





**Spring Creek 7FL6
EAA Chapter Meeting**

**Lessons Learned
Flying Vintage
Hyper-Sonic Gliders**



**Ken Cameron
Colonel, USMC (Ret)
NASA Astronaut (Former)**

21 May 2026













05

H&MS 12

MARINES

4339

05

WA

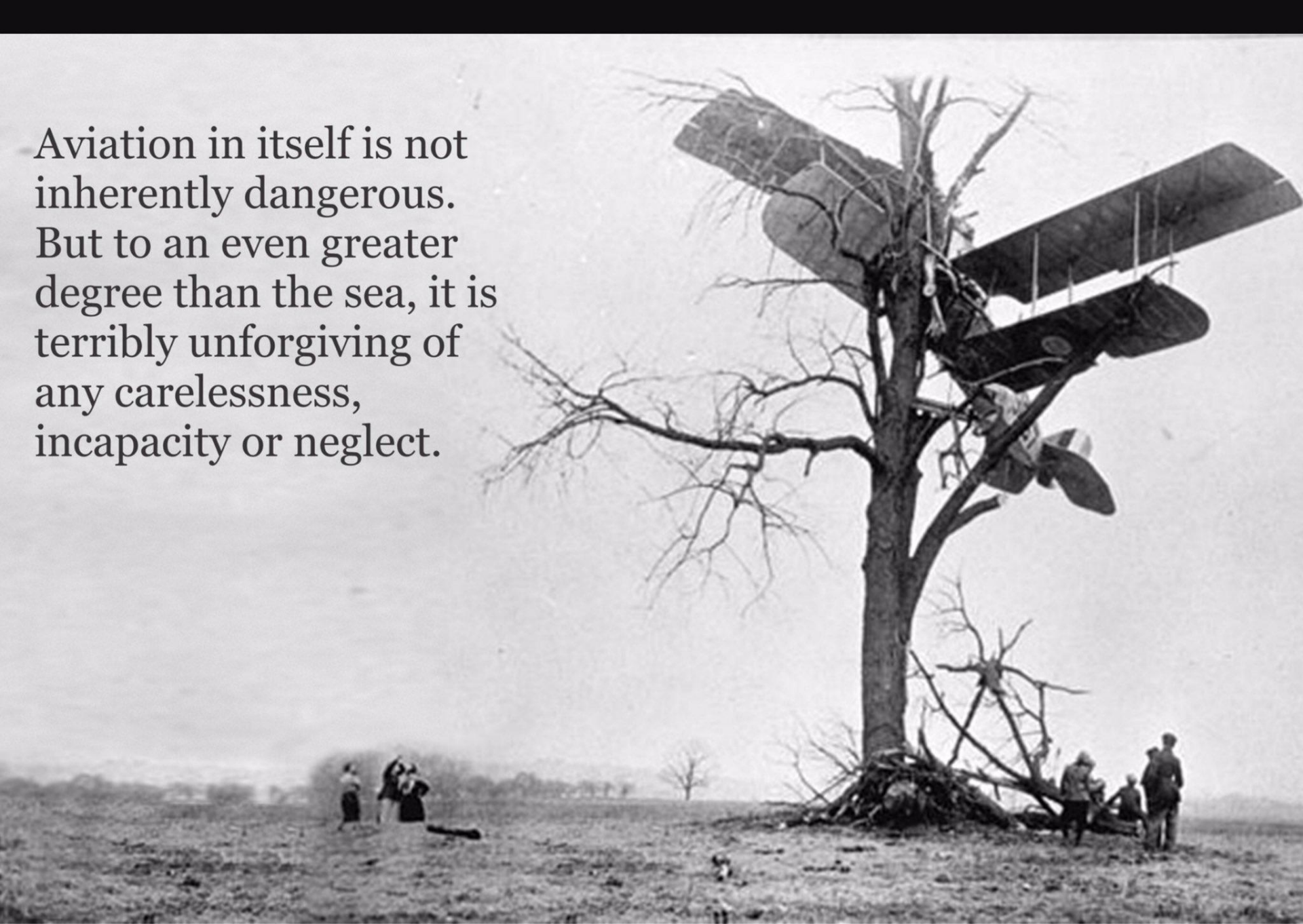
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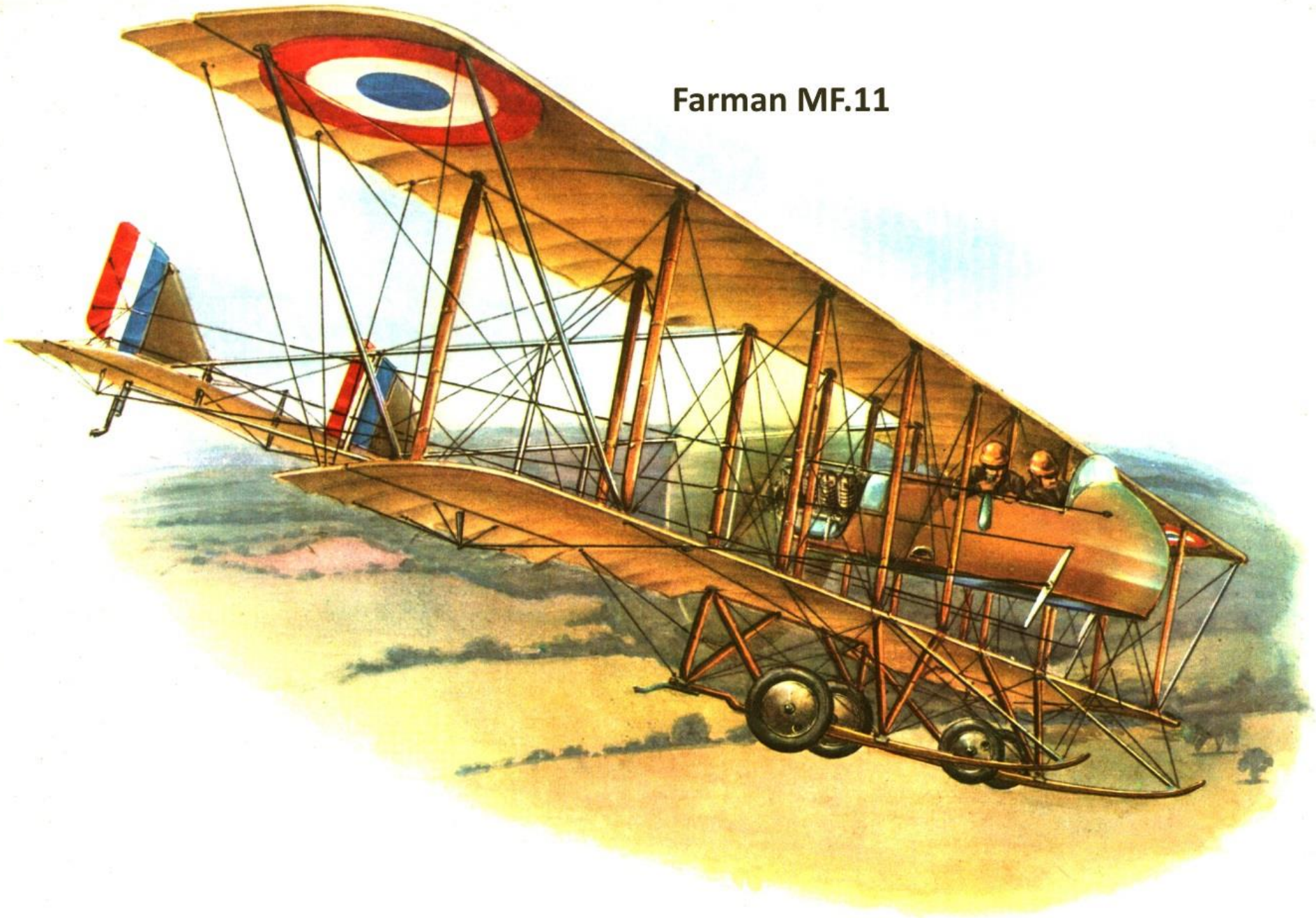




