

Haulaway Loading and Securement Standards

These requirements are to be followed when handling any FCA US products. All specific requirements for each model are listed on the Vehicle Loading Sheets. There are exceptions to these general rules contained within the individual sheets, making it imperative that any person handling an FCA US vehicle follows the individual vehicle loading instructions.



Apparel:

- No exposed metal – zippers, buttons, rivets on jeans, watches, chains, rings, etc.
- DO NOT wear gloves when inside vehicle.
- Safety apparel must be worn in every yard.

Loading/Unloading:

- Decks / Ramps must be free of debris, chains, straps, tie-down hooks, etc.
- Decks must be set as level as possible to prevent damage to the rocker panel, front fascia or undercarriage.
- All folding mirrors should be folded inward; must use power fold button when available.
- **DO NOT** mix chain tie-downs with strap tie-downs on the same vehicle.
- Emergency brakes must be set.
- Transmissions must be placed in ‘Park’ if automatic or 1st gear if manual.
- Keys placed in the cup holder or center console. If it is a fold down cup holder please leave it open for key storage.
- Do not rub up against, lean on, or sit on a vehicle at any time.
- All doors & windows must be closed during transport on a truck.

Securement Requirements on Haulaway Trucks:

- All vehicles must be secured using a strap tie system except; Wrangler, Wrangler Unlimited, Gladiators and all RAM Pickups.
- Basket type strap is acceptable, but must run parallel with the tire tread, it cannot pull inward/outward.
- Lasso straps are NOT acceptable.
- All decks must be level to prevent rocker panel damage at the break-over points.
- Ensure proper skid position / setup to prevent front fascia damage.
- Slow speed is essential when loading low profile models.

Only acceptable Haulaway tie-down methods:

Strap (must run parallel with tire)



“R” hook



Rail Loading and Securement Standards

These requirements are to be followed when handling any FCA US products. All specific requirements for each model are listed on the Vehicle Loading Sheets. There are exceptions to these general rules contained within the individual sheets, making it imperative that any person handling an FCA US vehicle follows the individual vehicle loading instructions.



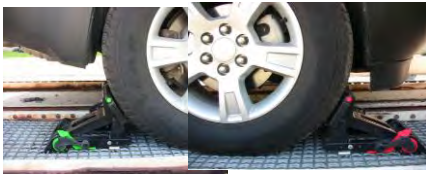
Apparel:

- No exposed metal – zippers, buttons, rivets on jeans, watches, chains, rings, etc.
- DO NOT wear gloves when inside vehicle.
- Safety apparel must be worn in every yard.

Loading/Unloading:

- No Passengers allowed, except for training purposes.
- Decks, ramps, and bridge plates must be free of debris, chocks, etc.
- Chocks can be placed on the railcar deck prior to loading, provided they are out of drive path.
- Railcars cannot have a variance of more than 4” in deck height and a spotter is required for a variance over 3”.
- Loading is not permitted in cases where ramp extends above the deck by more than one 1”.
- All folding mirrors should be folded inward; must use power fold button when available.
- Must not exceed 5mph on the ramp or in the railcar.
- Emergency brakes must be set.
- Transmissions must be placed in ‘Park’ if automatic or 1st gear if manual.
- Do not rub up against, lean on, or sit on a vehicle at any time.
- Keys placed in the cup holder or center console. If it is a fold down cup holder please leave it open for key storage.
- Chocks should be set at the maximum height allowable without causing damage to the vehicle or violating the AAR standard for clearance from vehicle body to chock.
- Maintain a minimum clearance of 3” between vehicles and 5” between vehicles and end doors. When possible maintain 5” between vehicles and 7” between vehicles and end doors.
- Do not open trunk, hood, or any doors, other than driver’s door on rail.
- All vehicles on Bi-level railcars must have a minimum of 4 chocks. Trucks must have a minimum of 6 chocks with the additional chocks placed on inboard side of the tires. If running boards are present additional chocks can be placed on the rear tires of the unit.
- All vehicles on Tri-level railcars must have 2 tires chocked.

Bi-level
Chock
Type



- Grate Lock
- Grip Lock
- Stay Put

Tri-level Thrall
Chock



Tri-level Chock
Type



(Co-Polymer)

“TTM” Through Track
Mandrel



Ocean Loading and Securement Standards

These requirements are to be followed when handling any FCA US products. All specific requirements for each model are listed on the Vehicle Loading Sheets. There are exceptions to these general rules contained within the individual sheets, making it imperative that any person handling an FCA US vehicle follows the individual vehicle loading instructions.



