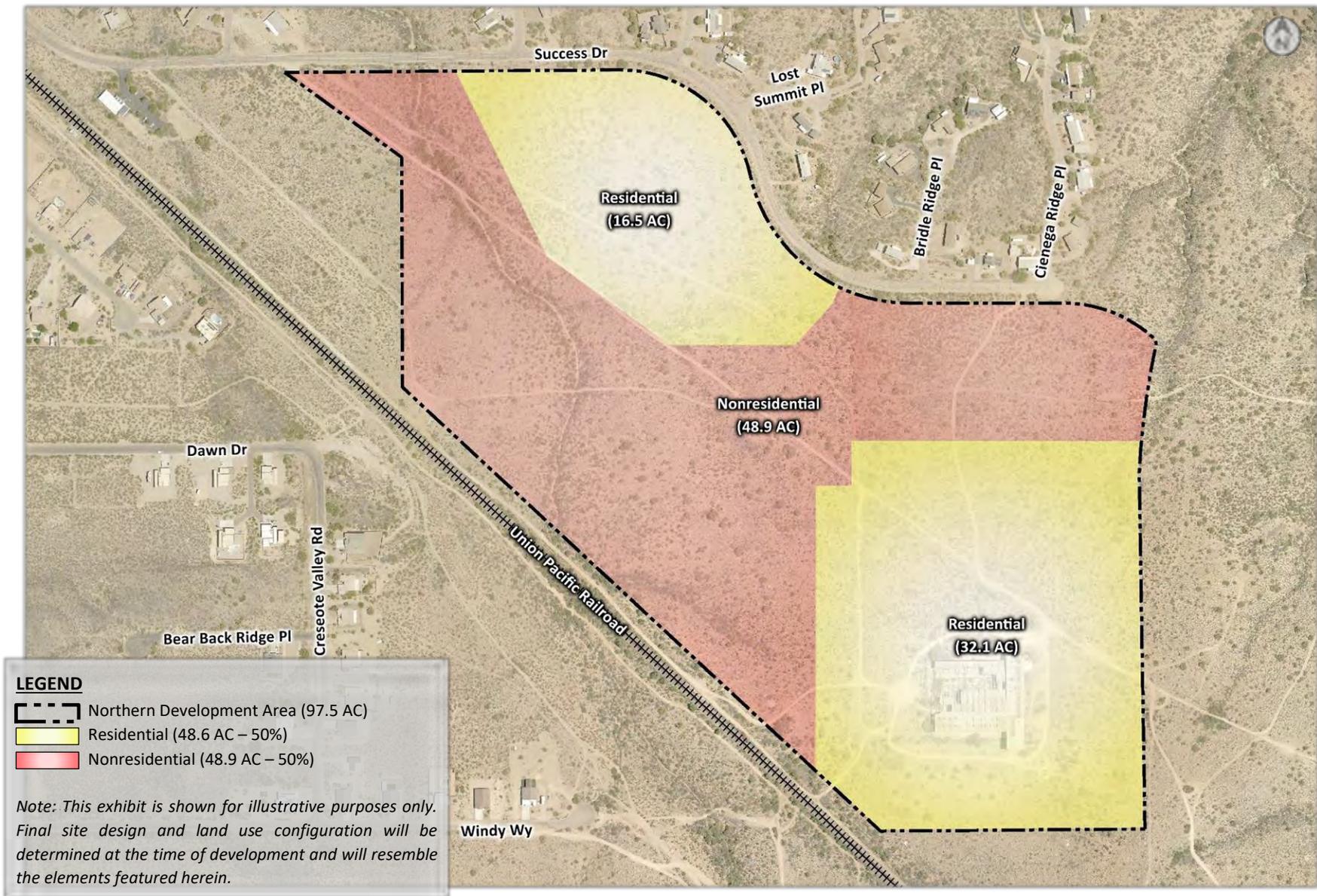


**LEGEND**

-  Northern Development Area (97.5 AC)
-  Residential
-  Nonresidential
-  Recreation / Open Space
-  Easements

*Note: This exhibit is shown for illustrative purposes only. Final site design and land use configuration will be determined at the time of development and will resemble the elements featured herein.*

Exhibit III.A.2: Northern Development Area Land Use Breakdown



## B. Southern Development Area Development Plan

Much like the Northern Development Area, the Southern Development Area is divided by easements, riparian areas, and informal roads, and is further broken up by existing development on-site and on outlying parcels in the middle of the development area. In addition to these constraints, the existing land uses and zoning designations adjacent to the site present compatibility considerations, yet opportunities exist to synchronize with and set the tone for the development of adjacent vacant parcels.

Recognizing that compatibility is a cornerstone of creating vibrant communities, the Southern Development Area positions similar development types next to one another and utilizes existing development, roadways (existing and proposed), and open spaces to transition between uses. Where appropriate, nonresidential buildings will be oriented away from residential uses and toward public streets, with logical site transitions (i.e., varied setbacks, landscaping, screening, parking area siting, etc.) to ensure compatibility, activate streets, and create a sense of arrival into the nonresidential areas. Colors, materials, theme walls, entry monumentation, and streetscapes will also help brand Vail Crossings as a distinct, unified community.

Due to anticipation of the State Land to the west and the vacant industrial park developing with nonresidential uses along the interstate, opportunities for logistics and warehousing, and other nonresidential uses that provide jobs and commercial services are anticipated to develop

along the western boundary and interstate frontage. As shown in *Exhibit III.B.1: Southern Development Area Development Plan*, these uses will likely span the west half of the development area with residential uses located on the east side with convenient bicycle and pedestrian interconnectivity. Since the exact residential housing layout and mix has not been determined yet, the single-family residential depicted in *Exhibit III.B.1* could develop as multifamily housing driven by market demand.

Three subdivisions consistent with the TR (Transitional) Zone are envisioned within the Southern Development Area to complement and buffer the adjacent residentially zoned properties from nonresidential uses proposed within the development area. Each subdivision will be accessed from one of two primary collector roads. The main collector road that serves all three subdivisions connects the I-10 Frontage Road to existing development to the north, with a roundabout near the middle of it. See *Primary Collector Road* cross-section and *Exhibit IV.F.ii: Northern*



*Development Area Circulation.* Identity monumentation on this street will be combined with commercial signage featuring common materials and color palettes as both the residential and nonresidential uses within Vail Crossings. The entry feature will be designed to signify a sense of arrival and establish a similar yet unique identity for residents, businesses, and patrons. The major entry on the other primary collector street east of the Vail School District Office will feature the same design aesthetic, but at a larger scale.

To ensure uses are horizontally mixed, a secondary collector road with a neighborhood entry monument at the roundabout near the middle of it bisects the two larger subdivisions. This will allow for the co-mingling of residential and nonresidential design themes moving along the collector road corridors within Vail Crossings. Neighborhood monumentation will be provided at the entry of each subdivision to signify a transition from the nonresidential areas to the residential community. Like the Northern Development Area, lots within the Southern Development Area are intended to be sized to provide opportunities for first-time homebuyers, working professionals, and families, and may consist of one- and two-story homes.

The Southern Development Area is bisected by a significant watershed and easements, providing opportunities for connected open spaces via multiple types of pedestrian travel. Not only does this allow for native plant material to be reintroduced on the site, but it

also provides opportunities for recreation and further enhances the overall walkability of the community. Multimodal connections between the proposed residential communities and the nonresidential areas will be provided via collector roadways, pedestrian accessways/gates, sidewalks and trails along streets, and walking paths proposed within and around the communities.

Landscaping within the Southern Development Area will be provided in accordance with Section 18.73 of the Pima County Code and will be utilized to beautify the site, frame, and soften uses where needed, and enhance the visual aesthetic of the site and the surrounding natural environment.

In keeping with the 50/50 split required by the Mixed Use Option, 50% of the Southern Development Area is residential and 50% is nonresidential as shown in *Exhibit III.B.2: Southern Development Area Land Use Breakdown*.

Areas left as natural open space or acreage set aside for public utilities (i.e Vail Water Company facilities) shall be removed from the overall acreage of the development area and the 50/50 split shall be applied to the net acreage without such natural open space/utility. If serving as the recreational requirement for a particular land use, open space must be included as part of the overall acreage listed for that land use category.

Exhibit III.B.1: Southern Development Area Development Plan

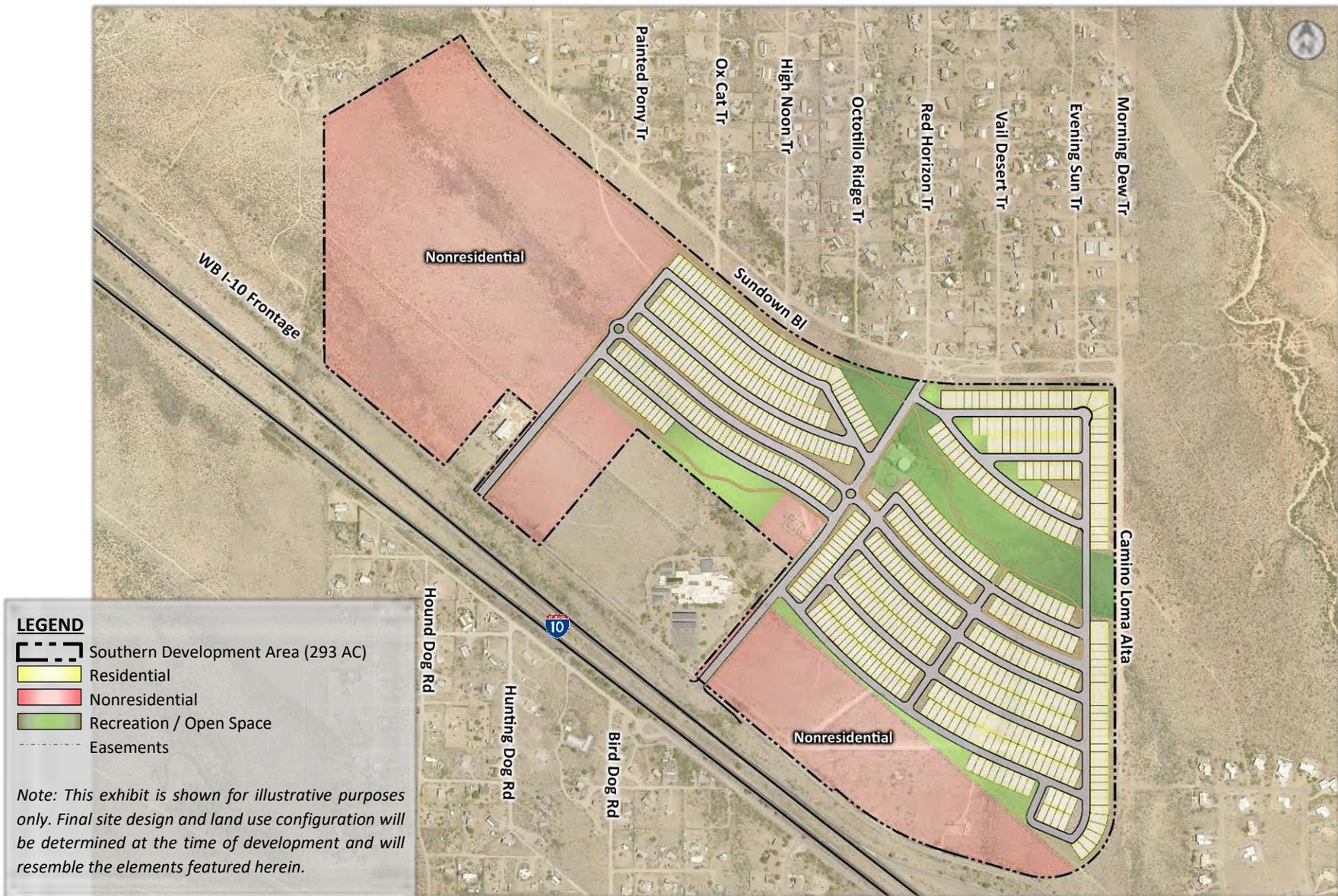
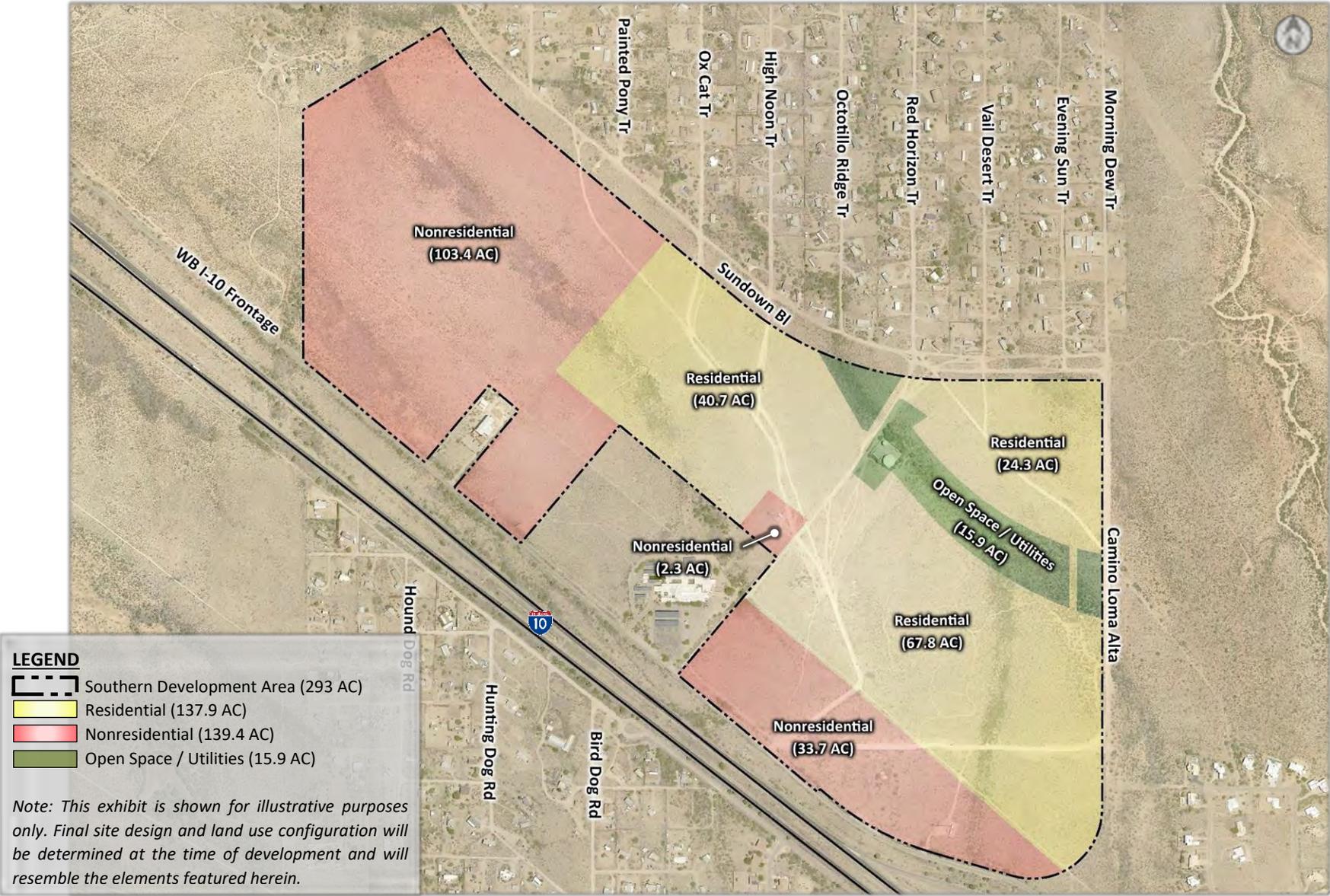


Exhibit III.B.1: Southern Development Area Land Use Breakdown





**VAIL CROSSINGS**  
PART IV - DESIGN STANDARDS

## IV. DESIGN STANDARDS

### A. Introduction

#### 1. Purpose and Intent

Vail Crossings features many aspects that are distinct to the southern Arizona Sonoran Desert. The proposed development will take advantage of the natural beauty, open space, washes, wildlife, and scenery unique to this environment. These design standards are crafted to serve as a tool to develop the community in a cohesive, and aesthetically and environmentally pleasing manner.