Aviation in itself is not inherently dangerous. But to an even greater degree than the sea, it is terribly unforgiving of any carelessness, incapacity or neglect.



Farman MF.11



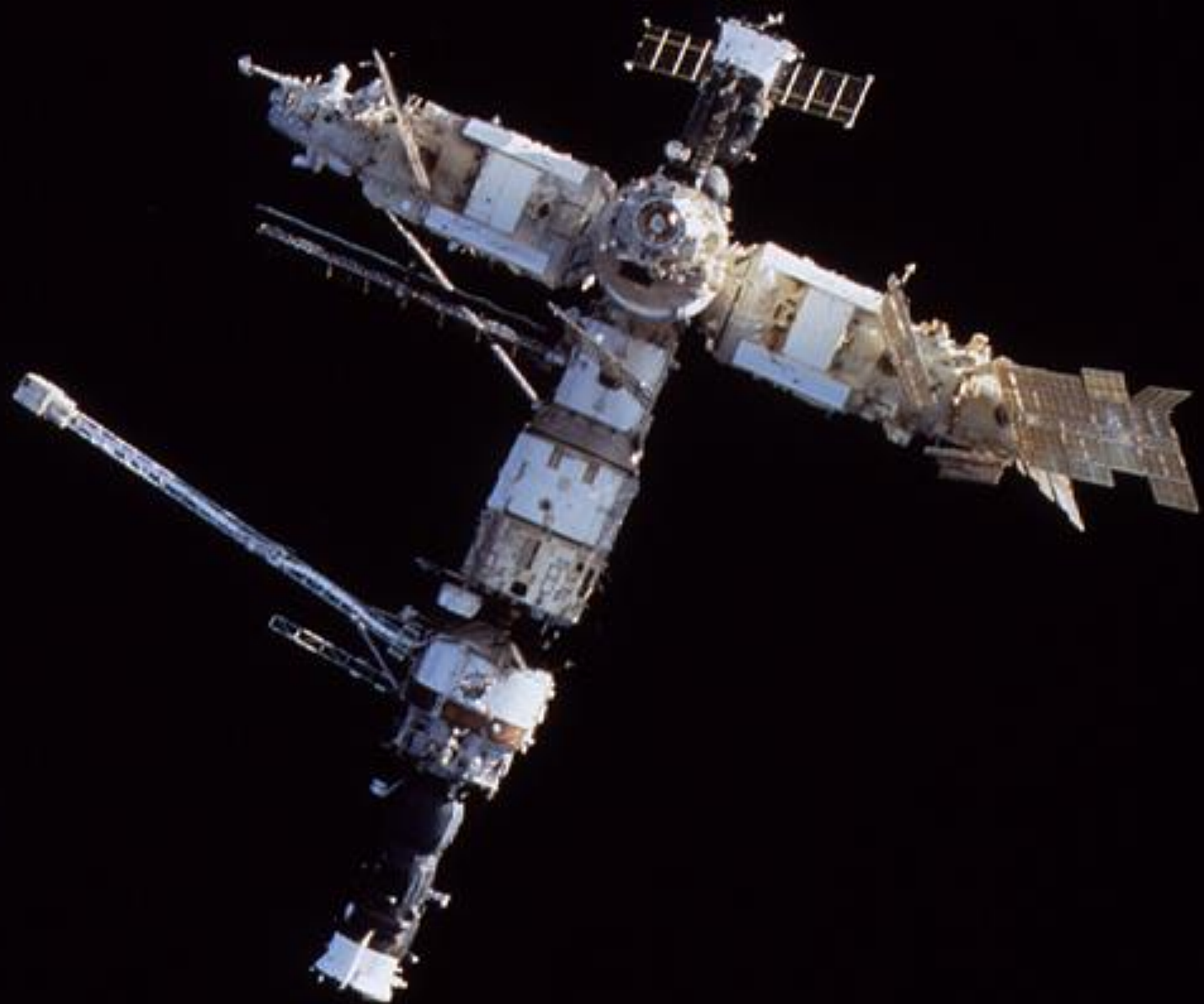


Lieut.
Alfred Gilmer Lamplugh,
J.R.C.,
4095 8 Dec. 1916