Apparel:

- No exposed metal – zippers, buttons, rivets on jeans, watches, chains, rings, etc.
- DO NOT wear gloves when inside vehicle.
- Safety apparel must be worn in every yard.

Loading/Unloading:

- Ramps and traffic patterns must be free of debris, lashing straps, etc.
- All folding mirrors should be folded inward; must use power fold button when available.
- Must not exceed 5mph on the ramp or in the vessel.
- Emergency brakes must be set.
- Transmissions must be placed in ‘Park’ if automatic or 1st gear if manual.
- Keys placed in the cup holder or center console. If it is a fold down cup holder please leave it open for key storage.
- Do not open trunk, hood, or any doors, other than driver’s door on the vessel, or at any time during transport.
- Do not rub up against, lean on, or sit on a vehicle at any time.
- When wheel lashing;
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- When using fixed loops as a tie down point vehicle must be lashed in front and rear on opposite sides.
- Do not carry any tools, straps, etc. between or around vehicles while on the vessel.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Vehicle	Length		Width		Height W/O Accessories		SAE Cargo Area (Behind 2nd Row)		SAE Cargo Area (max)		Curb Weight
	Inches	Millimeters	Inches	Millimeters	Inches	Millimeters	Cu. Ft.	Liters	Cu. Ft.	Liters	
Alfa Romeo 4C	157.1	3990	73.5	1868	59.2	1184	N/A	N/A	3.7	105	2,465 Coupe

Guidelines for Enclosed Carrier:

- **Vehicle is restricted to enclosed carrier only.**
- Strap/Soft tie securement only on these models.
- There is a high potential for fascia and rocker panel damage. Make sure all ramps / decks are as level as possible.
- **SLOW** speed is essential because this is a low profile model:
 - 3 mph during loading and unloading
 - 6 mph during handling in the yards
- A stop condition is required when entering or exiting the ramp.
- The front suspension is locked in place by spring blocks. Steering wheel must not be turned over 360° to prevent blocks to come out of position.
- Do not remove full body cover during transport.

Ocean unloading:

- At destination port, rubber mats are required to eliminate the drop between the ramp and the ground.

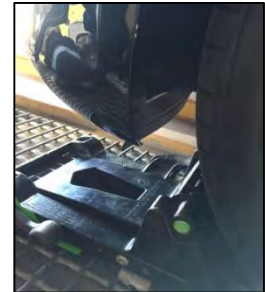
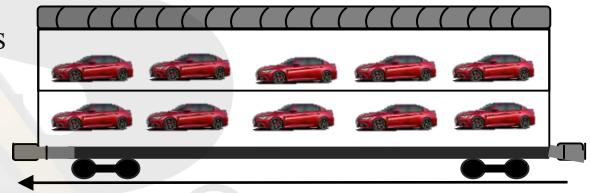
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Weights & Dimensions	Overall Length (in)	Width with Folded Mirrors (in)	Overall Width with Mirrors (in)	Overall Width Without Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight	Max* Weight	Approach Angle	Departure Angle	Breakover Angle
					Highest	Lowest						
Giulia	182.8	73.5	79.7	73.2	56.2	55.7	111.0	3430	3655	12.9°	16.9°	11.0°
Giulia Ti	182.9	73.5	79.7	73.2	56.2	55.7	111.0	3454	3678	12.2°	15.7°	11.0°
Giulia Quadrifoglio	182.6	73.5	79.7	73.2	55.8	55.7	111.0	3735	3828	11.4°	11.5°	9.3°

Guidelines for Rail Transport:

- Loading is restricted to bi-level railcars, unless authorization is given by FCA US Logistics to load on Tri-levels.
- Securement is restricted to Grate Lock Chocks ONLY.
 - Front wheel chock position – Low setting
 - Rear wheel chock position – Mid setting
- Vehicles are to be uniformly positioned on decks (A = 5, B = 5) to maximize the distance between vehicles and between vehicles and end doors.
- All chocks must be carefully installed from the side of the vehicle, never from the front.
- A minimum of 3" is required between vehicles and 5" between vehicles and end doors.
- For tri-level loading, a spotter is required on A-Deck when the chock tie-down track is on right side of vehicle to assist/guide driver to position vehicle for proper securement application and prevent vehicles tire/rims from contact/rubbing against chock tie-down track.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.