#### 2. Design Goals

The Vail Crossings Design Standards are an information source for site development, architecture, landscape architecture, art, and monumentation/signage. They are intended to create a cohesive and celebratory local identity and marketable community image. These standards will guide builder's, planners', and architects' conceptual design processes under the review and evaluation of the Master Developer and developer's consultants. They also provide a foundation for development of more formalized Covenants, Conditions & Restrictions (CC&R's). The integration of these standards into CC&R's will guarantee quality design consistency throughout all development phases of this project.

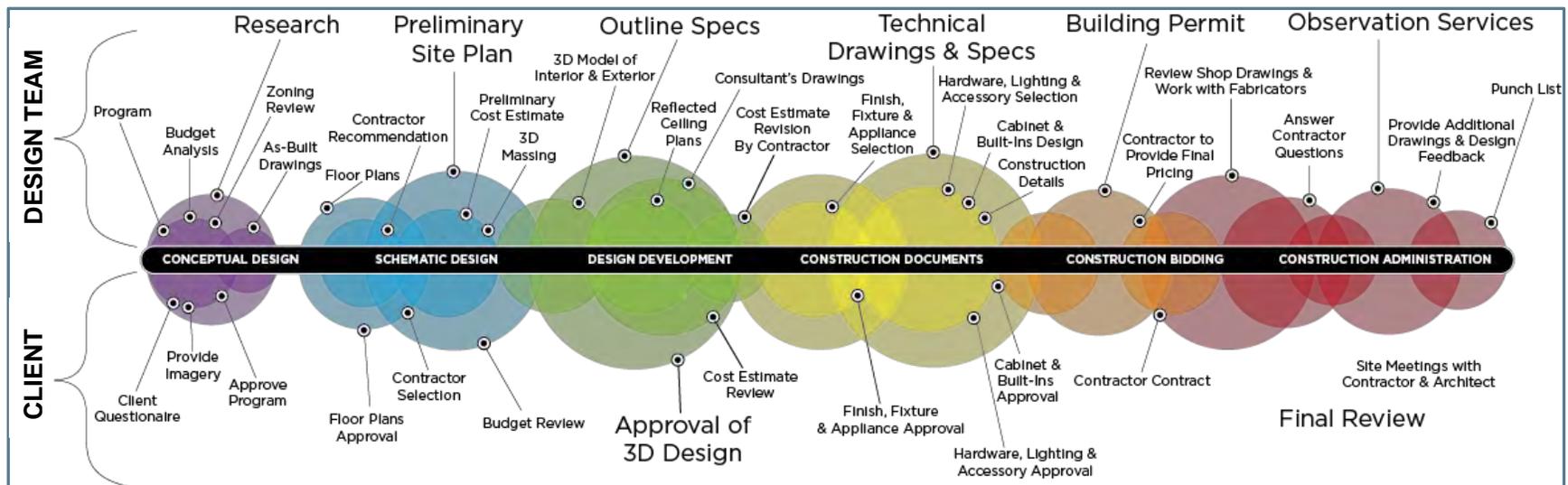


The following are the design goals for the Vail Crossings Design Standards:

- Create a community that emphasizes both "design excellence" and "attainability" in the Tucson region.
- Create a mixed-use community with integrated residential neighborhoods and direct pedestrian access to nonresidential land uses.

- Integrate wash systems, riparian areas, and utility easements with the developed portions of the site to provide open space and recreational opportunities, preserve views, and celebrate the unique desert environment.
- Establish paths and trails throughout open space corridors to enhance pedestrian connectivity throughout the site. Establish multi-modal streetscapes that encourage safe and comfortable pedestrian activity.
- Establish consistent design aesthetics that help to build a marketable community character.
- Ensure compatibility of new development with the environment, both natural and built.

The technical sections that follow will give specific standards. This section offers general outlined requirements that are intended to promote the overall design quality of the architectural development at Vail Crossings. Aspects of scale, proportion, and detail shall be considered from the beginning of the design process. All new standards, designs, and plans shall be reviewed by the Master Developer and Pima County to ensure conformance with these standards.



*Comprehensive diagram of the design process*

## B. General Site Design Standards

The Vail Crossings site is intended to accommodate residential uses, nonresidential uses, and open space. Due to its location, it is anticipated that the residential portions of this project will begin development prior to nonresidential uses. As such, the residential neighborhoods are anticipated to be located in highly visible and accessible portions of the site along existing major vehicular corridors. Nonresidential uses for this project are not yet determined but are also expected to be highly visible and easily accessible from public streets. While neighborhood and village commercial uses are encouraged, due to Vail Crossings' proximity to the interstate and railroad, it is also conducive to light industrial uses such as manufacturing, distribution, and logistics activities. Residential and nonresidential uses are intended to be highly linked via pedestrian and bicycle corridors, so that community residents can easily access employment opportunities. Open space will also link distinct uses throughout the project and accommodate paths and trails.

The following design techniques will be considered during the design of residential and nonresidential properties within Vail Crossings:

- Locate primary building entrances on the street with secondary entrances facing parking, when feasible, in nonresidential areas.
- Primary building entrances shall front major pedestrian access-ways. Primary building entrances shall feature enhanced architectural

detailing, lighting, signage, site furnishings, and landscape to assist with wayfinding.

- Nonresidential buildings shall feature four-sided architecture that incorporates colors and/or architectural detailing resembling the character and style of the front **façade's design**. This could include but is not limited to the use of the same or similar earth-tone color palettes and materials, and architectural embellishments like colored stucco, arched windows, stone veneer columns, roof overhangs, etc. on the sides and rear of the building.
- Nonresidential buildings shall be sited adjacent to right-of-way, where practical. All parking lots adjacent to right-of-way shall be screened with a 10-foot wide landscape border.



- Spaces between buildings and between land uses will be designed as attractive, safe, pleasant linkages.
- If several buildings are proposed for a site, the spaces between the buildings will contribute to the outdoor use and the open space enjoyment of the area by incorporating seating, landscape, and gathering space.
- Subdivision plats shall provide community open space/pocket parks to provide both physical and visual connection to and activity along the trails.
- Nonresidential areas shall encourage outdoor dining opportunities in close proximity to major vehicular and pedestrian circulation to emphasize liveliness and activity and attract interest from residents.





## C. Residential Architectural Design Standards

The following design standards apply to the residential portions of Vail Crossings. Residences will vary in size and affordability to provide homes for a diverse range of incomes. Residential areas within Vail Crossings shall adhere to the TR (Transitional) zone development standards, and support a mix of single-family homes or multi-family units with live-work opportunities.

### 1. Residential Architectural Style

The architectural style within the residential portions of the project may feature contemporary versions of mission, Spanish colonial, craftsman, or territorial design aesthetics consistent with similar products in the county. Modern architecture is acceptable if exterior construction materials, cladding, and rooftops consist predominantly of traditional/vernacular building materials. These materials include, yet are not limited to stucco, integral color concrete block, stone, metal, adobe, brick, and faux materials resembling the former. See *Exhibit IV.C.1: Residential Architectural Styles* for examples of styles anticipated to be featured in Vail Crossings.

The architectural design elements for Vail Crossings may derive their distinctive character from:

- Sand Finish or Medium Dash Finish Stucco
- Appearance of "Thick" Walls
- Courtyards and Intimate Spaces
- Offset Wall Planes
- Varied Building Masses and Architectural Details
- Building colors as a primary theme conveying element
- Articulation of simple, box-like architecture.

Exhibit IV.C.1: Residential Architectural Styles





## 2. Residential Architectural Criteria

### a. *Building Form and Massing*

Variety in building form must provide diversity and visual interest to the neighborhood street scene. The form of each house shall create a visually attractive exterior that is consistent with the **building's inherent architectural style**. Building form shall also reflect the human scale; thus, the visual appearance of residential massing to provide the maximum level of interest at the scale and speed of a pedestrian. Therefore, building form should emphasize **the home's front entrance and de-emphasize the garage**. The articulation of wall planes, particularly on the front elevation, may be staggered to create interest along the street scene and avoid visual monotony. Building forms can be simple and bold. The residential street scene should support a mix of one- and two-story building forms.



### b. *Corner Lots*

Homes on corner lots have a high degree of visibility and impact on the community image. Plotting of homes on corner lots shall be done sensitively. Residences along corner lots shall extend architectural detailing/ornamentation from the front of the house to the side of the house. The articulation of architectural elements along the corner lot side yard shall be comparable to front yard building articulation. These elements may include wrap-around porches or courtyards and accent colors/materials/detailing. Side yard fences shall begin at least as far back from the street frontage as the back of the house. Exceptions to this rule include providing security to bedroom and bathroom windows by placing them behind side yard walls where necessary.

### *c. Community Interior Edges*

Side yards outside of a residential plat and in the community common areas shall be landscaped consistently with other areas within the community or planted and irrigated to blend cohesively within the neighborhood.

### *d. Community Perimeter Edges*

Residential architecture shall not only provide interest internally (within individual neighborhoods or subdivisions), yet address views and connections from neighboring developments, adjacent open space, and surrounding streetscapes. Perimeter lots (those having a side or rear building elevation exposed to a community perimeter edge) define community image for residents and passersby alike. Residences on lots that are located on the perimeter edges of development parcels and visible from open space shall be designed to avoid a monotonous edge condition. Thus, residential architecture on perimeter lots shall comply with the following guidelines: 1) roof types (gable, cross gable, hip, mansard, shed, flat, etc.) shall be varied across the perimeter edge; 2) homes with rear and/or side edge conditions shall provide architectural elements/details that are consistent with the front of the home and inherent style of architecture (window shutters, decorative window frames, roof articulation, wall articulation, and accent colors/materials, etc.); and 3) two-story homes on perimeter lots shall provide architectural elements/details that accentuate the first level of the building (covered patios, accent colors/materials). No more than three adjacent residences shall feature the same elevation.

### *e. Garages*

The character of a community is predominantly defined by the street scene experience. De-emphasizing the visual impact of





garages allows the active architecture and style to permeate the street scene and reinforce the community identity. This also allows the active, visually interesting features of the house to influence the streetscape. To de-emphasize the visual impacts, residential garages should be positioned to establish a varied streetscape, and may include but are not limited to, sideloaded garages, tandem garages, standard front-entry garages, recessed front-loaded garages, and offset garages.



Setback distances for side loaded garages should also vary along the streetscape. No more than three adjacent lots with front or side loaded garages will feature the same distance garage setback. Side loaded garages must be at least 10 feet from the front property line. Side loaded garages must feature windows and may feature additional architectural details along garage walls that face the right-of-way (i.e. shutters, accent materials, wall articulation, etc.). A minimum of two garage door types with differing colors, styles, or materials shall be provided in each subdivision to create variety along the streetscape.



The scale and proportion of garage doors should correspond with the architectural style of the home. Ornamental doors should be included in the basic mix for doors at shortened driveways. Basic/plain two-car garage doors that fail to reflect the appropriate scale, proportion, and detailing of a residential dwelling are not appropriate for front loaded garages. Additional patterning and detailing are encouraged on single door two car garages with standard front entry. Garage doors on standard front entry garages shall feature detailing appropriate with the style of the home (decorative garage door surrounds, windows, hinges, handles, decorative paneling, and accent colors/materials, or similar design features).

#### *f. Driveways*

All driveways that are intended to accommodate vehicles must be 20 feet, or greater in length. Driveways not intended to accommodate vehicles must be 7 feet or less, from the back of the right-of-way to garage doors parallel to the street. Front entry driveways that exceed 20 feet in width along the street are

encouraged to feature hardscape enhancements. Enhancement options include: exposed aggregate, integral color, contrasting textures, decorative scoring, paving blocks, bands, or other similar treatments to break up expansive areas of pavement.

### *g. Roofs*

Variety of roof design and treatment is encouraged to provide visual interest to the neighborhood roofscape throughout Vail Crossings, including the use of gable, cross-gable, hip, flat, or a combination of these roof forms.

Roof styles will be consistent with the selected architectural styles. Roof slopes should complement the building mass. Simple, short roof overhangs are appropriate for Spanish Colonial and modern architectural styles. Flat roofs with parapets are suitable for mission and territorial architecture.

Roof slopes shall be consistent with the architectural style of the residence. Roofs with slopes of 2:12 minimum and 6:12 maximum shall be made of, but not limited to: clay, concrete tile or slate and must be either flat, barrel, or "S" shaped. Higher quality shingle roofing may be considered in select areas. Broken roof pitches extending over porches, patios or other similar features are encouraged where appropriate. A variety in roof lines is required to avoid a common roof line along neighborhood streets. Roof lines of adjoining residences are required to vary ridge heights, roof forms, and direction of gables. Repetitious gable ends framed side to side on rear elevations are not permitted along perimeter edges of residential neighborhoods.

A variety of roof materials are encouraged throughout the neighborhoods of Vail Crossings in order to avoid a monotonous roofscape appearance. Roof materials shall be compatible with the architectural style of the residence as indicated on the architectural checklist for each style.

#### Permitted Materials:

- Concrete or Clay Barrel & "S" Tiles
- Concrete Flat Tiles
- Concrete Shake
- Rolled roofing material (Parapet flat roof only)



- Standing Seam Metal
- Higher quality shingle (in select areas)
- Builders should offer an option for roof photovoltaic energy systems and/or solar hot water as a feature on new homes.

#### Prohibited Materials:

- Asphalt and Wood Shingles

#### *h. Porches, Patios and Balconies*

Front porches, rear patios and balconies are prominent elements of architectural composition that also provide a functional outdoor living area protected from extreme sun or rain. All porches, patios, and balconies shall be designed to complement the architectural style of the house. Porches that provide variety to the exterior appearance of the house are encouraged. Options for homes with front porches and/or low-walled front courtyards are encouraged.

#### *i. Doors and Windows*

Door and window openings within exterior wall surfaces shall be treated with special consideration to the visual appearance of the house. The use of architectural pop-outs, trim surrounds on all sides, and recessed windows into thick walls is encouraged to accentuate the architectural style of the house. Windows may be either divided light, or plain glass. Glazing may be either clear or tinted in a color appropriate to the style. Reflective glass

is not permitted, dual pane glass is required as a minimum. Metal, vinyl, or wood clad windows are permitted.

Window and door design should relate to the selected architectural style. Windows and doorways in southwestern architecture typically have rectangular, round or arched openings, and are either recessed to reflect the thick, protective exterior walls typical of the Spanish Colonial style, Mediterranean or Sonoran styles, or set flush with the wall surface as in Barrio or more modern architecture.



### j. Interior Fixtures

Interior fixtures should limit wasted resources as required. The following interior fixtures are encouraged:

- Low-flow flushing toilets
- Refrigerators with an in-door filtered water system
- Highly efficient thermal insulation
- Solar shades or similar products



### k. Chimneys

The design and material finish of chimneys shall complement the design character of the house. Flue covers are required.



### l. Lighting

Lighting sources are to be shielded, diffused or indirect to avoid glare to pedestrians and motorists, where practical. Architectural lighting is to be used to highlight special features. Lighting of expansive wall planes or the use of architectural lighting that results in hot spots on walls or roof planes shall be avoided. The lighting of address plaques is required. All external lighting must meet the Pima County Outdoor Lighting Code requirements.



### m. Accessory Structures

Accessory structures include any building or structure which is completely detached from the

house. Examples of these include, but are not limited to:

- Equipment enclosures
- Shade ramadas
- Cabanas
- Guest casitas
- Detached garages

The architecture of accessory structures must be compatible with and complementary to the architecture of the main house. All detached buildings used as living space shall conform to the design standards of the house.

### n. Colors

Exterior building colors shall reflect the architectural style of the home and the desert environment. Color variety within neighborhoods helps create unique character and is encouraged. Desert earth tones are acceptable (i.e. sandy browns, sandy golds, sandy reds, sandy creams, beige, etc.). Refer to *Exhibit IV.E.C.2.n: Residential Color Palette* for a non-exhaustive list of anticipated colors to be used for residential homes. Each home within a subdivision shall feature one base color and at least two accents (color, veneer or faux shutters) No more than two homes in a row shall feature the same palette of colors.