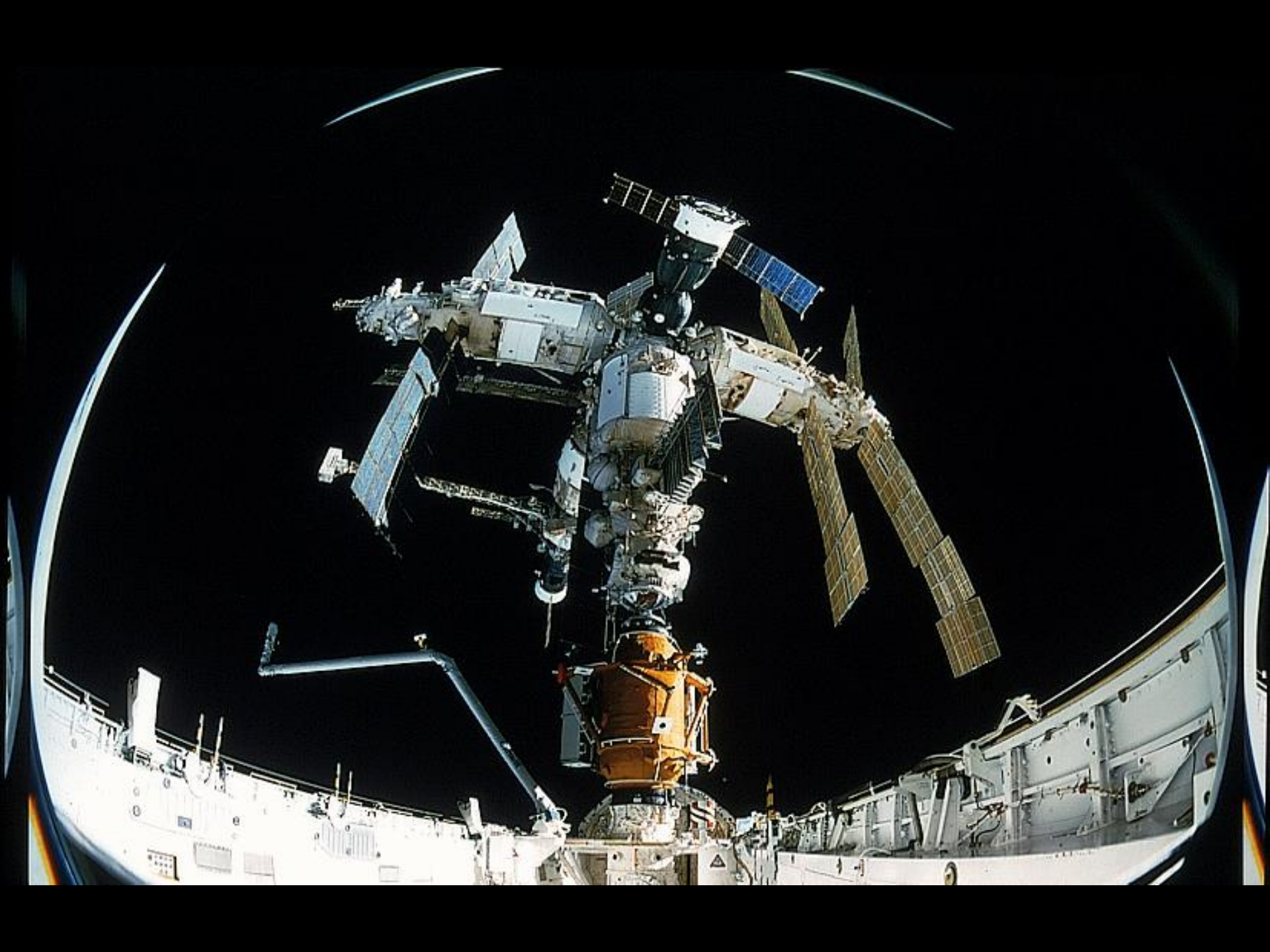












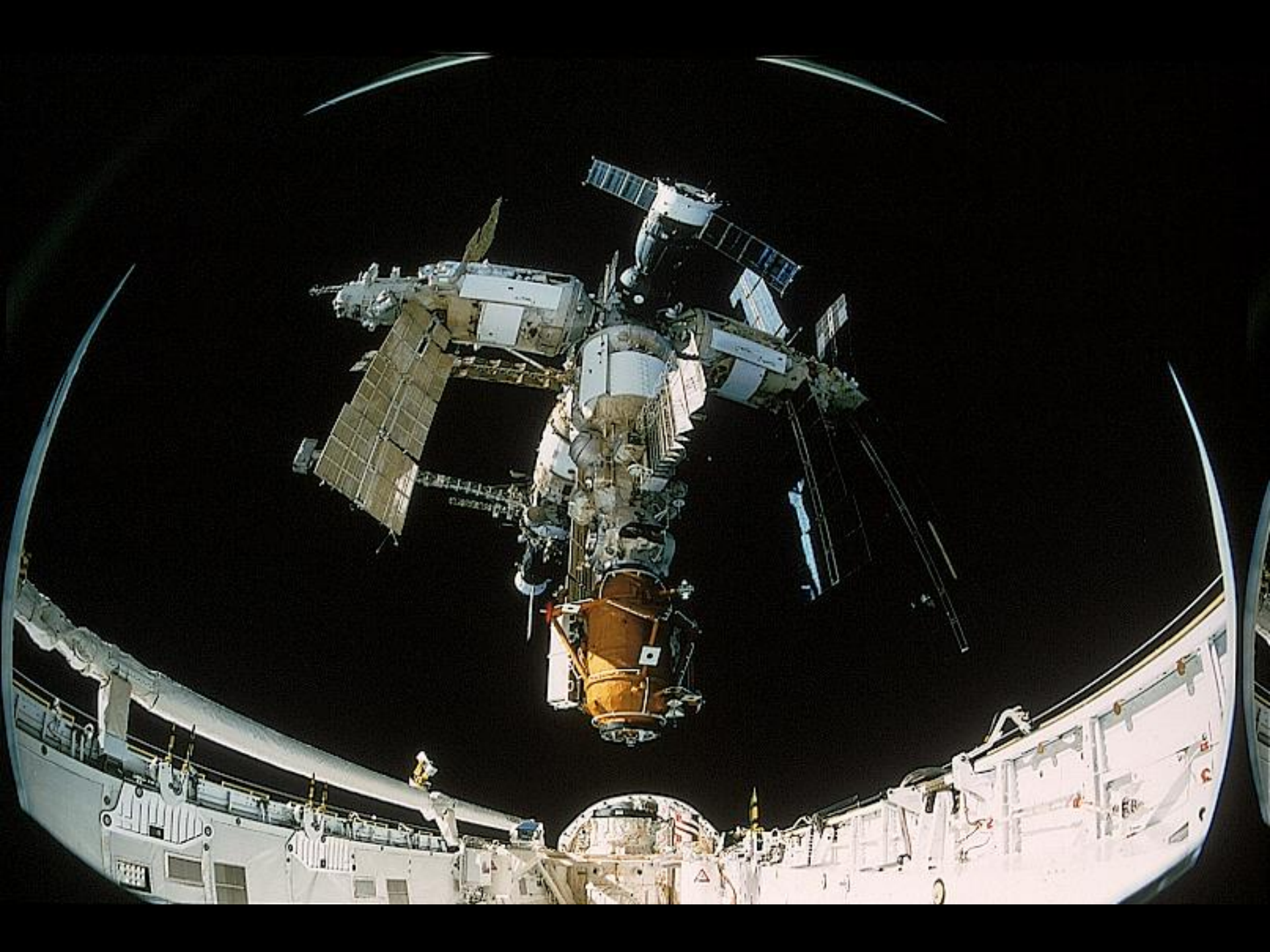




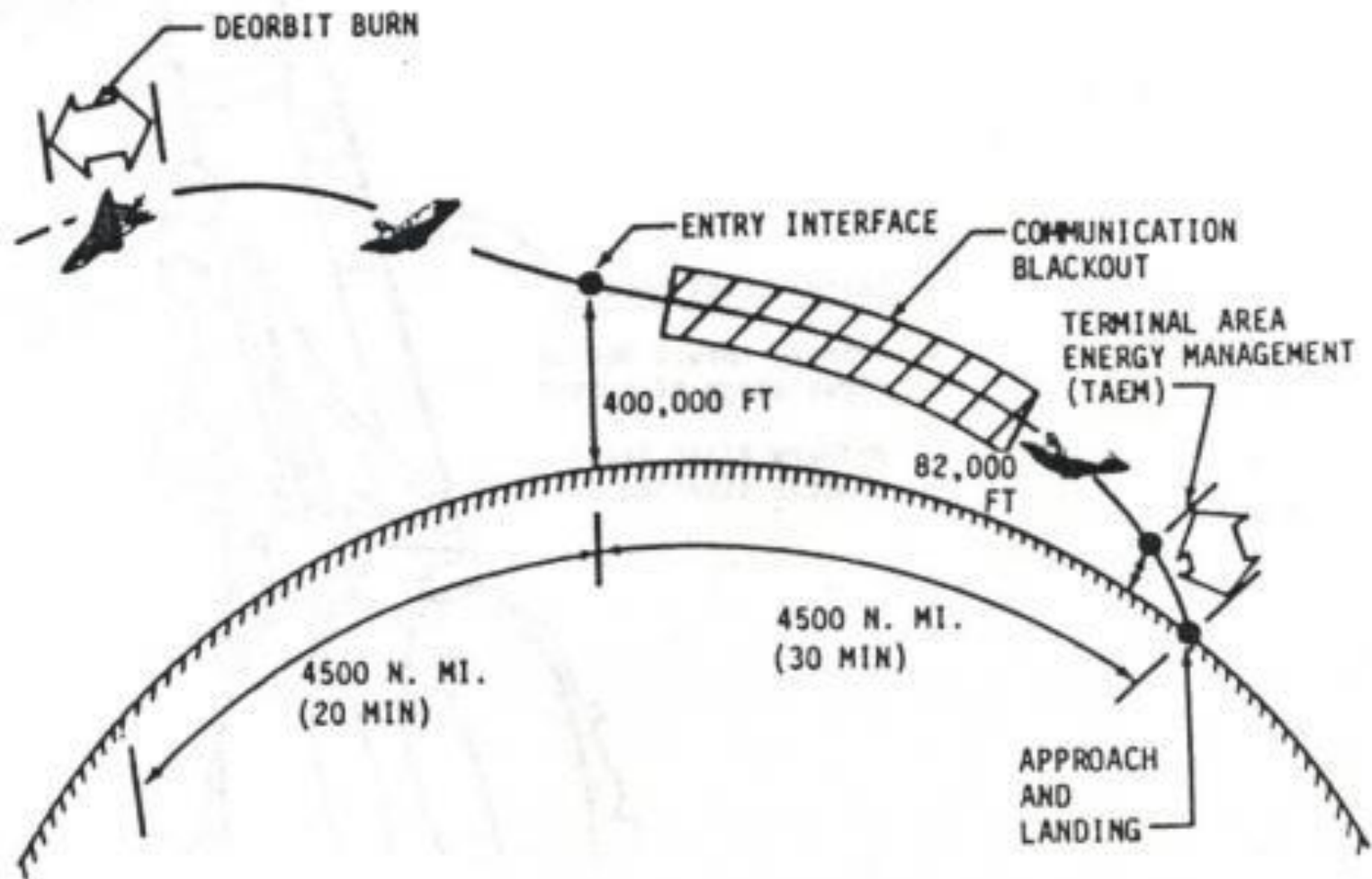












