

Concept | Shepherd Turn

Pedestrian safety and local traffic management through speed reduction and public realm improvements

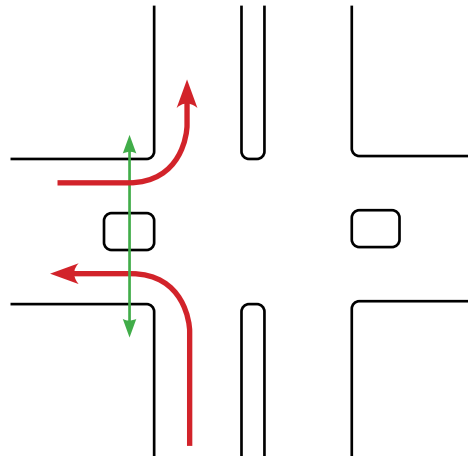
Existing Conditions

Wide median intersection (30m wide road reserve)

Typical condition in neighbourhoods such as West Melbourne

The current state facilitates:

- High entry speed for turning vehicles from the primary and secondary roadways
- Long crossing distances for vulnerable road users, with ambiguous crossing priority (refer road Victorian road rules)
- Large roadway surface area for relatively low-traffic streets
- Contributes to urban heat island effects due to unnecessary hard surface area.



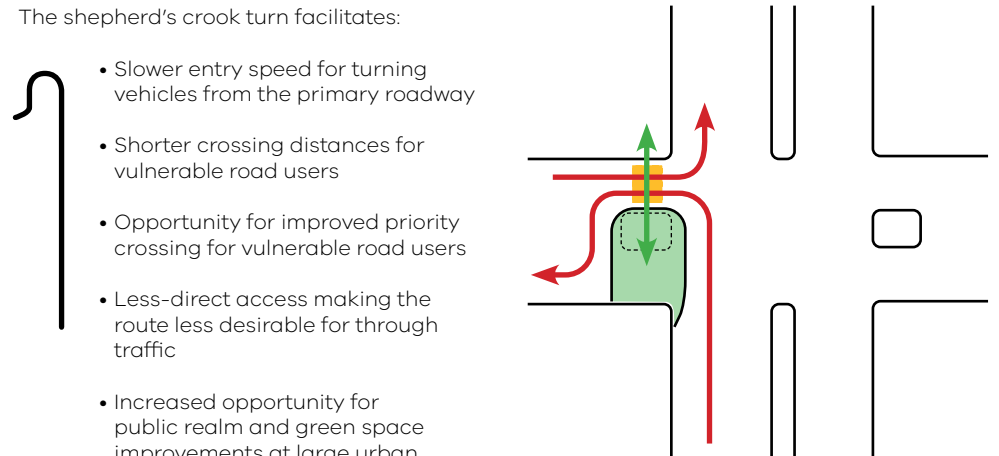
Proposed Treatment

Shepherd's 'crook' turn

Slower, safer and less desirable turns into local streets

The shepherd's crook turn facilitates:

- Slower entry speed for turning vehicles from the primary roadway
- Shorter crossing distances for vulnerable road users
- Opportunity for improved priority crossing for vulnerable road users
- Less-direct access making the route less desirable for through traffic
- Increased opportunity for public realm and green space improvements at large urban intersections
- Step change toward future street narrowing and introduction of linear parks



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Exemplar Neighbourhood
West Melbourne

Legend

- Priority Treatment Site (Declared Road)
- Possible Treatment Site (Local Road)

Exemplar Site | Rosslyn Street at Spencer Street

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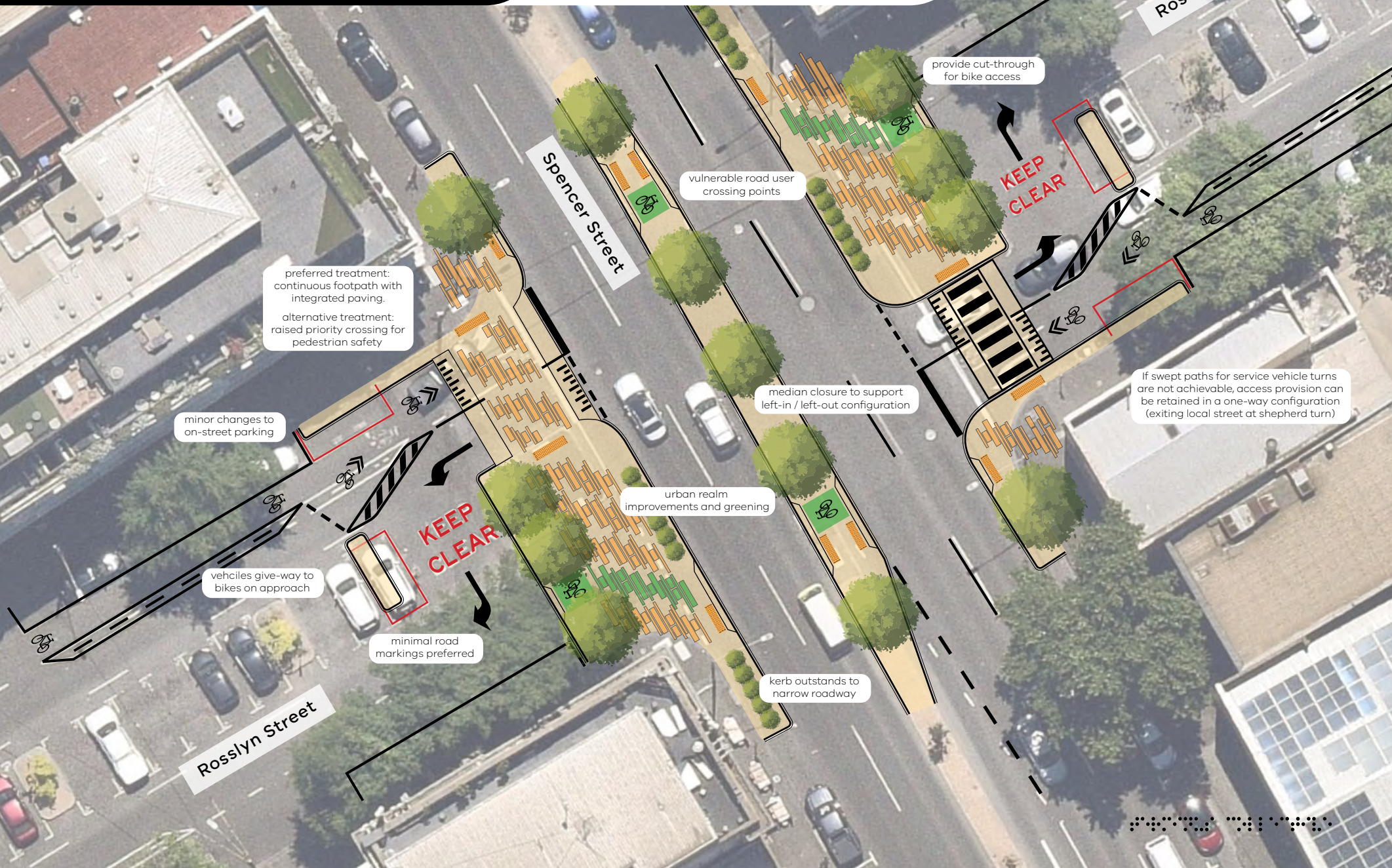
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Configuration A

Tight turn configuration suitable for primary roadways with lower operating speed



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Configuration B

Provision of deceleration lane for primary roadway with higher operating speed