Exhibit IV.C.2.n: Residential Color Palette



## *o. Materials*

Materials for exterior building walls shall complement the architectural style selected for the house. Where more than one building material is proposed for the exterior walls, careful consideration shall be given to the successful integration of the different material types and individual character of the house. Large expanses of wall surfaces shall be designed with the use of varied materials. If varied materials are not used, then large expanses of wall surfaces shall be designed to feature detailed set backs or projections, color wrapping or additional windows.

A random, sand or hand troweled stucco look is typical. No patterned stucco. Accent materials and colors used to complement the stucco are encouraged in moderation. Regional stone materials may also be used for facing.

Recommended primary building materials include:

- Concrete or Adobe Block
- Cement Stucco over Concrete Block or Wood Frame
- Slump Block
- Minimal use of Natural Stone as accent.

Prohibited primary building materials include:

- Vinyl Siding

Recommended accent building materials include:

- Adobe and Fired Adobe Brick
- Mud Stucco over Stabilized Adobe Block
- Split Face Concrete Block
- Brick and Mortar Washed Brick
- Faux stone which resembles regional natural stone.

Additional material guidelines include:

- All exposed building materials must be finished



- All exposed wood is to be treated with a preservative, stain, or painted
- No highly reflective building materials shall be used.



### *p. Hardscape*

Hardscape layout and material selection in the front yard shall reflect the style and character of the residential architecture. Entrance patios and low-walled courtyards are encouraged within the landscape to provide functional outdoor gathering space where feasible and suitable to the design style of the architecture. Courtyard walls must be low enough to not obstruct views of the residence. Walkways that connect the front of the home directly with the adjacent sidewalk are encouraged. Walkways may connect directly to the driveway, rather than the adjacent sidewalk.



The use of accent colors / materials in the landscape that draw visual interest toward the front entry, and away from driveways and garage doors, is encouraged. Enhancement options, including but not limited to integral colored or stained concrete paving, brick / concrete pavers, decorative finishes and veneer, landscape boulders, and seating are encouraged. Decorative rock mulch shall be required in every yard. All decorative rock mulch throughout a neighborhood or block shall be consistent. Hardscape elements in the landscape are encouraged to drain toward vegetative catchment areas. Permeable hardscape materials are also encouraged within walkways, patios, and courtyards.



### *q. Builder Walls*

Builder walls are interior to projects that are not community walls. Builder walls and fences shall be consistent in color and material to the architectural style selected for the community. Long expanses of builder walls must be interrupted by the use of freeform curves, offsets, piers, pilasters or material changes. The area in front of walls should be visually softened by irrigated and planted landscape appropriate to the landscape standards provided. 4-inch thick concrete block walls are permitted as dividing walls for lots (rear and side). Block walls along major collector or arterial streets shall be no less than 6-inch thick in width. Integral colored block shall be used for all block walls.

Builder walls along open space or view areas may include wrought iron pickets or similar features to enhance viewing opportunities and open spaces.



### *r. Utilities and Equipment*

Utility and gas meters shall be screened from view to the extent possible. All residential air conditioning/heating equipment, soft water tanks, pool and spa equipment, irrigation timers, and sewage clean-outs should be screened through either built elements such as screen walls of architectural extensions, screened with landscape materials, or masked by aggregate. Equipment must meet utility company standards for access. All new electric, telephone and cables shall be installed underground and shall be designed and constructed in accordance with county code.



## D. Nonresidential Architectural Design Standards

The following standards apply to the nonresidential areas of Vail Crossings. Diversity of architectural design shall be encouraged within the nonresidential areas with the allowance of vendors' specific branding elements and design features. Nonresidential structures should be compatible with surrounding neighborhoods by using similar textures, colors, and materials. Design review shall consider the visual impact of taller buildings in relation to other existing and proposed buildings, with the goal of creating a varied skyline and view corridors throughout the commercial development.

### 1. Nonresidential Architectural Style

All buildings within the nonresidential areas shall feature a fashionable architectural style. Buildings within nonresidential areas should be consistent with one another through the use of 'in-kind' architectural detailing/accents, materials, and colors. While the architectural style of the nonresidential buildings will differ from that of the residential buildings, the materials and color palettes should act to reinforce the character of the project.

The visibility of roof-top equipment will not be allowed, and all plumbing vents, ducts, and roof-top mechanical equipment will be screened from the public view.

The first nonresidential building will establish the precedent that all future phases of the project will emulate. Form, materials, and colors shall harmonize across all nonresidential buildings throughout the project site.

## 2. Nonresidential Architectural Criteria

### *a. Building Form and Massing*

Nonresidential buildings will be the largest structures in Vail Crossings. Building form should be simple and bold. Buildings are to be oriented to maintain any view corridors and toward the direction of primary circulatory routes for pedestrians, cyclists, and vehicles. Connectivity to both pedestrian and bicycle pathways shall be integrated into the design of nonresidential development. Building heights shall be designed for a comfortable pedestrian experience. Building design shall incorporate textured surfaces, projections, recesses, shadow lines, color, window patterns, overhangs, reveals, or changes in parapet height to avoid monolithic shapes and surfaces.

### *b. Corner Buildings*

The front facade of a building is expected to be the focal point with architectural detail and features, yet all sides of the building are to incorporate architectural detailing which resembles the character and style of the front façade's design. Corner buildings are encouraged to have primary entrances that face the intersection, where feasible.

### *c. Perimeter Facing Buildings*

Where practical, all walls and roof lines visible from arterial, collector, and other public rights-of-way should have architectural detailing to create interesting auto corridors. Also, building facades shall be articulated and roof styles shall be varied.

### *d. Loading Docks and Service Areas*

Loading docks and service area (refuse areas) should not impede access and should be oriented away from the nearest residential uses where possible. Loading docks shall be designed per Pima County standards.





### *e. Roofs*

A variety of roof lines, roof pitches (limited in use), parapet heights or accents shall be provided. Screening of all rooftop or pad mounted mechanical units is required. It is encouraged that rooftop screening be accomplished by an extended parapet or freestanding rooftop screen(s) that complement the architectural façade design.

The design of the roof top and gutter systems shall be in-wall types and should consider water harvesting applications, either through passive drainage into catchment basins or active collection into cisterns.

### *f. Colors*

Nonresidential color palettes are not to contrast against the residential color palettes to the greatest extent feasible. Nonresidential buildings shall feature one base color and at least two accents (colors, veneer, furring, awnings, etc.). Nonresidential users may use corporate color prototypes if desired.

### *g. Materials*

Detailing and accent materials are required to add interest. Appropriate accent materials include; stucco, stone, cast stone, precast concrete, wrought iron, steel, glass, etc. The use of vertical elements such as pilasters, pillars, columns, and other monumentation are encouraged. Integration of wood or faux wood awnings, flat metal awnings, and trellises is encouraged to create interest and inviting building entries. Use of raised planters, potted plants and incorporation of landscape softens the impact and is encouraged.

#### *h. Doors and Windows*

Doors and windows will be appropriately located for the desired use and ease of circulation. When designed by an architect, awnings and overhang features above building fenestration shall provide shade along western, southern, and eastern exposures.

#### *i. Lighting*

Varieties of wall-mounted light fixtures that fit the proposed architectural styles are encouraged. Lighting sconces are encouraged and can be used to break up blank walls, adding interest to the architecture. All lighting is to be engineered to minimize light pollution to the nearby residences.

#### *j. Site Walls*

Site walls to screen trash and other service locations must be designed to fit the architectural style of the surrounding commercial buildings. All walls must be durable and low maintenance. Vegetative screening is encouraged along commercial site walls to soften the hardscape and mitigate reflective heat.



#### *k. Parking*

The orientation and layout of retail buildings within the nonresidential areas shall encourage shared parking. Parking lots are to harvest water to catchment basins and landscape beds, as required. Parking areas shall incorporate vegetation as required to help mitigate for the urban heat island effect. Shade canopies and structures, including solar parking structures, are encouraged. Depressed basins are encouraged to be installed in the parking to capture and harvest rainwater.

Large nonresidential parking lots without a retail component should be located toward the rear of buildings and located away from collector and arterial streets where practical.

---

Parking lots may reflect parking reductions through an Individual Parking Reduction Plan in accordance with Pima County standards.

## E. Community Entries, Signage, Theme Walls, and Public Art Design Standards

### 1. Entries and Theme Walls

Vail Crossings contains a series of entry features with an integrated design of softscape, hardscape, monumentation, and signage. Entry features aid in defining the community's character and present an introduction to the development. Entries serve as gateways and thresholds. In addition to monuments and signs, hardscape, functional public art, and landscape lighting are encouraged along entries. Vegetation is to be bold, orderly, and large in scale. The species, layout, and hierarchy of plant materials at entries shall complement the landscape design along streetscapes; however, entries may showcase unique specimen plantings.

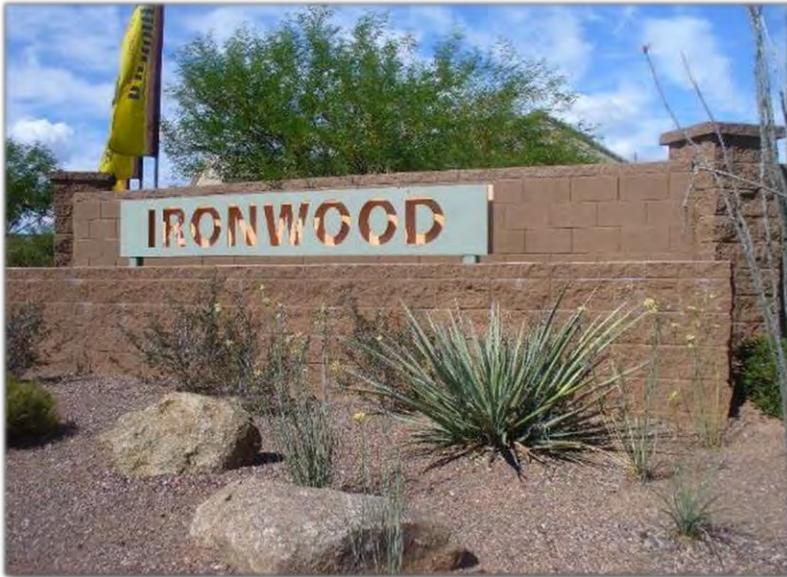
#### a. Major Entry

Vail Crossings will feature one major entry in the Southern Development Area. The major entry should feature monumentation constructed to a vehicular scale to define a point of entry. The monuments, seen from a distance, provide vertical scale, backdrop for the project name, and logo. They signify arrival and establish the theme of the development in addition to serving as wayfinding elements. The major entry feature for Vail Crossings, which may include wall, signage, landscaping, and pavement treatments, will be designed and installed by the Master Developer.

#### Major Site Entry Features:

- 1 Monument
- Decorative hardscape paving at crosswalks (concrete pavers, stamped asphalt, decorative stamped concrete, etc.)
- Enhanced landscape featuring native vegetation and desert-adapted plant species
- Decorative rock mulch and boulders or similar





*b. Secondary Entry*

Vail Crossings will feature one secondary entry in each development area. The secondary entry will be less prominent than the major entry yet should feature monumentation constructed to a pedestrian scale to define a point of entry. The secondary entry will utilize similar color, textures, plant materials, etc. as those featured in the major entry. The secondary entry features for Vail Crossings, which may include wall, signage, landscaping, and pavement treatments, will be designed and installed by the Master Developer.

*c. Neighborhood Entries*

Neighborhood entry features identify the entrances into the individual subdivisions within Vail Crossings. Each entry into a subdivision shall be uniquely designed with common elements that correspond with the overall Vail Crossings community character. Neighborhood entry features will be designed and constructed by the Builder. Entry monumentation shall be reviewed and approved by the Master Developer. The final location and number of neighborhood entry features will be determined at the time of development. Gated neighborhood entrances are generally prohibited; however, lifestyle neighborhoods may feature gates. Lifestyle neighborhoods emphasize common amenities and cater to a leisure class with shared interests (i.e. retirement, recreation, assisted living, etc.). Gates must not impede access to the streetscape paths, trails, inter-block paths, neighborhood parks, or community park.



*d. Nonresidential Entries*

Nonresidential entry features identify the entrances into commercial or industrial developments and may consist of single-tenant or multi-tenant entry features, pylon signs or freeway signs. Each entry into nonresidential areas shall be scaled appropriately and uniquely designed with common elements (colors, materials, textures) that correspond with the overall Vail Crossings community character. Nonresidential features will be designed and constructed by the Developer of each nonresidential development. Entry monumentation shall be reviewed and approved by the Master Developer. While the final location and number of nonresidential entry features will be determined at the time of development, it is anticipated that there will be up to ten nonresidential entries within Vail Crossings, two of which are freeway signs.

*e. Landscape Treatment at Monuments*

Monument entries will be defined through special landscape treatments to emphasize and contrast it with the surrounding environment. The landscape and hardscape components create symbolic gateways with architectural elements and a unique massing of plant materials. The entry plant palette will provide an aesthetically appealing yet appropriate contrast to the natural environment and remainder of Vail Crossings landscape design.

See the Vail Crossings Specimen Plant list for more information.





#### f. Theme Walls

Theme walls are decorative masonry block walls (must feature integrally colored block and decorative finish, or stucco). Theme walls shall be used in place of standard screen walls or perimeter walls, along all high-visibility corridors.

Theme walls shall feature two of any of the following decorative enhancement options:

- Two block types of distinct size and finish throughout
- Two distinct integral colors throughout
- Stucco Finish
- Decorative accent wall cap
- Decorative wall column (featuring an accent material) (every 40 feet – 50 feet approx.)
- Horizontal wall articulation (every 40 feet – 50 feet approx.)
- Block type or color varied wall segments (every 40 feet – 50 feet approx.)



The first constructed theme wall will establish the precedent that all future phases of the project will emulate. Forms, materials, and colors shall complement all theme walls throughout the project site.



## 2. Site Signage

Site signage must feature consistent forms, colors, materials, lighting, and logos throughout the project. Site signage is permanent and shall be constructed of durable and lasting materials, common to monumentation throughout the Tucson metropolitan region. Acceptable sign materials include: concrete, aluminum, steel, acrylic, composite resins/plastics, weathered or corten steel, etc. See *Exhibit IV.E.2: Examples of Site Signage* for sample imagery.

The first constructed sign will establish the precedent that all future phases of the project will emulate. All forms, materials, and colors of signs shall not contradict the design features of the development.

Any commercially used strings or pennants, banners or streamers, cluster of flags, strings of twirlers or propellers, flares, balloons and similar attention-getting devices, may be allowed per leasing agreements and terms and shall be temporary in purpose.

Where permitted by the County, Builder implemented pennants, banners, balloons, or flags used in conjunction with residential subdivision sales offices and tract entry points are acceptable.

The following signs shall be restricted in all land use designations within Vail Crossings:

- Inflatable signs
- Rooftop signs
- Signs on trailers or painted on the sides of disabled or parked vehicles
- Rotating, revolving or flashing signs
- Signs advertising or displaying any unlawful act, business or purpose. Any sign, notice or advertisement affixed to any street right-of-way, public sidewalk, crosswalk, curb, lamp post, hydrant, tree, telephone pole, lighting system, or upon any fixture of the fire or police alarm system of Pima County.
- Any holiday decorations shall be regulated by the Vail Crossings Master Association or sub-association through project CC&R's.



Exhibit IV.E.2: Examples of Site Signage





### 3. Public Art

Public art shall be featured within the project site in areas of high visibility and high use to facilitate the creation of a sense of place. Public art shall be implemented to accentuate the project theme and establish identity, all the while resembling the surrounding environment. Refer to *Exhibit IV.E.4: Illustrative Public Art Imagery* for examples of how art work is intended to be integrated into Vail Crossings.

#### Additional areas of artistic enhancement:

- Decorative Building Materials or Artwork at Neighborhood Entries
  - Shall include two of the following (not limited to): themed signage, painted mural, tile mosaic, decorative accent features, or sculpture.
- Decorative Building Materials or Artwork within features at the Community Park
  - Shall include two of the following (not limited to): painted mural, tile mosaic, decorative walls, sculpture, etched/sand blasted hardscape with desert motif, botanical signage, custom seating/site furnishings, or sculpture.
- Ornamental Utility Infrastructure
  - Developer to coordinate with utility providers to enhance the aesthetic of existing utility infrastructure, may include: decorative screening, painted murals, and enhanced landscape.
- Wayfinding and Signage
  - May include: themed signage, recreation areas, street signs and focus areas.
- Specimen Landscape Gardens
  - May include: contemporary plantings and highly formal and heavily manicured desert adapted landscapes.