Nominal entry trajectory

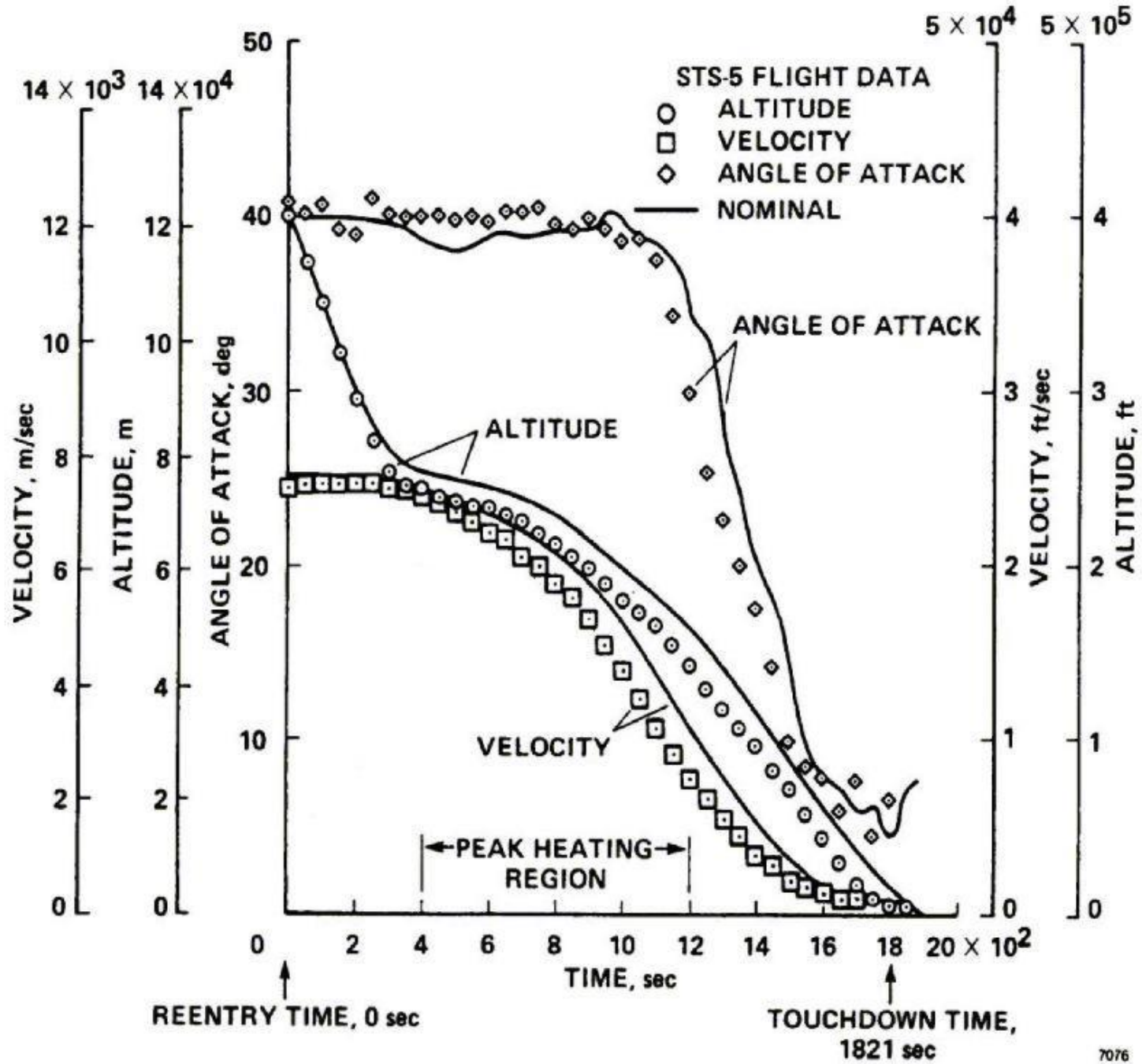


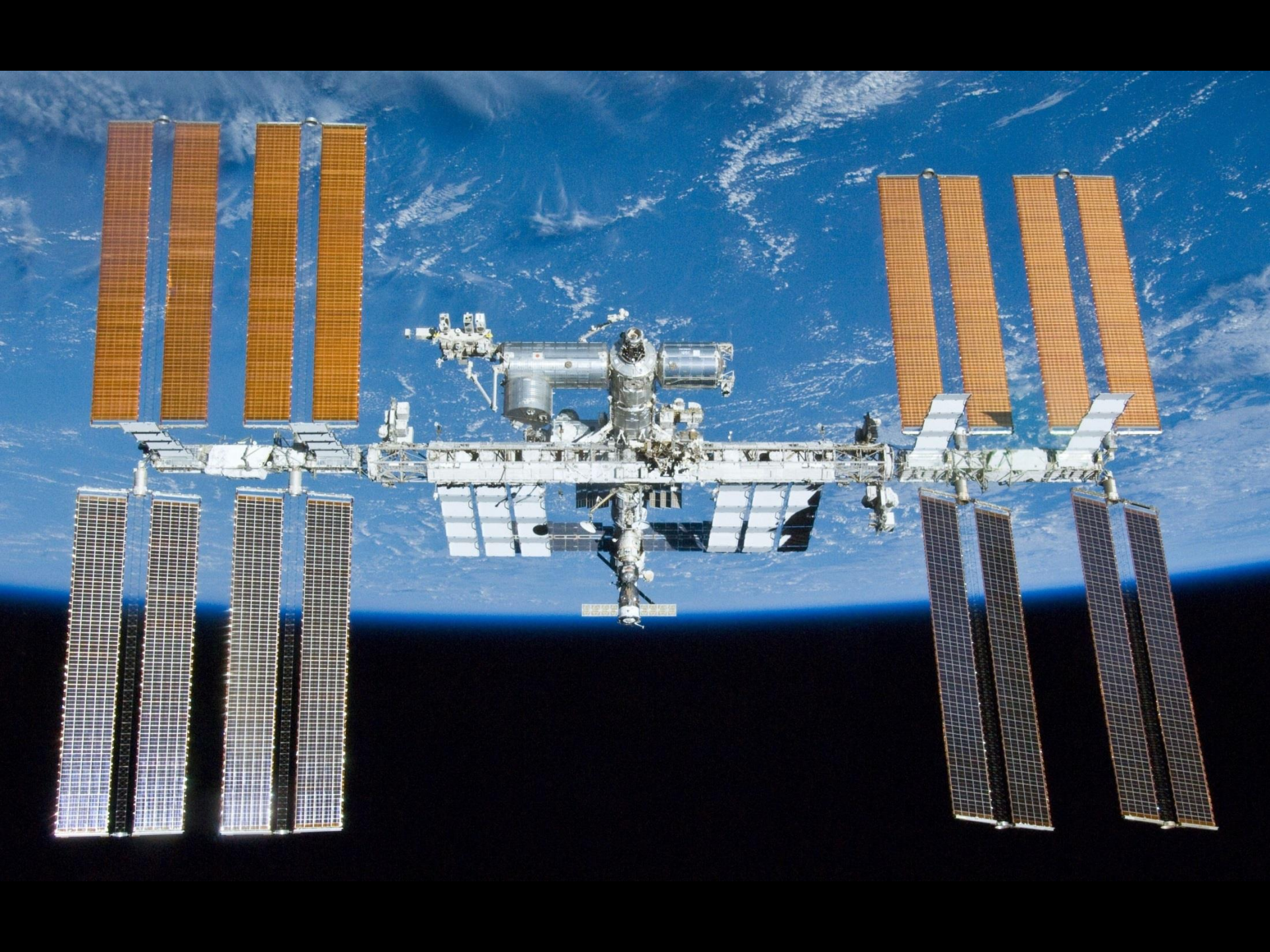
Figure 2. STS-5 trajectory (ref. 14).