Guidelines for Haulaway Transport:

- **Giulia Quadrifoglio:**
 - Restricted to enclosed carrier only.
 - Will be equipped with both front and rear spring blocks. Steering wheel must not be turned over 360° to prevent blocks from coming out of position.
 - Do not remove full body cover during transport.
- **All Other Models:**
 - Are to ONLY be transported on the top deck (3 units), excluding the head rack and ONLY the last position (1 unit) on the lower deck. All forward loaded.
 - Strap/Soft tie securement only on these models.
 - **There is a high potential for rocker panel damage, so ensure all ramps/decks are as level as possible.**
 - Properly set skid position to prevent front fascia damage.
 - A stop condition is required when entering or exiting the ramp.
 - **SLOW** speed is essential because this is a low profile model. 5



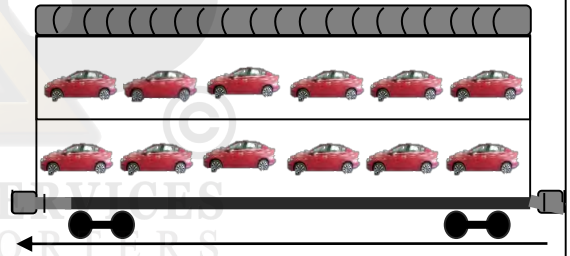
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



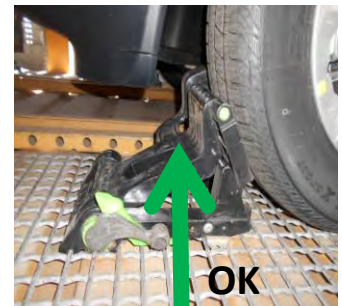
Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors (in)	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight (lbs.)	Max* Weight (lbs.)	Approach Angle	Departure Angle
					Highest	Lowest					
Giulietta Veloce	171.4	71.2	71.8	70.8	57.7	57.7	103.8	3075	4023	13°	15°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- A minimum of 3” roof clearance must be maintained.
- Units must be positioned 3” bumper to bumper and 5” between bumper and end door to allow for a load factor of 6 / deck and to maintain adequate spacing. **This is very critical, as there is no room for error.**
- Vehicles are to be uniformly positioned on decks (A = 6, B = 6) to maximize the distance between vehicles and between vehicles and end enclosures.
- Front and rear chocks must be placed in the high setting.
- All chocks must be carefully positioned from the side of the vehicle, never from the front (see photo).
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- Do not exceed 5 mph / 8 kmh speed limit when loading and unloading and utilize caution inside the railcar to avoid damage.

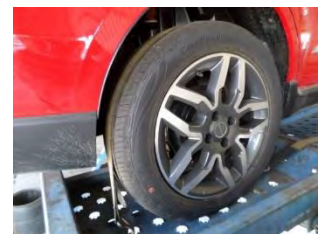


Make sure to install chock from the side.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.
- Make sure all decks are as level as possible to prevent rocker panel and / or front fascia damage.
- Properly set skid position to prevent front fascia damage.
- **SLOW** speed is essential because this is a low profile model.



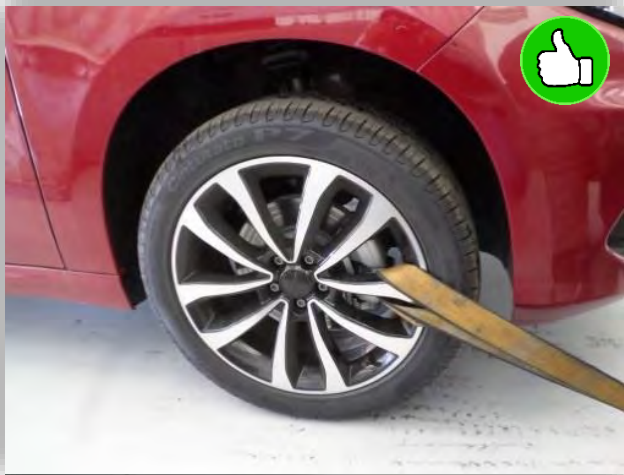
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