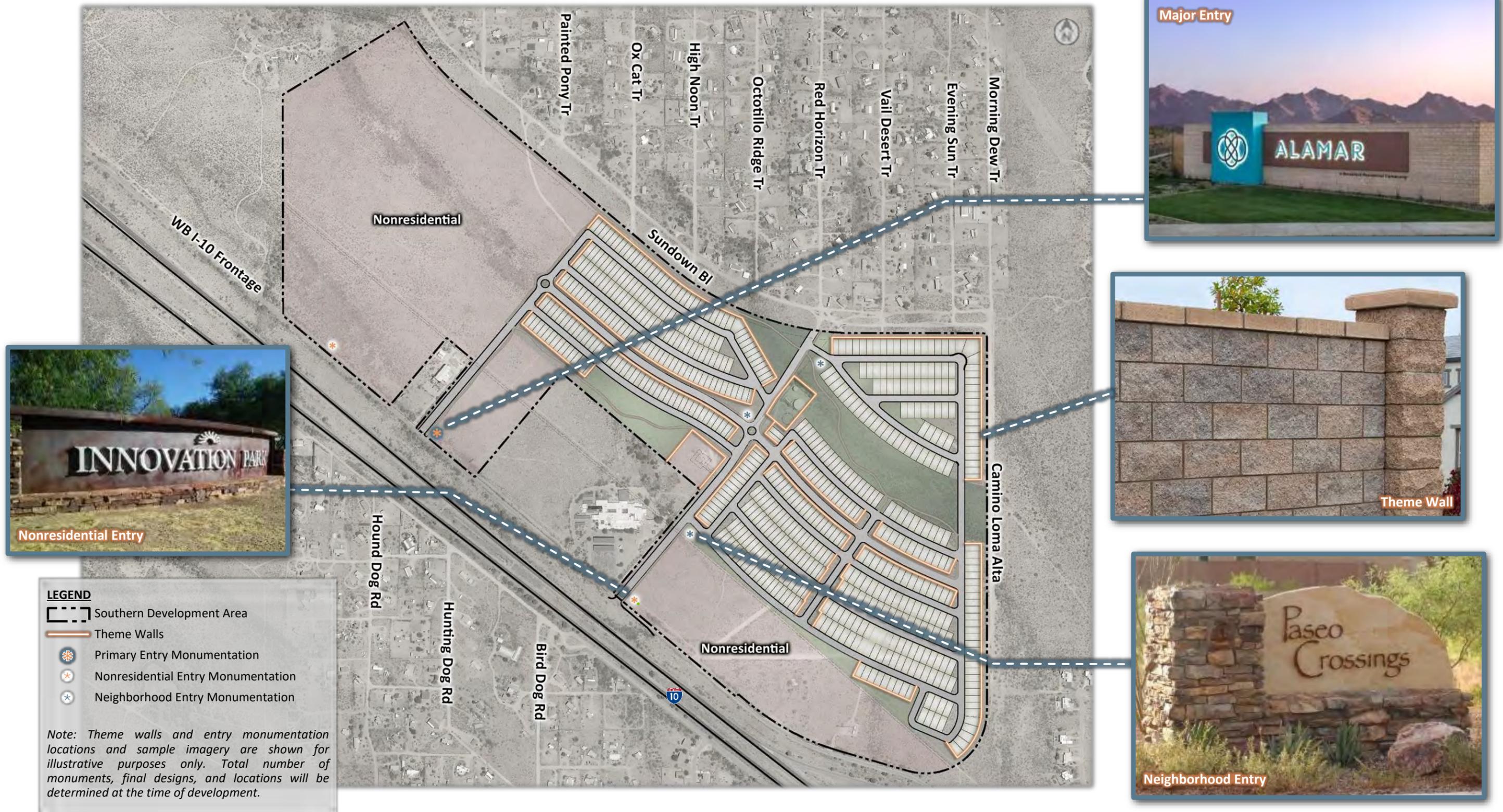


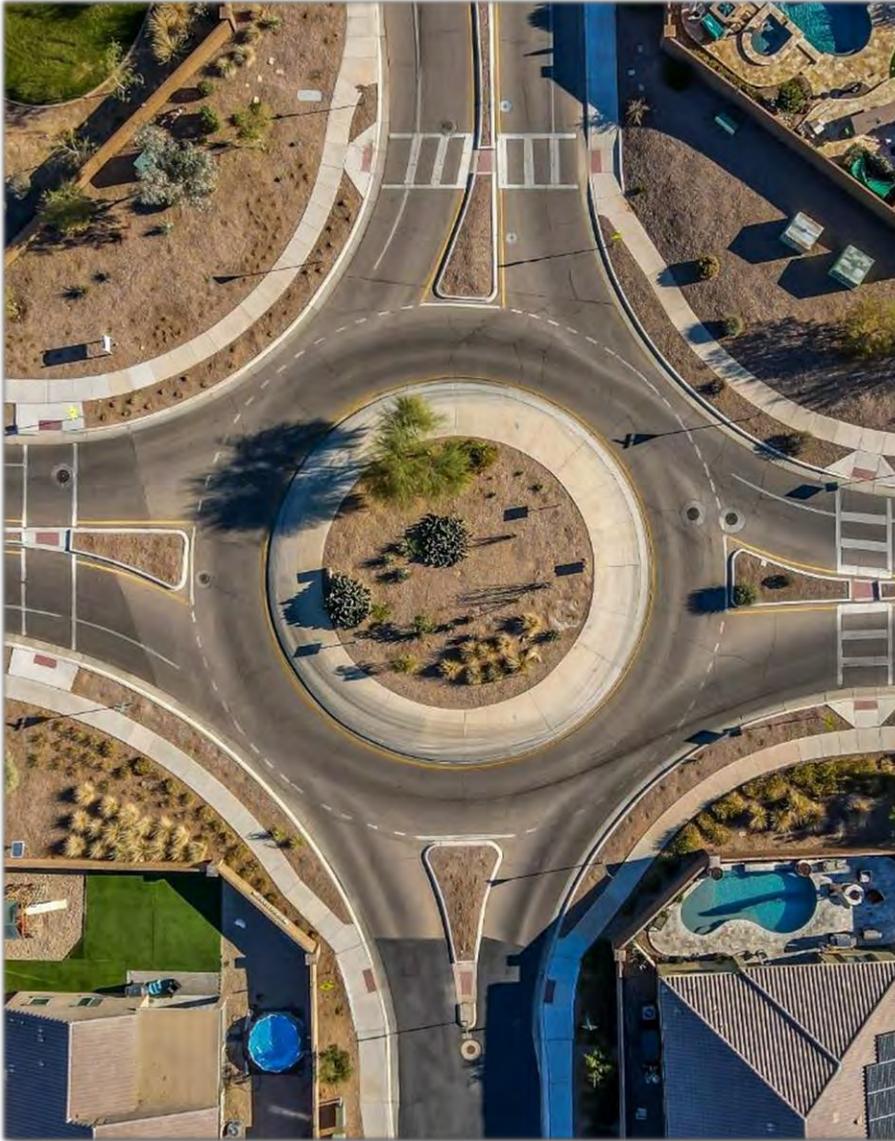
Exhibit IV.E.4: Illustrative Public Art Imagery



Exhibit IV.E.1.a: Northern Development Area Entries and Theme Walls







## F. Circulation and Streetscape Standards

Vail Crossings will provide a variety of multimodal transportation opportunities throughout residential and nonresidential areas of the project that promote safe vehicular circulation and encourage pedestrian activity, cycling, and recreation. Site circulation will occur on public roadways, public trails, and sidewalks.

Circulation will occur predominantly along the streets within Vail Crossings, which will be designed to unify the community functionally, as well as aesthetically. The Vail Crossings streetscape is intended to move vehicles, bicycles, and pedestrians safely and comfortably through the community, while reinforcing a common identity, promoting marketable values, and establishing a distinct sense of place. A variety of streetscape typologies will be utilized throughout the project: collector roads (primary collectors and secondary collectors) and local roads (residential and nonresidential).

Each of these roadway classifications are intended to 1) provide safe and comfortable circulation for motorists, bicyclists, and pedestrians, 2) encourage walking and alternative forms of mobility, 3) enhance wayfinding, 4) soften the threshold between the public right-of-way and built environment, 5) mitigate the urban heat island to the extent practical, 6) harvest rainwater runoff, and 7) reinforce the community identity.

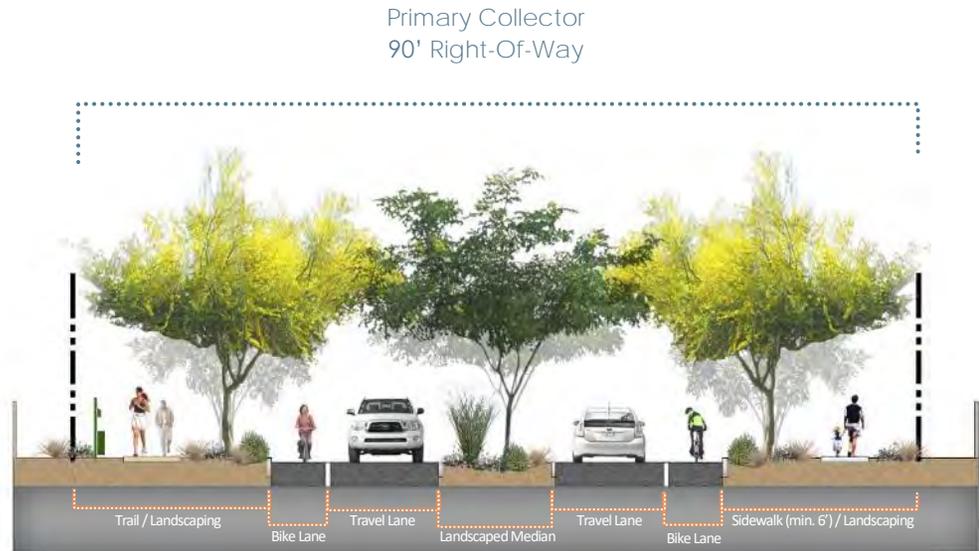
## 1. Collector Roads

A series of collector roads will be provided through Vail Crossings to facilitate central access into each development area as well as circulation between local residential and nonresidential roads while maintaining a common design identity as you move throughout the community. Collector roads will be shared by residential and nonresidential users, since they will provide access through the community to neighborhoods, commercial activity, industrial activity, open space, and recreation. Since end-users and overall site configuration is currently unknown, Vail Crossings proposes two collector road typologies to facilitate primary circulation throughout each development area.

Collector roads are designed to enhance wayfinding and stimulate a sense of place by establishing a formal hierarchy of plant species, vegetative scales, and mass plantings. The selection and layout of plant species proposed within the right-of-way shall reflect the large scale of the right-of-way, while the plant species adjacent to pedestrian paths and walks shall relate to the human scale. Street furniture (which may include, but is not limited to, benches, waste receptacles, bollards, tables, chairs, planters, bike racks, etc.), pedestrian lighting, street lighting, paving, and colors / materials must be uniform and cohesive. Refer to *Exhibit IV.F.i: Northern Development Area Circulation* and *Exhibit IV.F.ii: Southern Development Area Circulation*.



**Primary Collector Roads** serve as the prominent spine roads through Vail Crossings and facilitate interconnectivity between residential neighborhoods and nonresidential uses. Primary collector roads are a minimum of 90 feet in width and consist of two vehicular travel lanes, bikes lanes in both directions, a depressed landscape median, landscaping on either side of pavement, a sidewalk no less than 6 feet in width on one side of the road and earthen trail on the other, and site furnishings (i.e. bench seating, pet waste stations, waste receptacles, etc.). Transit stops can be accommodated in this road type if service is extended to Vail Crossings.



**Secondary Collector Roads** serve as the primary connectors between residential blocks and as the main access into nonresidential area. These roadways range from 60 - 90 feet in width and consist of two vehicular travel lanes with bikes lanes, sidewalks or a sidewalk and trail, and depressed landscaping along either side of the roadway. Parking may be provided on either or both sides of the roadway depending on builder / end-user preference. Vertical or wedge curbs or concrete headers are appropriate. Sidewalks or other pedestrian improvements are not required where adjacent to existing development.





## 2. Collector Road Streetscape Design Criteria

Vail Crossings Collector Roads will comply with the following performance standards:

- Central median shall be depressed with concrete header to capture street runoff and facilitate water retention.
- Central median shall support desert adapted plant material listed in the Pima County Landscape Design Manual. Additionally, 50% of median plant material shall be selected from the Vail Crossings Specimen Plant List, identified in this manual.
- Central median trees do not need to be planted in equidistant rows. Trees may be clustered, where space is sufficient to accommodate growth requirements.
- Central median shall feature permanent underground point source drip irrigation system. Broadleaf trees will require deep root irrigation system.
- Landscape area within the central median shall be depressed between 4 to 8 inches to accommodate rainwater harvesting. Curb cuts shall be designed to allow for the flow of water into the median. Rip Rap shall be used at curb cuts to limit erosion and used for rock check-dams needed to mitigate erosion within medians with significant longitudinal slopes.
- Central median shall feature rock mulch groundcover at a two-inch depth throughout. Screened Rock, Rip Rap, Desert Cobble (mixed rock of varied sizes), and Decomposed Granite are acceptable inert groundcovers. Large accent rock mulch (i.e. Rip Rap and Desert Cobble) are encouraged. In areas of high visibility and activity, 15% (min.) of central median shall feature large rock mulch.
- Bike lanes shall be 6 feet wide and striped.
- Primary collector streets shall feature vertical curb at the edge of asphalt. Vertical curb shall protect the curbway and adjacent sidewalk/pathway.
- Secondary collectors shall provide a sidewalk, trail or multi-use path on both sides of the collector unless adjacent to existing development.

- Secondary collector streets may feature vertical or wedge curb at the edge of asphalt. A 6-foot wide (min.) curbway is encouraged. Landscape area within the curbway shall be depressed between 4 to 8 inches to accommodate rainwater harvesting. Curb cuts shall be designed to allow for the flow of water into the curbway.
- Curbway shall support desert adapted plant material listed in the Pima County Landscape Design Manual. Additionally, 50% of median plant material will be selected from the Vail Crossings Specimen Plant List, identified in this manual. Tree species in the curbway must maintain single trunk form, as identified in the Vail Crossings Specimen Plant List.
- Primary collector streetscapes must accommodate one sidewalk (min.). Sidewalk shall be at least 6 feet wide (min.) and may comprise concrete, decorative concrete, or pavers.
- Primary collector streetscapes must accommodate one trail at least 6 feet in width that may be made of materials, including but not limited to, asphalt, concrete, or stabilized decomposed granite.
- Primary collector streetscapes will feature the following minimum site features for each side of the streetscape: 1 pet waste station, 1 bench, 1 waste receptacle every 1/4 mile. Site furnishing will adhere to the Site Furnishing Standards presented in this document.
- Permanent point-source drip irrigation systems will be installed throughout streetscapes to meet plant water demand. Irrigation systems will require: flow sensors/monitors, soil moisture sensors, and rain-freeze sensors. If the irrigation system is located within the public right-of-way, a license agreement with Pima County will be obtained.