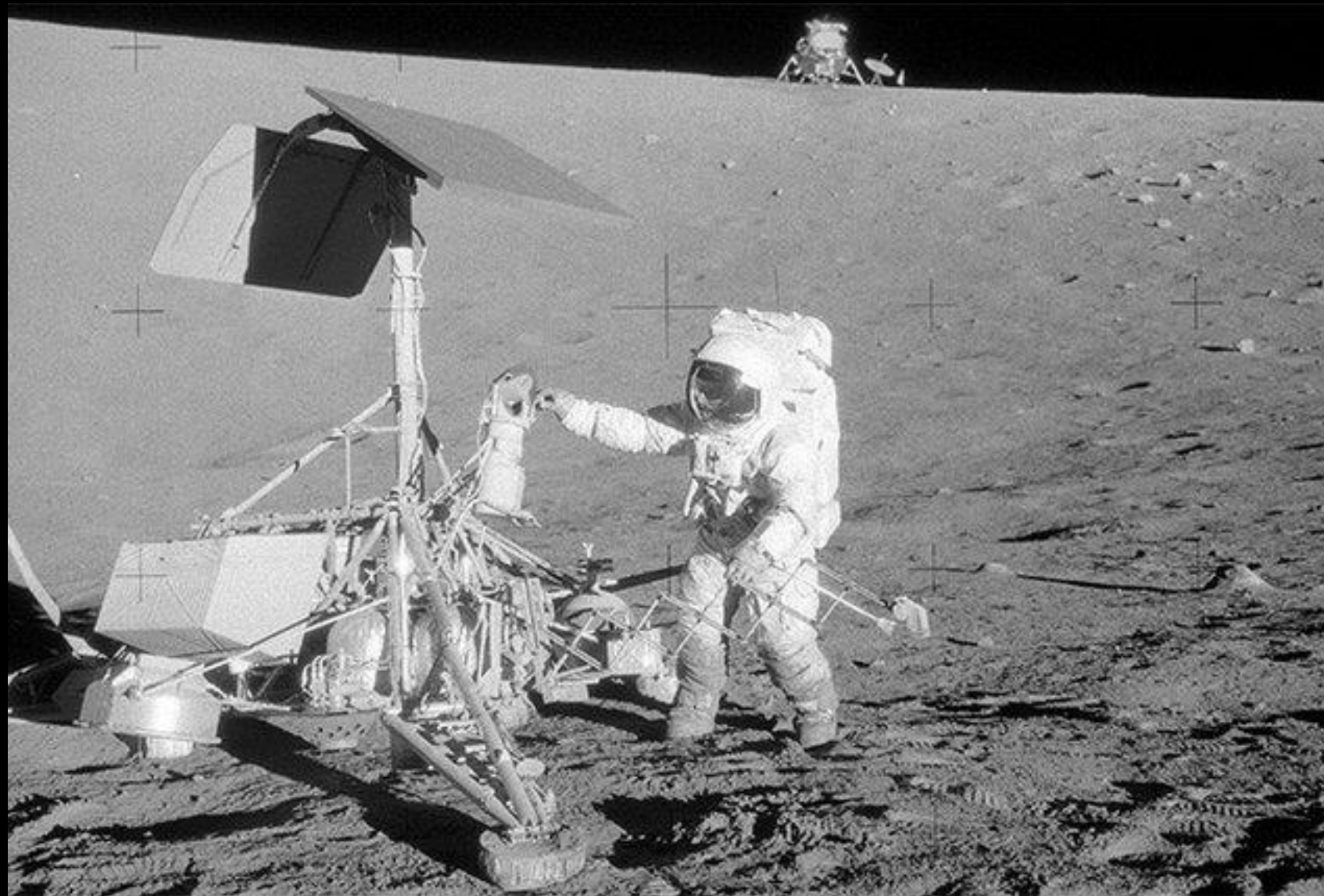














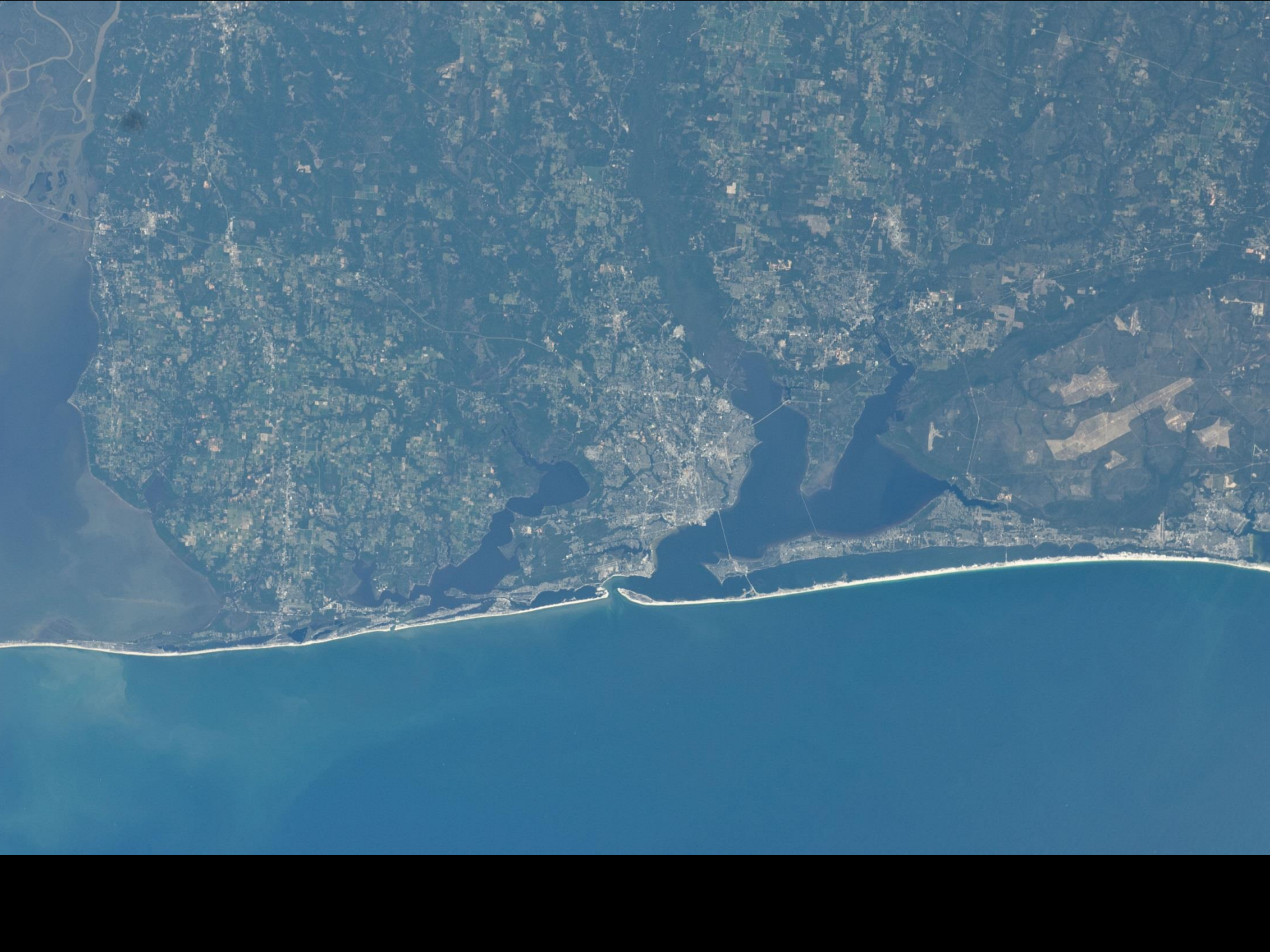
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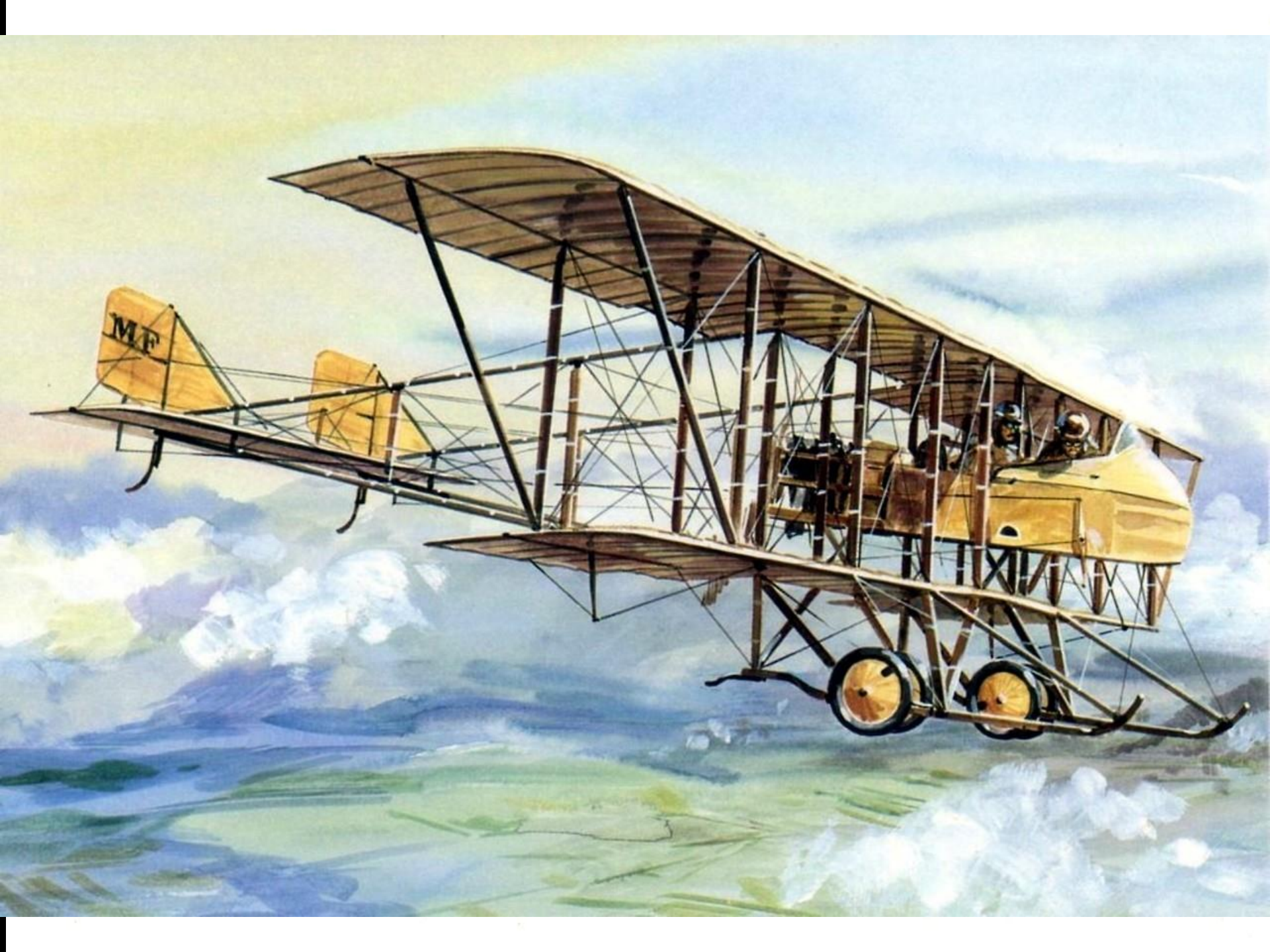
21 May 2026