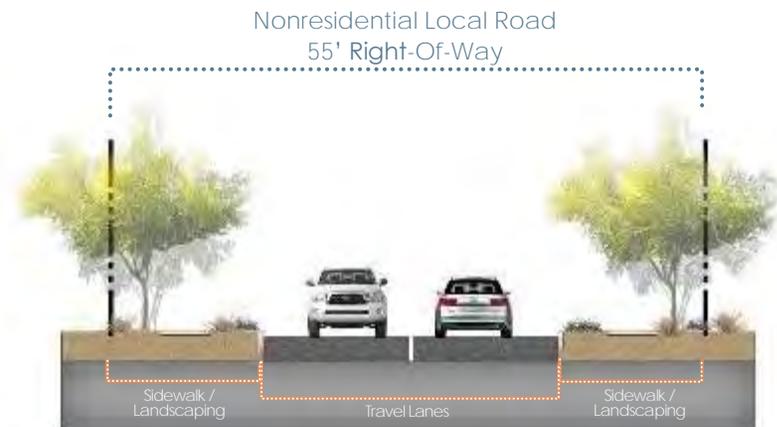


### 3. Local Roads

Local roads will be provided to facilitate circulation through residential subdivisions and nonresidential developments. Local roads shall complement the character of collector roads using common plant palettes and surface treatments. Vegetation along local roads will be supported primarily within private residential lots or nonresidential developments rather than within the right-of-way. Individual subdivisions may incorporate unique limited specimen plants to emphasize a distinctive sense of place. Vail Crossings will feature two types of local roads – residential and nonresidential.

**Residential Local Roads** are lightly trafficked, pedestrian-scaled streets that connect neighborhoods to each other and the larger transportation network. These roads are envisioned to be public streets approximately 50-feet in width consisting of two travel lanes with on-street parking, landscaping, wedge curb and sidewalk on both sides. Vertical curb is appropriate if desired by homebuilders.

**Nonresidential Local Roads** are transitional streets that typically connect nonresidential uses to primary and secondary collector. These roads are public streets envisioned to be 55 feet in width consisting of two travel lanes with landscaping and sidewalks on either side of the roadway. Vertical or wedge curb is appropriate. Pedestrian improvements will not be added where adjacent to existing development.



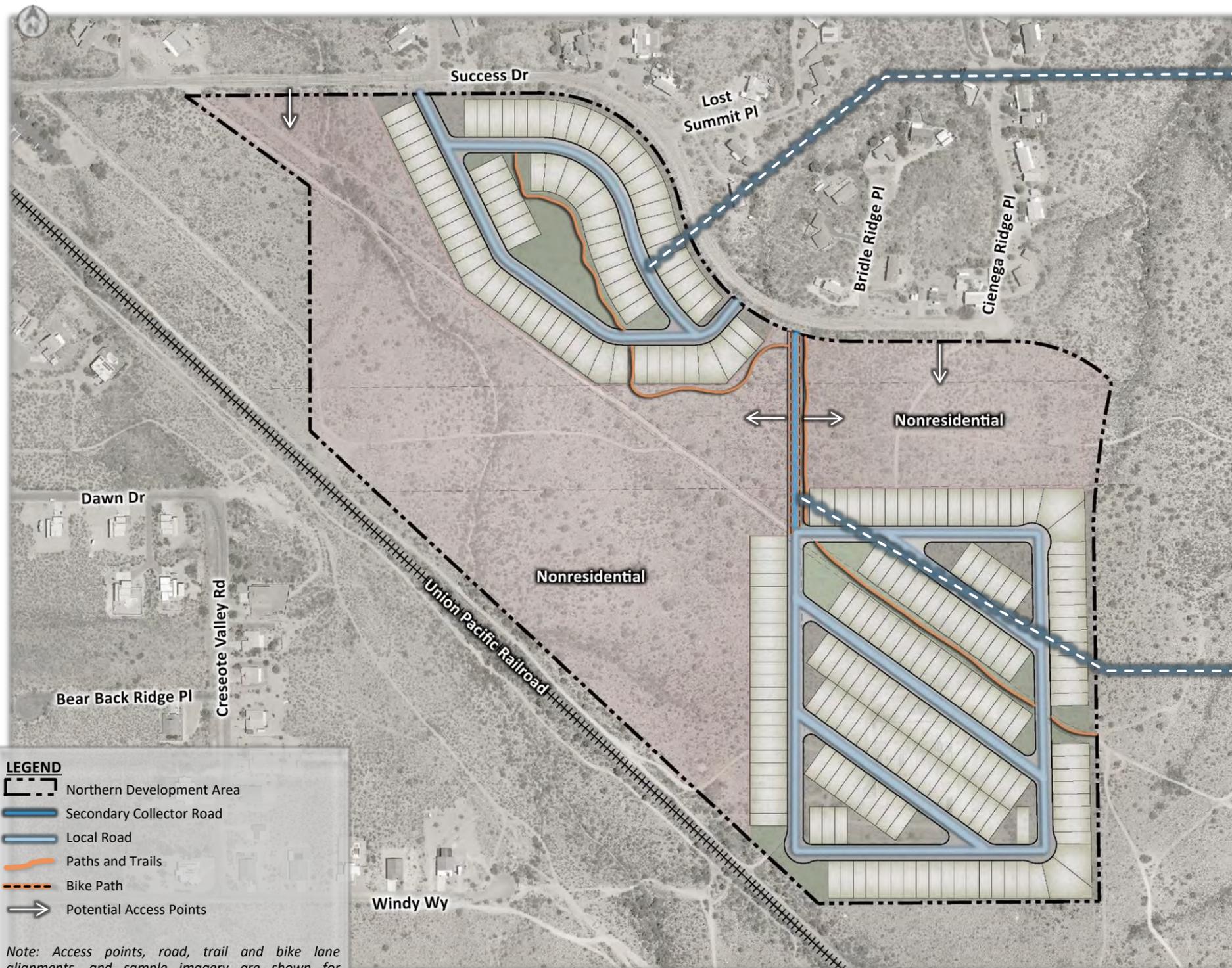


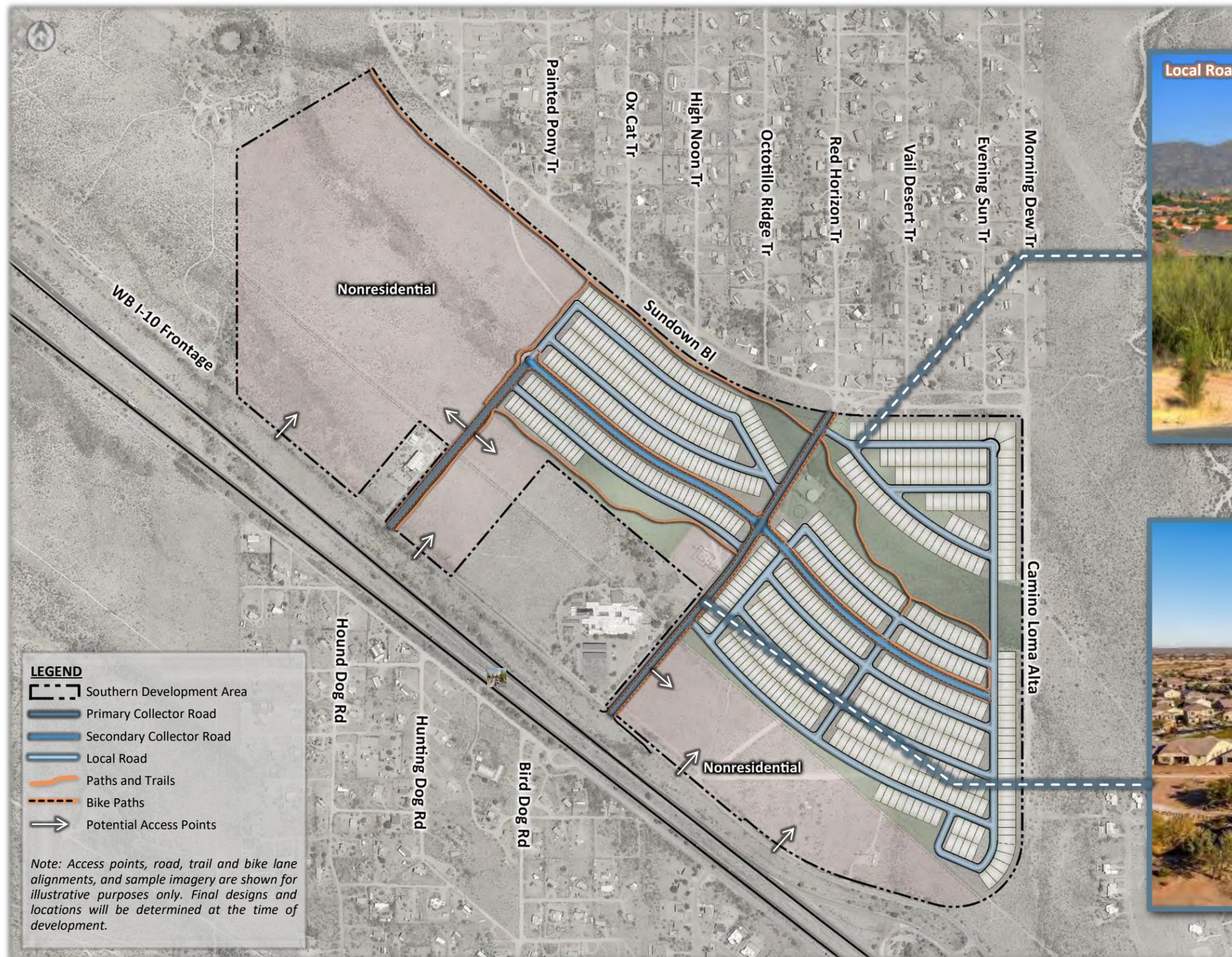
#### 4. Local Road Streetscape Design Criteria

Local roads will feature a 50-foot (min.) right-of-way (ROW) width. The local road ROW will include: 28 feet (min.) pavement width, concrete curbs, and a 5-foot (min.) sidewalk width on both sides of the street. Nonresidential local roads will feature a 55-foot (min.) right-of-way width, per Pima County Standards. The nonresidential ROW will include: 34 feet of pavement, concrete curbs, and a 5-foot (min) sidewalk width on both sides of the street.

Vail Crossings Local Roads will comply with the following performance standards:

- Local roads must accommodate one sidewalk on each side of the street. Sidewalks shall be 5 feet wide (min.). Sidewalk materials may comprise natural concrete, decorative concrete, or concrete pavers.
- Local roads that provide primary access into residential neighborhoods shall provide a decorative pavement at cross walks. Decorative pavement may include painted art, stamped asphalt, pavers, or decorative concrete.
- Landscape along the streetscape will not be provided within the ROW; however, each abutting residential lot shall feature one 15 gallon size tree (min.) and three 5 gallon size shrubs (min.) within 10 feet of the ROW boundary. 10-foot wide (min.) landscape bufferyards shall be provided along nonresidential areas to accommodate shade trees on each side of the road. Trees and vegetative ground cover shall be selected from the Vail Crossings Specimen Plant List, within this document. Also, see *Residential Landscape Guidelines* section of this document for more information.
- Postal boxes shall be per the guidance of the local Postmaster.







## G. Landscape Design Standards

Thoughtful approach to landscape master planning, design, and maintenance can unite residential and nonresidential portions of the project site with a singular identity, while also providing the sense of community pride, comfort, and vitality that encourages homeownership, community engagement, and activity. Vail Crossings landscape intends to celebrate the beauty of the Sonoran Desert and unify residential and nonresidential development, improved open space, natural open space, streetscapes, parks, and recreational areas with a consistent theme. It is intended that residential landscapes complement nonresidential landscapes (i.e. commercial and industrial uses) to reinforce a united identity throughout the project. Residential landscapes will enhance the community at a pedestrian scale to encourage outdoor activity for all generations throughout the year.

The future Community/Master Association ("the Association") is the entity responsible for the maintenance and management of all shared private common areas that are not dedicated to Pima County, as well as landscape and streetscape improvements and irrigation within Pima County public rights-of-ways. Streetscape landscape shall be designed and installed by the Master Developer and maintained by the Association. Streetscape vegetation will be irrigated with high efficiency and ET (evaporation-transpiration) based drip irrigation systems and receive supplemental irrigation through passive water harvesting systems. Landscape throughout Vail Crossings will be comprised of plant material, inert ground cover, irrigation, and site furnishings.

General Plant Material and Irrigation performance standards:

- The plant palette at Vail Crossings shall adhere to the *Pima County Landscape Design Manual*, although the Vail Crossings Specimen Plant List will identify required plant species for use in particular situations. All plant material and specifications shall conform to *Arizona Nursery Association* standards. Plants shall be quality material (healthy, vigorous, free of insects, free of injury/damage) and demonstrate standard growth habit for the species.
- Minimum plant sizes at installation shall be: trees at 15 gallon, shrubs at 5 gallon, accents (cacti, succulents, Ornamental Grasses) at 5 gallon, groundcovers @ 1 gallon. All container grown trees and shrubs shall meet *Arizona Nursery Association* minimum guidelines as it pertains to caliper and height during the selection process of plant materials. In certain focus areas, larger trees will be implemented.
- Common area natural turf shall be limited to functional and active park spaces.
- No maximum coverage standard shall apply to formal ball fields. Permitted warm season turf varieties include Buffalo Grass (*Buchloe dactyloides* 'Prestige') and Bermuda Hybrids.
  - Recommended Bermuda Hybrids include Midiron (general application), and Tifway (sports fields).
- Artificial turf is permitted in common and private landscape areas.





- All passive landscaped common areas shall be depressed 4 to 8 inches to collect rainwater where it falls. Curb cuts and sidewalk scuppers shall be provided along roadways, where roads are crowned and landscape is capable of harvesting rainwater. Capable landscape area is flat (less than 4% slope), more than 10 feet from a building foundation, and void of infrastructure interference.
- All landscaped common areas and front yards shall be covered with inert groundcover, which comprises the following options: screened rock, rip rap, desert cobble (mixed rock of varied sizes), and decomposed granite. When inorganic groundcovers are used, they shall be in combination with live plants and not exceed two-thirds of the total area of applied groundcovers.
- Hydro-seed groundcover may be utilized in place of inert groundcover for environmental restoration along natural open space corridors, within natural park spaces, and alongside trails. Hydro-seed mix shall utilize only native plant species. Hydroseed shall be installed in accordance with Section 805 of *PAG Standard Specifications and Details for Public Improvements*.
- Permanent point-source drip irrigation systems will be installed throughout residential common areas to meet plant water demand. Irrigation systems will require flow sensors/monitors, soil moisture sensors, and rain sensors. Irrigation systems shall be installed according to the performance standards outlined in Section 18.73.030.B.8 of the *Pima County Zoning Code*.

## 1. Residential Landscape

The intent of the landscape in residential areas is to activate the public realm by providing an array of comfortable pedestrian routes and outdoor gathering spaces. In order to maintain an activated streetscape throughout all seasons, mixed-use landscapes shall provide for a range of microclimates, offering significant shade in the summer and sun in the winter.

Promenades, plazas, courtyards, and pocket parks are all appropriate residential areas. Hardscape accents throughout the public realm shall reinforce the identity of the community and reflect hardscape found along the primary collector. Typical walkways will be a minimum 5' wide.

Vegetation in the residential zones shall provide a scaled transition between adjacent building heights and outdoor spaces. Large trees shall be used to identify major public spaces. Passive water harvesting applications and techniques are encouraged where practical.

Additional performance standards for residential area landscape includes:

- Homebuilders shall provide the option to homebuyers, at time of construction, to install rainwater harvesting micro-basins (4 to 8 inches) depressed landscape area) in front yards. 100% of all speculation homes (spec homes) shall include micro-basins within front yards, where landscape area is capable of harvesting rainwater. Capable landscape area is flat (less than 4% slope), more than 10 feet from a building foundation, and void of infrastructure interference. Micro-basins in front yards shall feature a 1-inch or greater screened rock mulch, to a depth of 2 inches (min.). Micro-basins in front yards shall be feature a natural aesthetic (i.e. no formal geometric shapes, basins slopes shall be 4:1 or greater).





- Maximum allowable private natural turf on residential lots shall be limited to 25% of the overall lot size, including backyard, front yard, and side yards. Natural turf may not be installed within 3 feet of a shared lot wall, to reduce irrigation overspray on masonry units. Typical warm season turf varieties include: Buffalo Grass (*Buchloe dactyloides* 'Prestige'), Bermuda Hybrids, St. Augustine, and Zoysiagrass. Permitted cool season turf varieties include: Perennial Ryegrass and drought tolerant hybrid Tall fescue.
- The front yards of each lot shall feature at least one tree, two shrubs and two accent ground covers. .