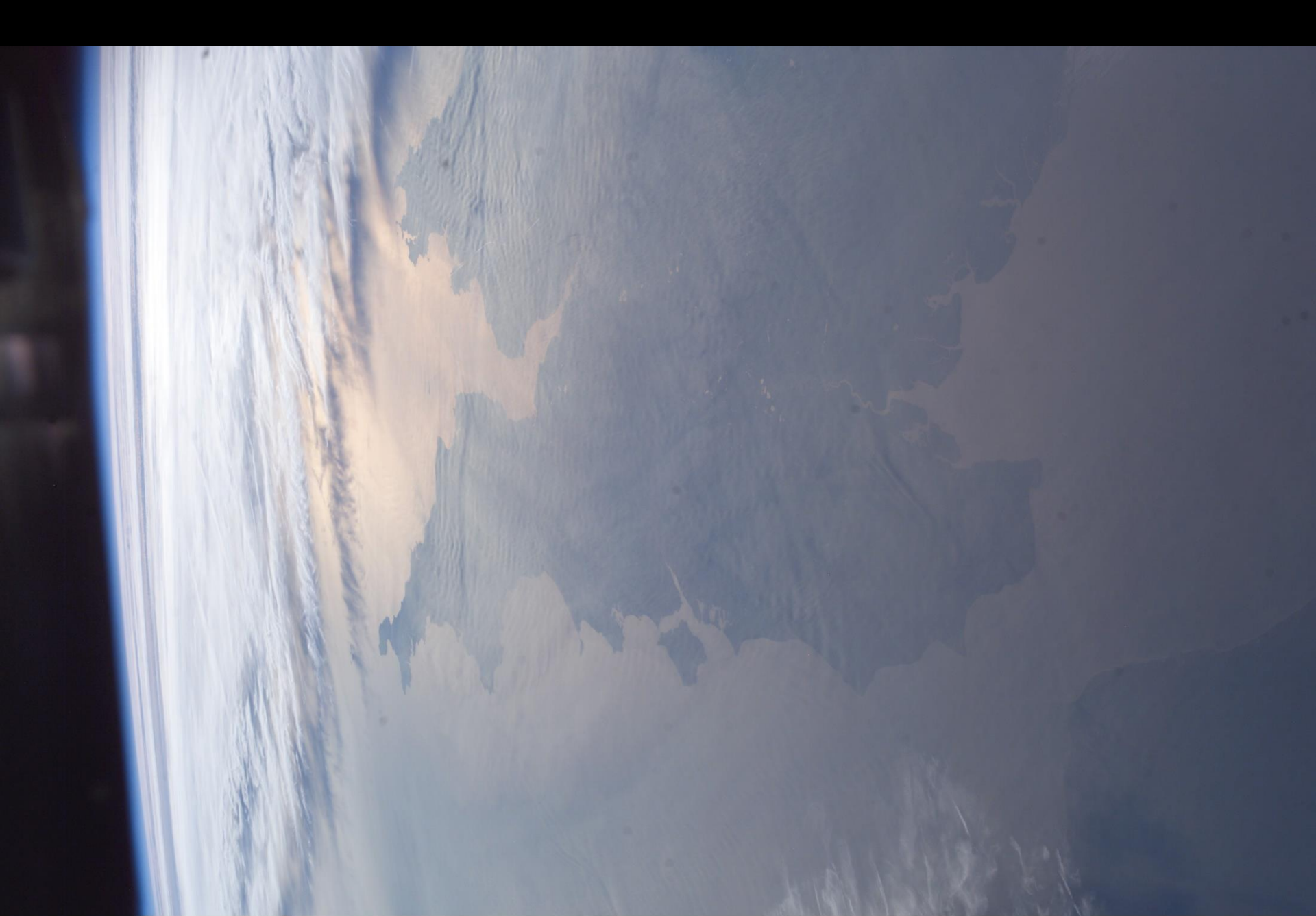


CAPT. ALFRED GILMER LAMPLUGH, C.B.E., F.R.Ae.S., M.I.Ae.E., F.R.G.S., "Lamps" to his friends, is one of the best-known figures in British aviation. He learned to fly in 1913, and after serving in the R.F.C. and the R.A.F. he became a commercial pilot. Later he began to specialize in aviation insurance and to-day he has an unrivalled knowledge of the business; indeed, it is largely due to him that Britain became the world centre of it. Lamps himself always tested aircraft before insuring them, until recent years, thus getting an intimate knowledge of the risk involved and incidentally accumulating a unique piloting experience. His hobby is motor yachting, which accounts for his rig in our picture. The festooning aircraft are some of those which passed through Lamps's hands, and they range from the venerable open-cockpit biplane, bottom left, clockwise through the years, as it were, to the Comet bottom right





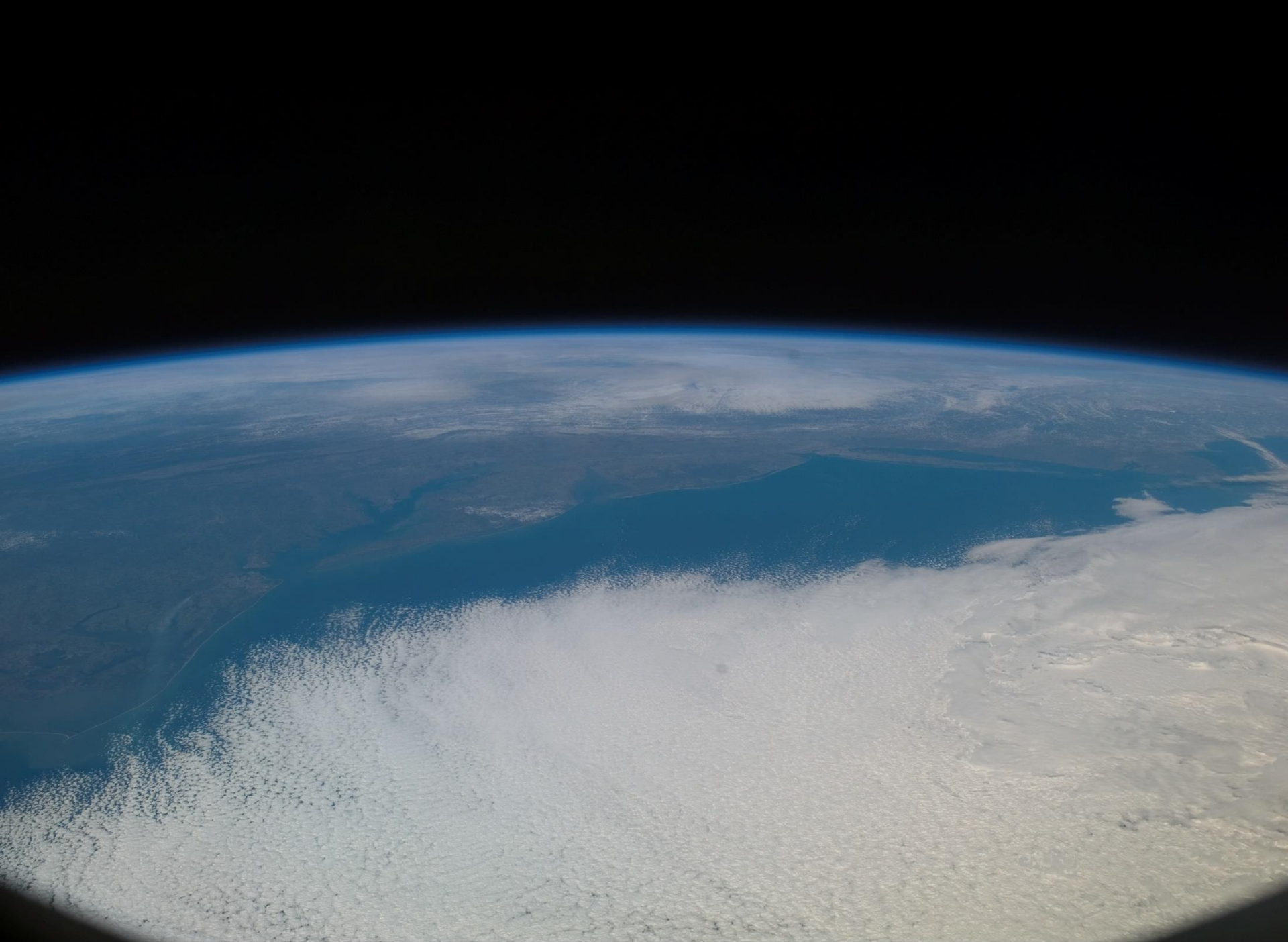




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