## 2. Nonresidential Landscape

The intent of landscape in the nonresidential areas (commercial, office, industrial, etc.) is to soften and unify the appearance of the commercial buildings with the surrounding community. The commercial landscape will emphasize the major commercial entryways, shade parking areas, highlight architectural features, frame views, guide circulation, and mitigate storm water throughout area. The nonresidential landscape shall complement the primary collector streetscape through its use of consistent hardscape materials and patterns, as well as softscape schemes. Site furnishings shall also remain compatible with the primary collector; however, nonresidential areas will likely accommodate a wider selection of site elements in high use gathering spaces (plazas, outdoor dining spaces, and building entrances, etc.).

---

The following standards will ensure that commercial landscapes exemplify a high standard of natural site preservation/mitigation, efficient water usage, healthy plant installation, and landscape maintenance.



### 3. Vail Crossings Specimen Plant List

Vail Crossings plants must be selected from the *Pima County Approved Plant List*. To ensure that Vail Crossings will showcase native/near-native vegetation and maintain a cohesive identity throughout common areas, the project will meet the following performance standards:

*70% (min.) of "non-park" common area trees must comprise any number of the following species:*

- Celtis Reticulata (Netleaf Hackberry)
- Chilopsis Linearis (Desert Willow), Seedless Varieties
- Ebanopsis Ebano (Texas Ebony)
- Olneya Tesota (Ironwood)
- Parkinsonia 'Desert Museum' (Desert Museum Palo Verde)
- Parkinsonia Florida (Blue Palo Verde)
- Parkinsonia Microphylla (Foothills Palo Verde)
- Parkinsonia Preacox (Palo Brea)
- Prosopis Glandulosa (Honey Mesquite)
- Prosopis Velutina (Velvet Mesquite)

*70% of "park" common area trees must comprise any number of the following species:*

- Celtis Reticulata (Netleaf Hackberry)
- Chilopsis Linearis (Desert Willow), varieties
- Fraxinus species

- Ebanopsis Ebanus (Texas Ebony)
- Olneya Tesota (Ironwood)
- Parkinsonia 'Desert Museum' (Desert Museum Palo Verde)
- Parkinsonia Florida (Blue Palo Verde)
- Parkinsonia Microphylla (Foothills Palo Verde)
- Parkinsonia Preacox (Palo Brea)
- Platanus Wrightii (Arizona Sycamore)
- Prosopis Glandulosa (Honey Mesquite)
- Prosopis Velutina (Velvet Mesquite)
- Quercus species



*70% of common area shrubs must comprise any number of the following species:*

- Abutilon Palmeri (Indian Mallow)
- Acacia Constricta (Whitethorn Acacia)
- Acacia Greggii (Catclaw Acacia)
- Berlandiera Lyrate (Chocolate Flower)
- Buddleia Marrubifolia (Woolly Butterfly Bush)
- Calliandra Eriophylla (Fairy Duster)
- Calylophus Hartwegii (Sundrops)
- Celtis Pallida (Desert Hackberry)
- Dalea Capitata (Lemon Dalea)
- Dalea Frutescens (Black Dalea)
- Dalea Greggii (Trailing Indigo Bush)
- Dodonea Viscosa (Hop Bush)





- *Encelia farinosa* (Brittlebush)
- *Justicia californica* (Chuparosa)
- *Justicia candicans* (Red Justicia)
- *Justicia spicigera* (Mexican Honeysuckle)
- *Larrea tridentata* (Creosote)
- *Leucophyllum* species
- *Ruellia peninsularis* (Baja Ruellia)
- *Salvia greggii* (Autumn Sage)
- *Senna covesii* (Desert Senna)
- *Simmondsia chinensis* (Jojoba)
- *Sphaeralcea ambigua* (Globemallow)
- *Tecoma stans* (Yellowbells)
- *Thymophylla pentachaeta* (Golden Dyssodia)
- *Vauquelinia californica* (Arizona Rosewood)
- *Viguiera parishii* (Goldeneye)

70% of common area accent plants (cacti, succulents, grasses) must comprise any number of the following species:

- Agave species
- Asclepias Subulata (Desert Milkweed)
- Bouteloua species
- Carnegiea Gigantea (Saguaro)
- Cyllindropuntia species
- Dasylirion Wheeleri (Desert Spoon)
- Echinocereus Engelmannii (Hedgehog Cactus)
- Euphorbia Antisyphilitica (Candelilla)
- Ferocactus species
- Fouquieria Splendens (Ocotillo)
- Hesperaloe species
- Muhlenbergia species
- Nolina Micorcarpa (Bear Grass)
- Opuntia species
- Pedilanthus Macrcarpus (Lady's Slipper)
- Yucca species



Common Area Restricted Plant List:

- Citrus Trees
- Palm Trees
- Eucalyptus Trees
- Pine Trees



#### 4. Site Furnishings

Site furnishings will be included throughout Vail Crossings in parks, recreation areas, streetscape, and trail junctions. A coordinated, consistent selection of site furnishings will help to establish a cohesive project identity and unite distinct portions of the community. Site furnishings may include: benches, tables outdoor grills, bike racks, waste receptacles, pet waste stations, drinking fountains, flag poles, decorative fencing, tree grates, planters, etc.

Site furnishings shall be rated for outdoor weather in arid environments. Site furniture shall be constructed from durable materials that deter graffiti and vandalism. They must hold up to heavy use and require minimal maintenance. Covered bus stops and waiting areas to shelter pedestrians from extreme heat, wind, or rain shall be provided if transit service reaches Vail Crossings.

The first site furnishings installed during the first phase of this project will set the precedent that all future phases of the project will emulate. Site furnishings shall be reviewed and approved by the Master Developer prior to installation. Aesthetic styles, colors and finishes shall coordinate throughout the entire project. Site furnishings may feature unique aesthetic, color, or finish when used specifically for artistic expression as an accent in the landscape.

**Recommended materials:** concrete, steel, aluminum, plastic coated metal (thermoplastic, plastisol, etc.), powder coated metal.

**Restricted materials:** wood, plastic, recycled plastic.

## 5. Hardscape

Hardscape is the term used to describe built structure and building material within landscape common area and open space. Vail Crossings hardscape will include sidewalks, paths, trails, seating areas, plazas, retaining walls, raised planters, theme walls, theme columns, residential lot walls, monuments, site furnishings, etc. Hardscape construction materials shall be selected to withstand the arid climate, deter vandalism, minimize maintenance, and reinforce community identity.

All hardscape materials shall coordinate with each other. Theme Walls, Theme Columns, Residential Lot Walls, and Monument Signs shall utilize materials, colors, and finishes consistent with the Master Development theme. Accented flatwork (decorative walkways, seating areas, plazas, gathering areas, etc.) shall also feature coordinating materials. Hardscape materials shall be reviewed and approved by the Master Developer prior to construction.

*Recommended building materials, according to standard hardscape elements:*

- Sidewalks and standard flatwork (for pedestrian activity): natural grey concrete



- Trails: 1/4-inch minus stabilized decomposed granite (or raked earth only in natural areas (i.e. washes or riparian areas))
- Accent flatwork (for pedestrian activity): integral colored concrete, painted concrete decorative finished concrete, concrete pavers, brick pavers, exterior rated tile, stabilized decomposed granite, etc.
- Retaining walls: poured in place concrete, concrete block (cmu)
- Theme walls and columns: see “theme wall” section of this document
- Monuments: see “monument” section of this document
- Site furnishing: see “site furnishing” section of this document
- Fences: steel, aluminum, and wrought iron view fencing

*Restricted hardscape building materials:*

- Sidewalks and standard flatwork (for pedestrian activity): raked earth and stabilized decomposed granite
- Retaining walls: smooth natural grey block or smooth concrete, wood
- Monuments: Natural Grey Block or Concrete, Wood
- Site furnishings: see “site furnishing section of this document

- 
- Fences: wood, vinyl, and plastic.

## H. Recreation and Open Space

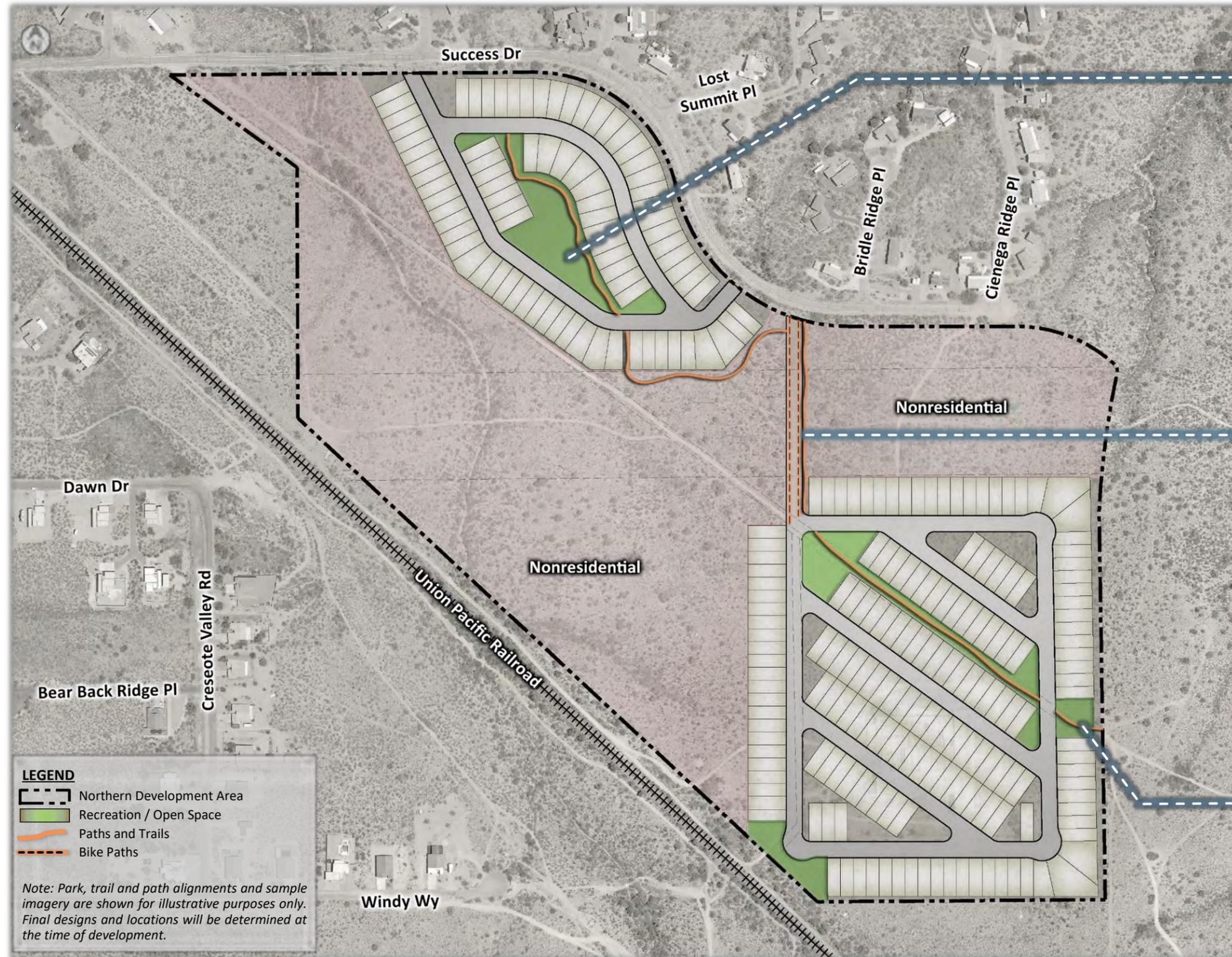
### 1. Parks and Open Space

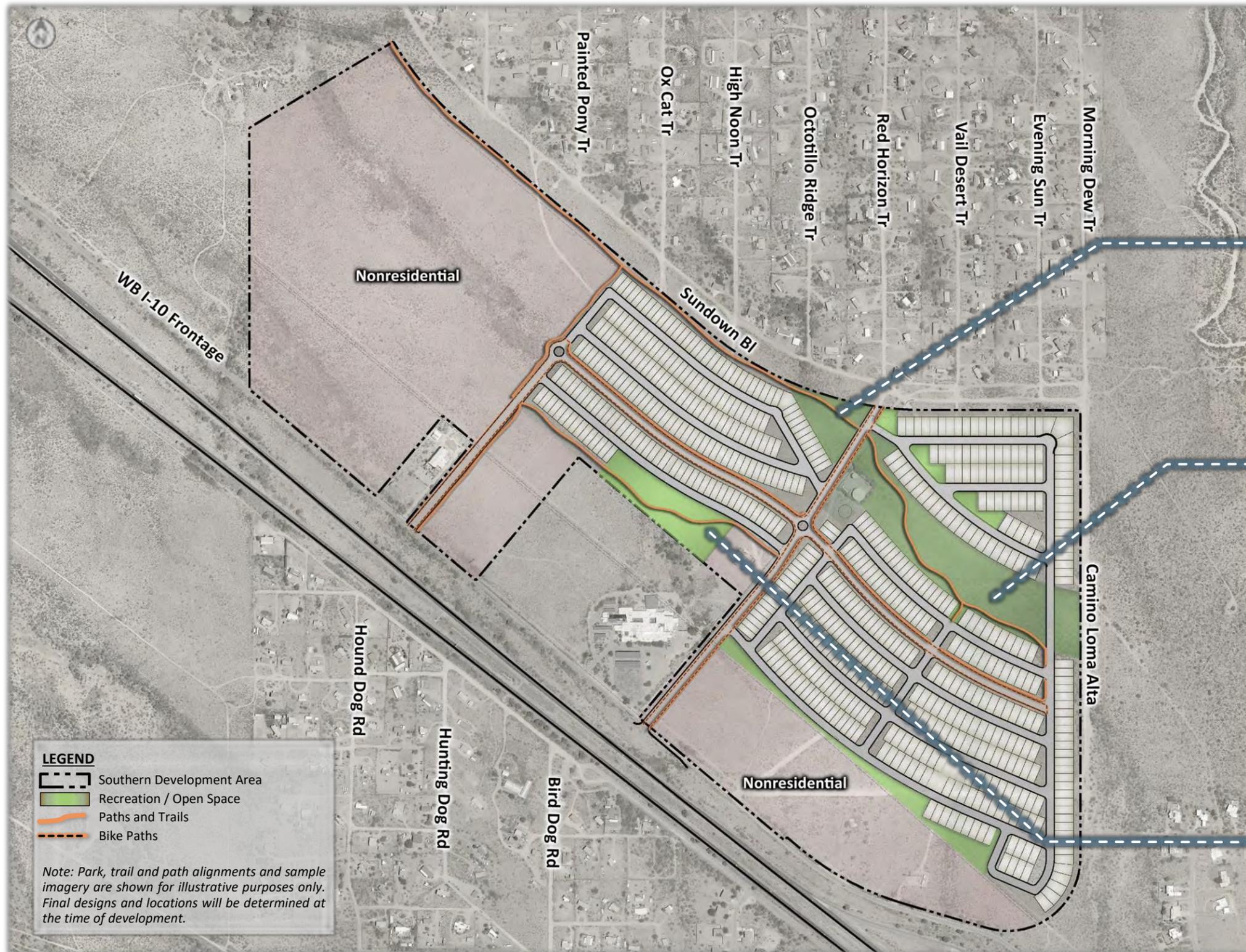
Parks and open space are a key element to building a great community. Vail Crossings will meet all recreation standards established in the *Pima County Recreation Area Design Manual*. Vail Crossings will provide strategically located park and pocket parks with the proper amenities and connections to open space and easily accessible and visible from streets. Park areas will accommodate both active and passive recreational uses. Park/site elements shall complement streetscape/site elements through the use of similar hardscape materials, colors, and patterns, as well as similar site furnishings (benches, waste receptacles, lighting, bike racks, etc.). Permeable hardscape materials are encouraged where practical. Accentuating contrast, while remaining true to the overall community theme, is a key to the success of each park and the community as a whole.

While park vegetation should complement streetscape vegetation, unique specimen plantings are encouraged, including shade trees and pedestrian scaled sensory plantings. Vegetation along park boundaries shall maximize views into the park and frame viewsheds to iconic park elements and high activity spaces (play areas and gathering spaces).

Parks shall be shaped and graded to accommodate large scale passive rainwater harvesting through the use of berms, vegetated swales, terraces, gabions, curb inlets, and culverts. High efficiency evapotranspiration (ET) drip irrigation systems will be installed in all non-turf landscaped areas.







## 2. Neighborhood Pocket Parks

Neighborhood pocket parks are to be located within each residential block, and primarily serve adjacent neighbors. These parks are smaller and less amenitized than the Community Park. The builders will develop, and then HOAs will maintain the neighborhood parks.

### Neighborhood Park Elements include:

- One themed park sign
- Pedestrian paths
- Shade structure with concrete pad and picnic table
- One play area
  - One play structure
  - One integrated shade canopy
  - Free standing play elements
  - Play surface
- One drinking fountain or water bottle refill station
- Two bicycle racks
- Two park benches (or seating areas)
- One waste receptacle
- One pet waste station
- Landscape (native preferred)
- Permanent point source sub-surface irrigation for all landscaped areas





### 3. Community Park

The Community Park is a centralized park that serves the greater community. This is the largest park at Vail Crossings and is sufficiently amenitized.

#### Community Park Elements include:

- One themed park sign (with public art component)
- Integrated pedestrian paths
- Shade structure with concrete pad with 2 picnic tables and 1 grill
- One play area
  - One handicap accessible play structure
  - One integrated shade canopy
  - One swing set
  - Free standing play elements
- One drinking fountain or water bottle refill station
- Bicycle racks
- Park benches (or seating areas)
- Waste receptacles
- Pet waste stations
- One doggie drinking fountain
- Landscape (native preferred)
- Permanent point source sub-surface irrigation for all landscaped areas.
- Rainwater harvesting basins in all landscaped areas more than 10 feet from a structural foundation
- Event Lawn (natural or artificial).

Park amenities may be phased in over time. In the event that *Pima County Natural Resources, Park, and Recreation (NRPR)* takes over the maintenance of the park, facilities shall conform to the NRPR Development Standards, Specifications, and Details, shall require review of improvement plans by NRPR, and shall be fully developed prior to turnover.

#### 4. Open Space (Paths, Trails, and Greenbelts)

##### a. Paths and Trails

Paths and trails provided throughout Vail Crossings are intended to be used actively for non-vehicular multi-modal transportation and recreation by pedestrians (walkers, joggers, runners, etc.), cyclists, skateboarders, scooters, and others. Paths are at least 6 feet wide (min.) and feature concrete, asphalt, or stabilized earthen surface.

Vail Crossings' paths will be located within the right-of-way of the collector roads (primary and secondary). The proposed paths are intended to provide residents the ability to connect to nonresidential uses, other residential communities, open space, and recreational uses throughout the project site. Paths serve both a functional purpose (move pedestrians safely and efficiently through the site) and recreational purpose. Streetscape landscape will provide shade areas, buffering, and interest between pathways, roadways, and adjacent private properties.

Vail Crossings trails will be located primarily along open space corridors and collector streets. Trails comprise a 6-foot wide (min.) stabilized decomposed granite walking surface along collector streets that will provide access to the natural areas of the site where the trail surface may transition to raked earth for a more natural, environmentally-friendly aesthetic. Trails may connect residents, patrons and office workers with adjacent nonresidential uses for dining, entertainment, and recreation activities.

The Master Developer and/or its successors (such as homeowners association(s)) will maintain trails and paths within public rights-of-way within the limits of Vail



---

Crossings, unless part of the Community Park and Pima County NRPR has agreed to take on maintenance responsibilities.

### *b. Greenbelts*

Every effort will be made to maintain the natural look, location, and function of the original washes. Corridors will be enhanced to provide natural habitat connectivity across the site. Native plants will be preserved in place where practical. Greenbelts will also provide recreational and aesthetic value to the community. Corridors may be left in a natural state (non-channelized) with bank protection pending review and approval by the Regional Flood Control Department.

## I. Views and Lighting

Vail Crossings is uniquely situated within the outskirts of the Tucson metropolitan area, where mountain views dominate the horizon and dark nighttime skies shimmer with the stars above. The Southern Development Area is highly visible from the freeway, while the Northern Development Area is located adjacent to the southernmost foothills of the Rincon Mountain Range.

### 1. Views

**Northern Development Area:** Unlike the Southern Development Area, the Vail Crossings Northern Development Area is not located in an area of high freeway visibility. It is situated between a neighborhood of manufactured homes (to the north), railroad tracks (to the south), and natural desert (to the east and west). Varied topography, however, provides views across the property and to the north of the Rincon Mountains beyond Success Drive and the adjacent neighborhood. The rolling topography throughout this area will naturally generate a variety of views into, through, and from the project site. Landscape, theme walls, monuments, and architectural character will be clearly visible to passersby along Success Drive. View fences are encouraged for homes adjacent to open space. Screening (walls and vegetation) will be required adjacent to parking lots and nonresidential uses (commercial, industrial, office, etc.) See the *Residential and Nonresidential Architectural Design Criteria* for more information about views along perimeter edges, corner lots, and internal building massing/orientation.

**Southern Development Area:** The Vail Crossings Southern Development Area is located at the Interstate 10 and Highway 83 interchange. The project site is highly visible from the freeway, which will enable the Vail Crossings community to serve as the first community landmark into the Tucson Metropolitan region for westbound travelers along I-10 and

northbound travelers along SR-83. Landscape, theme walls, monuments, and architectural character will be clearly visible to passersby.

The community within this Southern Development Area will provide an array of internal views through a diversity of streetscapes, parks, recreation area, natural open spaces, wash corridors, landscape and varied architectural character. View fences are encouraged for homes adjacent to open space. Screening (walls and vegetation) will be required adjacent to parking lots and nonresidential uses (commercial, industrial, office, etc.) See the *Residential and Nonresidential Architectural Design Criteria* for more information about views along perimeter edges, corner lots, and internal building massing/orientation.

## 2. Lighting

Vail Crossings recognizes the importance of responsible site lighting, which enhances safety and security while encouraging activity throughout the evenings. Site lighting fixtures also reinforce community identity and should be selected to complement aesthetic architectural styles and site furnishings. It is important that all site lighting comply with the Pima County Outdoor Lighting Code. Dark skies that maintain visual access to astronomical activity are important to this project and neighboring properties.

The following lighting standards shall apply to all Vail Crossings land development:

- Vail Crossings shall comply with the Pima County Outdoor Lighting Code.
- Site Construction Permit Package shall include Outdoor Lighting Plan.
- Retro-reflectance for signs and markers shall be used as passive tools (instead of active lighting) to aid visibility.
- Use timers, motion sensors, automatic shutoff devices, smart controls to dim or turn off lights during periods of less use and increase levels during periods of activity.
- Use of certain lamps (color or wavelength combined with bandwidth minimization) is encouraged to minimize adverse environmental effects of artificial light at night.
- Use fully-shielded lights to the greatest extent feasible.
- Lights within park amenities shall automatically shut off after a certain duration (for example: 20 minutes unless the user takes further action (presses an 'on button' or activates a motion sensor).
- Encourage the use of smart controls so that parking lots are not lit all the time if they aren't used. As the night wears on, lights for spaces further from the activity could be turned off or dimmed if they are not needed.





# VAIL CROSSINGS

PART V - IMPLEMENTATION &  
ADMINISTRATION

## V. IMPLEMENTATION & ADMINISTRATION

### A. Purpose

This section of the Vail Crossings Mixed Use Design Guidelines is intended to provide regulatory procedures designed to guide the implementation of guidelines presented herein. This section also provides guidance regarding the general administration of the amendment procedures to the design guidelines. The provisions below shall apply to the entire project area as defined herein.

#### 1. General Implementation

The implementation of the Vail Crossings Mixed Use Design Guidelines is the responsibility of the Master Developer and Pima County.

The Master Developer is the entity responsible for providing the basic infrastructure needs to support the development of Vail Crossings including utility extension and construction of collector roads, and for ensuring a common community identity.

At any time, the Master Developer may relinquish its rights and assign any or all rights to one or more successors. Associated responsibilities of the Master Developer would also be transferred to the future Master Association/Homeowner's Association at that time.

Pima County Development Services Department and the Master Developer will be responsible for ensuring all guidelines and standards laid out herein are adhered to during the review of all development within Vail Crossings.

#### 2. Interpretation

The Chief Zoning Inspector shall be responsible for interpreting the guidelines presented herein. Appeals to the Chief Zoning Inspector's interpretation may be made to the Board of Adjustment within fifteen days from the date of the interpretation. Per the LOU established between Pima County and the property owner, and Pima County Code Section 18.51.070.E.3, final site configurations may be modified up to 25% subject to approval by the Planning Official.

#### 3. Development Review Procedures

All development within Vail Crossings shall be conducted substantially in accordance with the standards and guidelines outlined herein. Per the LOU established between Pima County and the property owner, and Pima County Code Section 18.51.070.E.3, standards and may be modified up to 25% subject to approval by the Planning Official.



# VAIL CROSSINGS

## PART VI - CONFORMANCE MATRIX

# CONFORMANCE MATRIX

## A. Layout Guidelines

DESIGN GUIDELINES	LOCATION (SECTION)
- Pedestrians should be able to easily identify primary entrances into commercial establishments.	IV.B, IV.D.2, IV.E.
- Buildings should be oriented to a defined pedestrian walkway or street.	IV.E
- Building materials and colors should exhibit quality and help establish a human scale while providing visual interest.	IV.B, IV.D.2.a - .c
- To facilitate the creation of a sense of place, developers are encouraged to incorporate artists into the design team from the inception of planning in order to integrate art into their projects.	IV.E.3
- Low occupancy uses such as warehouses, storage, and parking will not be counted in the evaluation of mixed-use functions.	APPENDIX
- Special attention should be given to the design of project and building corners as an opportunity to create visual interest and invite activity.	IV.B.2.b
- Transit stops should be incorporated into the layout of all mixed-use developments, regardless of service availability. Space should be available.	IV.F.1
- Buildings should be located in areas that recognize local viewpoints and landmarks. Views of landmarks and natural features should be highlighted through the placement of structures.	IV.I.1
- Larger sites should feature places where people can gather. Public spaces should contribute to the overall sense of place and site identity and help to attract pedestrian users to the development.	IV.B
- Loading docks should be located in areas that have the least amount of impact on residential uses.	IV.D.2.d

## B. Parking, Vehicular, And Pedestrian Access Guidelines

DESIGN GUIDELINES	LOCATION (SECTION)
<ul style="list-style-type: none"> <li>- The design of access and circulation should tie the development into the overall neighborhood creating opportunities for nearby residents to access the project either on foot, bicycle, or other form of alternate transportation.</li> </ul>	IV.F
<ul style="list-style-type: none"> <li>- The joint use of driveways and parking areas should be encouraged to reduce overall parking needs. A convenient pedestrian connection must exist between the building facilities and/or properties to qualify as a joint use parking facility.</li> </ul>	IV.D.2.k
<ul style="list-style-type: none"> <li>- Primary building entrances should front major pedestrian access-ways.</li> </ul>	IV.B, IV.D.2
<ul style="list-style-type: none"> <li>- Link new mixed-use developments with existing developments and trail systems.</li> </ul>	III.A, III.B
<ul style="list-style-type: none"> <li>- Provide variety in setback, height, color and building size and form to enhance the pedestrian experience. This should be done under a unified concept.</li> </ul>	IV.D.2
<ul style="list-style-type: none"> <li>- Parking should be conveniently located near nonresidential uses yet visibly minimized from arterial streets and public spaces.</li> </ul>	IV.D.2.k
<ul style="list-style-type: none"> <li>- Roadways and pedestrian access should be designed to provide maximum access to public transportation</li> </ul>	IV.F

## C. Landscaping Guidelines

DESIGN GUIDELINES	LOCATION (SECTION)
- Preserve public access to public areas of interest such as parks, natural features, landmarks and monuments.	III.A, III.B, IV.G, IV.H
- Include open spaces with special amenities that encourage use, such as benches and sitting areas.	IV.B, IV.G.4, IV.H.2 - .4
- If several buildings are proposed for a site, the spaces between the buildings should contribute to the overall positive open space of the area.	IV.B
- Open spaces should connect with and provide views to natural amenities.	IV.H.4
- Service and trash areas should be screened from view on all sides.	IV.D.2.d
- Service areas should not impede access to amenities.	IV.d.2.d
- Tree selection and location should promote safety and security, enhance natural environment, provide shade for vehicles and pedestrians and minimize maintenance requirements.	IV.G
- Plant selection and placement should reduce heat islands wherever possible.	IV.D.2.K, IV.G
- Low water use plants and native vegetation should be used to landscape new developments.	IV.G
- No invasive plant species should be allowed in the landscaping design.	IV.G.3

## D. Signage Guidelines

DESIGN GUIDELINES	LOCATION (SECTION)
- Design buildings with careful consideration for the incorporation of signage and lighting.	IV.D.2.I, IV.E, IV.I.2
- Signage should contribute to the overall architectural and landscape theme.	IV.E
- Signage should be made of materials that can tolerate extreme weather.	IV.E
- Signage should be used to clearly identify public versus private/residential areas.	IV.E

## E. Safety Guidelines

DESIGN GUIDELINES	LOCATION (SECTION)
- Architectural features should be used to provide weather protection and shade, as well as highlight building features and entries.	IV.C.2.a - b, IV.D.2.a - b
- Sidewalks, paths and bike lanes, which are protected from traffic, are encouraged.	IV.F
- Landscaping and lighting should be used to identify entrances, pathways, public spaces, and bus stops.	IV.D.2, IV.G
- Covered bus stops and waiting areas should be provided to provide pedestrians with outdoor areas sheltered from extreme heat, wind, or rain.	IV.G.4
- Lighting should contribute to the overall safety of the development, and landscaping should incorporate safe-by-design standards.	IV.C.2.I, IV.D.2.i, IV.G, IV.I.2

## F. Context Guidelines

DESIGN GUIDELINES	LOCATION (SECTION)
- Views of significant landmarks should be maximized in the design of mixed use developments.	<i>III.A, III.B, IV.I.1</i>
- Significant architectural styles existing in the area should be reflected in the selection of architectural styles for new mixed-use developments.	<i>IV.C.1-.2, IV.D.1-.2</i>
- Local street patterns should be considered and connected to as much as possible in the layout of mixed use developments.	<i>III.A, III.B, IV.F</i>
- Visitors should find mixed use developments interesting in appearance and buildings within these developments should be varied as much as possible yet based on a similar architectural style.	<i>IV.B, IV.C.1-.2, IV.D.1-.2</i>
- Existing historic sites, as well as the natural environment, should be incorporated and highlighted in the overall architectural and landscape design.	<i>III.A, III.B, IV.C.1-.2, IV.D.1-.2, IV.G, IV.H</i>



# VAIL CROSSINGS

## APPENDIX



# APPENDIX – VAIL CROSSINGS LETTER OF UNDERSTANDING

RMG Vail II LLC  
2441 North Leavitt Street  
Chicago, Illinois 60647  
(773) 364-2800

December 8, 2021

Thomas Drzazgowski  
Chief Zoning Inspector  
Pima County Development Services  
201 N. Stone Avenue  
Tucson, AZ 85701

Re: Vail Crossings – Letter of Understanding for DRC Proposal

Dear Tom:

Thank you for recently taking time to discuss Vail Crossings, the approximately 293-acre site owned by RMG Vail II LLC (“Developer”) and located near Benson Highway and Camino Loma Alta (the “Site”) in unincorporated Pima County (“County”). The Site is currently zoned CI-1, which allows for a Mixed-Use Option. As you know, the Developer and County staff (“Staff”) have engaged in numerous discussions regarding the proposed uses and design of Vail Crossings as shown on the enclosed Concept Plan (the “Project”) designed to comply with CI-1’s Mixed Use Option Design Guidelines (“Guidelines”). We greatly appreciate the County’s cooperation during this process.

Our goal is to present a Project development proposal (“Submittal”) to the Design Review Committee (“DRC”) that Staff will support. To that end, this letter memorializes the understandings that the Developer and the County have reached regarding the Project. Both the Developer and Staff (the “Parties”) understand that the items in this letter are a collaboration between the Parties, and the Staff Report will be consistent with this letter. The Parties also acknowledge that the DRC is the sole approval body for the Project’s conformance with the Guidelines, pursuant to Pima County Zoning Code (“Code”) § 18.99.030.A.5. The Developer will prepare the Submittal to show conformance with the Guidelines and present the Submittal to the DRC. While the County currently supports the Project, the Staff Report and conditions will be based both on the Submittal and the understandings described in this letter.

The following are the points of understanding agreed to by the Developer and County:

- 1) **Mixed Use Definition.** The County acknowledges that the Project meets the overall purpose of the Mixed-Use Option and generally meets the definition of Horizontal Mixed Use – Detached, pursuant to Code § 18.51.070.B.3. Developer acknowledges that while the Project is a mixed-use development, Developer must

Vail Crossings  
Letter of Understanding  
Page 2 of 5

show that the Submittal conforms with the Mixed-Use Option Guidelines, and that the DRC has the final authority to approve conformance with the Guidelines.

- 2) **Concept Plan.** Staff has reviewed the Concept Plan and is agreeable to recommending approval of the plan subject to review of the Submittal, as described in Sec. 3.
- 3) **Submittal.** Developer will prepare the Submittal to address how the Project will conform with the Guidelines. The Submittal will include exhibits such as, but not limited to, a circulation exhibit, building material and colors palette, and landscape/hardscape palette. The Submittal will also describe how the Project meets the Guidelines’ six Design Objectives: Layout; Parking, Vehicular and Pedestrian Circulation; Landscaping; Signage; Safety; and Context.

Developer will provide Staff a pre-Submittal copy for review. Staff will review the pre-Submittal and provide its comments to the Developer. Staff will not prepare its Staff Report until it has given the Developer the opportunity to enhance and revise the Submittal to address the pre-Submittal comments.

- 4) **Guideline Clarification.** Developer requests clarification of the Layout Guideline stating that certain industrial low occupancy uses “will not be counted in the evaluation of mixed-use functions.” Staff clarifies that low-occupancy industrial storage uses such as warehouses, commercial storage and non-business parking lots will not count as commercial uses under the Mixed-use Option, including the site coverage requirements of Sec. 18.51.070.D.2.a. However, larger scale, job creating commercial uses, including but not limited to distribution centers and manufacturing facilities, and self-storage uses are considered commercial and shall count towards the non-residential site coverage requirements of Sec. 18.51.070.D.2.a, and the phasing described in this letter, Sec. 4.
- 5) **Project Phasing.** The Parties agree that a residential population base is essential to attracting and keeping commercial businesses within a development area. Therefore, the Parties agree to the following phasing of the Project’s residential land area (“Residential Parcels”) and commercial land area (“Commercial Parcels”):
  - a. Developer<sup>1</sup> shall be permitted to develop up to fifty percent of the Residential Parcels prior to the sale or development of any portion of the Commercial Parcels. This does not limit the Developer from selling or developing any portion of the Commercial Parcels at any time.
  - b. The Developer shall be permitted to perform site work on any and all areas of the Residential Parcels in order to make the Commercial Parcels “Shovel Ready,” which is defined as a Commercial Parcel having the requisite paved

<sup>1</sup> The points of understanding in this letter run with the land and apply equally to any third-party purchaser of real property within Vail Crossings.

public access and all major utilities (sewer, water, gas, and electric) to its perimeter. Road and utility stub outs for future residential and commercial development areas are allowed. Onsite infrastructure improvements in residential areas must comply with the phasing structure herein, except to the extent said improvements are necessary in order to make the Commercial Parcels Shovel Ready.

- c. The next twenty-five percent of the Residential Parcels (over 50 percent to 75 percent) will be released for residential development upon the earlier of:
  - i. The sale of twenty-five percent of the Commercial Parcels to a user or preferred developer for a user; or
  - ii. The Developer making twenty-five percent of the Commercial Parcels Shovel Ready.
- d. The final twenty-five percent of the Residential Parcels will be released for residential development upon Developer making one hundred percent of the Commercial Parcels Shovel Ready.
- e. If at any time during the development of the Site twenty-five percent or more of the Commercial Parcels are developed with vertical construction, the entire area within the Residential Parcels shall be released for development.

6) Riparian In-lieu Fee.

- a. The Developer agrees to pay the requisite in-lieu fee to mitigate any riparian area located in the following areas (collectively the "In-lieu Parcels"):
  - The commercial property located southwest and adjacent to Block 2; and
  - The commercial property located southwest of Block 7.
- b. Upon payment of the riparian in-lieu fee, the In-lieu Parcel/s shall be considered commercial in their entirety regardless of whether they are eventually developed or left as open space. The in-lieu fee can be paid at any time prior to the final release of the residential land.
- c. The Developer shall have the right to study riparian areas A, B and C to determine whether it wishes to pursue a remapping of these riparian areas or pay an in-lieu fee to develop these riparian areas. In the event that the developer is successful in either remapping any of the riparian areas or paying an in-lieu fee to develop said areas, then, in such event, the Concept Plan will be adjusted to eliminate some open space as a result of the additional developable land that is created through remapping or paying an in-lieu fee provided, however, under no circumstances shall the residential portion of the site exceed fifty percent of the Total Net Acreage for the

site. Total Net Acreage is defined as the total gross acreage minus any acreage dedicated to open space. Any modifications to the Concept Plan based on the foregoing adjustments to the riparian areas shall not require DRC approval.

- 7) Native Plant Preservation ("NPP"): The Parties acknowledge that this Project will be phased over time, and that the applicable NPP plans will be submitted in conjunction with each subdivision plat submittal. The Parties also acknowledge that future phases may use any (or combination) of the NPP methods provided in Code § 18.72.090. If a future phase chooses to use the Set-aside Method (Sec. 18.72.090.A.3), the set-aside area must be located fully within the applicable development area (i.e., residential set-aside must be located within the Residential Parcels only, and commercial set-aside must be located within the Commercial Parcels only). The Riparian Areas shown on the Concept Plan shall not be used for Residential or Commercial NPP set asides.
- 8) Reports: The Parties acknowledge that Developer will submit its master drainage study and traffic impact analysis with the Submittal.
- 9) Future Concept Plan Changes: The Concept Plan is schematic in nature, and it is agreed that some modifications are likely to be necessary as the Concept Plan is further refined and developed. As allowed by Code § 18.51.070.E.3, any change in design or layout covering twenty-five percent or less of the Site does not require DRC approval. Such changes shall be approved by the Planning Official. Any change in design or layout of over twenty-five percent of the Site must be approved by the DRC.
- 10) Density Adjustments: Developer shall define the Project's permitted residential densities on the Concept Plan. The location of the residential densities identified on the Concept Plan are conceptual only and Developer may adjust the location of the residential densities within the defined residential area without further approval so long as the densities comply with the maximum density stated on the Concept Plan. Any increase in density greater than 10 percent of what is stated on the Concept Plan shall not be considered a material modification that requires DRC approval. In addition, the Parties acknowledge there are no minimum lot sizes applicable to the Project.
- 11) Approval Timeframe: Staff and Developer acknowledge that the Initial DRC approval will expire two years from the date of approval. Developer shall effectuate the DRC approval upon first subdivision plat approval and the Developer performing initial site work. Pursuant to Code § 18.51.070.E., Staff will extend the DRC approval for two additional years if the circumstances and basis for the DRC's decision have not significantly changed since the approval.

12) Transportation. Staff and Developer acknowledge that the County Transportation Department ("Transportation") will review the Project's traffic impact analysis ("TIA") and certain offsite improvements may be required, which could include Sundown Dr and Camino Loma Alta. Additional requirements by the Arizona Department of Transportation are likely, based on the TIA. These improvements will be determined separately from the DRC process, and will be approved as part of the site development process. The development, including any offsite improvements that may be required, will be subject to all applicable County and ADOT standards. The Parties acknowledge that the Developer may submit and present to, and obtain a decision from the DRC regarding its Submittal prior to finalizing any roadway requirements with Transportation.

If this letter accurately summarizes the understanding between the Developer and the County, please acknowledge your agreement by signing below. Thank you for your time and cooperation regarding the Project.

Very truly yours,  
RMG Vail II LLC

  
Sheldon J. Mandell  
Manager

Agreed:  
Pima County Development Services

  
Thomas Drzazgowski  
Chief Zoning Inspector

  
Carla Blackwell  
Director

Enclosure: Concept Plan

# CONCEPTUAL LAND USE PLAN | LOTTING CONCEPT

## RESIDENTIAL DESCRIPTION:

VAIL CROSSINGS FEATURES 461 SINGLE FAMILY LOTS  
 107,023 SQ FT ACRES OF VAIL CROSSINGS RESIDENTIAL LAND  
 1,000' BY 100' TO 1,500' BY 100' TO 2,000' BY 100' TO 2,500' BY 100'  
 HIGH DENSITY LAND USE - 7 TO 10 RACS. THE PROJECT MAY  
 FEATURE UP TO 720 TOTAL LOTS  
 135.53 AC / 230.07 (TOTAL SITE AC) = 46.24  
 155.53 AC / 274.95 (TOTAL SITE AC - RIPARIAN AREA) = 49.34

## RESIDENTIAL NOTES:

- 177 LOTS (43)
- 92 LOTS (41)
- 96 LOTS (48)
- 50 LOTS (44)
- 105 LOTS (62)
- 40 LOTS (44)
- 131 LOTS (58)

## RESIDENTIAL NOTES:

- LOTSIAMOUNT PARK IS INCLUDED WITHIN THE RESIDENTIAL AREA/ACREAGE AREA.
- DEVELOPER WITHIN REGULATORY AREA MAY WANT TO EXCLUDE SINGLE FAMILY RESIDENTIAL WITHIN THE HIGH DENSITY RESIDENTIAL AND VILLOTTE CORRIDOR WITHIN THE RESIDENTIAL AREA/ACREAGE AREA AT TIME OF PLAN.

## OPEN SPACE DESCRIPTION:

VAIL CROSSINGS FEATURES 34.52 ACRES OF OPEN SPACE WHICH INCLUDES RIPARIAN HABITAT, ORNAMENTAL, AND REMAINT LAND. THERE ARE THREE RIPARIAN CORRIDORS WITHIN THIS SITE, WHICH TOTAL 16.43 AC.

## COMMERCIAL DESCRIPTION:

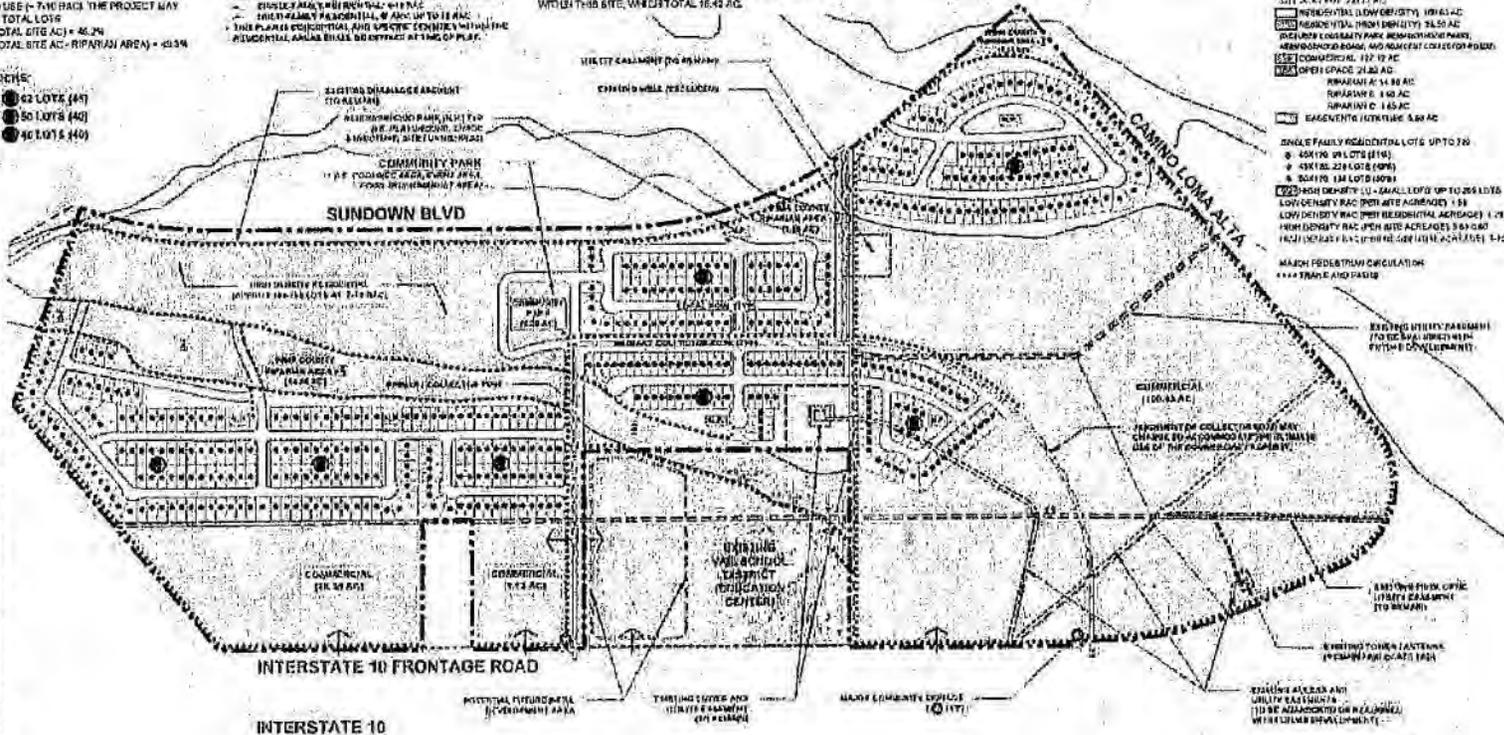
VAIL CROSSINGS FEATURES 197.13 ACRES OF COMMERCIAL LAND (1,411,111).

## NOTES

APPROXIMATE PIMA COUNTY  
 197.13 ACRES (1,411,111 SQ FT)  
 200' WIDE CH 10 RAMP INDUSTRIAL  
 DEVELOPMENT PLAN BY  
 DATA THROUGH CONCEPT

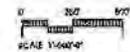
- 177 LOTS (43)
- 92 LOTS (41)
- 96 LOTS (48)
- 50 LOTS (44)
- 105 LOTS (62)
- 40 LOTS (44)
- 131 LOTS (58)

## ILLUSTRATIVE RENDERING



## VAIL CROSSINGS

PROJECT NAME: VAIL CROSSINGS  
 (ALL NAME PLANS) (INTERCONCEPT) (DWG)



## VAIL CROSSINGS MIXED-USE OPTION RESTRICTIONS

1. No more than three garage dominant homes shall be provided in a row.
2. No more than three two-story homes shall be provided in a row unless the subdivision is made up entirely of two-story homes.
3. The second story on two-story homes shall have articulations and diversity in the plane.
4. Residences along corner lots shall extend architectural detail/ornamentation from the front of the house to the visible side of the house. Stucco pop-outs, wall veneers, and decorative paint schemes shall wrap corners.
5. Side yard fences on corner lots shall begin at least as far back from the street frontage as the back of the house unless privacy concerns arise. AC units shall be screened if not enclosed by fences.
6. No more than three adjacent residences shall feature the elevation.
7. A minimum of two garage door types with different colors, styles, or materials shall be provided in each subdivision.
8. Residential roofs shall not feature asphalt or wood shingles.
9. Each home within a subdivision shall feature one base color and at least two accents (color, veneer, faux shutters, etc.).
10. No more than two homes in a row shall feature the same palette of colors.
11. Decorative rock mulch shall be provided in every yard and consistent throughout the neighborhood (block).
12. The front yards of each lot in a subdivision shall feature one tree, two shrubs, and two accent ground covers.
13. Builder walls shall use integral colored blocks for masonry walls.
14. Nonresidential buildings shall feature a variety of roof lines, roof pitches, parapet heights, or accents.
15. Nonresidential buildings shall feature one base color and at least two accents (colors, veneer, furring, awnings, etc.).
16. Vinyl siding is prohibited.
17. Neighborhood entries shall include at least two decorative building materials or artwork.
18. The community park shall feature at least two decorative building materials or artwork.
19. A public art component shall be provided in the community park.