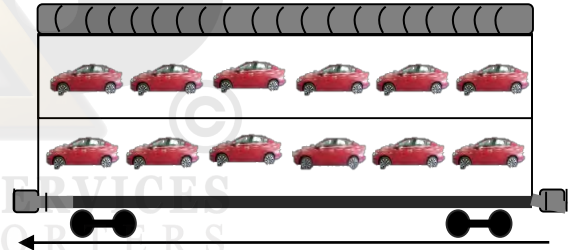
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors (in)	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight (lbs.)	Max* Weight (lbs.)	Approach Angle	Departure Angle
					Highest	Lowest					
Mito Progression Luxury	160.1	68.2	69.1	67.8	57.0	57.0	98.9	2348	3439	13°	15°
Mito Veloce	160.1	68.2	69.1	67.8	57.2	57.2	98.9	2348	3439	13°	15°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- A minimum of 3” roof clearance must be maintained.
- Units must be positioned 3” bumper to bumper and 5” between bumper and end door to allow for a load factor of 6 / deck, and to maintain adequate spacing. **This is very critical, as there is no room for error.**
- Vehicles are to be uniformly positioned on decks (A = 6, B = 6) to maximize the distance between vehicles and between vehicles and end doors.
- Front and rear chocks must be placed in the high setting.
- All chocks must be carefully positioned from the side of the vehicle, never from the front (see photo).
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- Do not exceed 5 mph / 8 kmh speed limit when loading and unloading and utilize caution inside the railcar to avoid damage.



Make sure to install chock from the side.



Guidelines for Haulway Transport:

- Strap/Soft tie securement only.
- Make sure all decks are as level as possible to prevent rocker panel and / or front fascia damage.
- Properly set skid position to prevent front fascia damage.
- **SLOW** speed is essential because this is a low profile model.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Vehicle	Length		Width		Height W/O Accessories		SAE Cargo Area (Behind 2nd Row)		SAE Cargo Area (max)		Curb Weight
	Inches	Millimeters	Inches	Millimeters	Inches	Millimeters	Cu. Ft.	Liters	Cu. Ft.	Liters	
Alfa Stelvio	184.5	4686	74.9	1903	65.0	1652	28.1	795	55.3	1565	3,963 AWD GME4

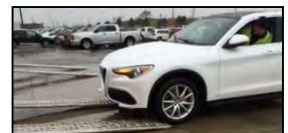
Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Units must be positioned 3” bumper to bumper and 5” between bumper and end door to allow for a load factor of 5/deck and to maintain adequate spacing.
- Vehicles are to be uniformly positioned on deck (A = 5, B = 5) to maximize the distance between vehicles and enclosures.
- Chock Specifications:
 - No AVR’s on front wheels
 - No Block Chocks on rear wheels
 - Grate-Lock Chocks should be placed in the mid position
- All chocks must be carefully positioned from the side of the vehicle, never from the front.
- Do not exceed 5 mph / 8 kmh speed limit when loading and unloading.
- Utilize caution inside the railcar to avoid damage.



Guidelines for Haulaway Transport:

- **Stelvio**
 - Restricted to enclosed carrier only.
 - Do not remove full body cover during transport.
- **All Other Models:**
 - Load all positions on both top and bottom decks EXCLUDING BEHIND THE CAB.
 - If the bottom deck is in lower position, BACK THE VEHICLE ON. This is essential for proper clearance.
 - Strap/Soft tie securement only on these models.
 - Properly set skid position to prevent front fascia damage.
 - A stop condition is required when entering or exiting the ramp.
 - **SLOW** speed is essential during loading process.



Please note:

- Do NOT use the red string to lower/close the tailgate.
- The string must only be used to disengage the latch when the tailgate is closed.



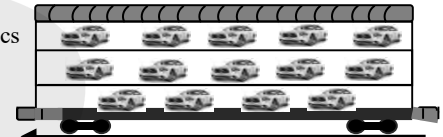
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Vehicle	Length		Width		Height W/O Accessories		SAE Cargo Area (Behind 2nd Row)		SAE Cargo Area (max)		Curb Weight
	Inches	Millimeters	Inches	Millimeters	Inches	Millimeters	Cu. Ft.	Liters	Cu. Ft.	Liters	
LA Challenger	197.9	5027	75.7	1923	57.5	1460	16.2	458	N/A	N/A	3,865 ATX V6
LX—300	199.2	5059	74.9	1902	58.3	1481	16.3	461	N/A	N/A	3,983 RWD V6
LD—Charger	198.4	5040	75.0	1905	58.2	1479	16.5	467	N/A	N/A	4,015 RWD V6

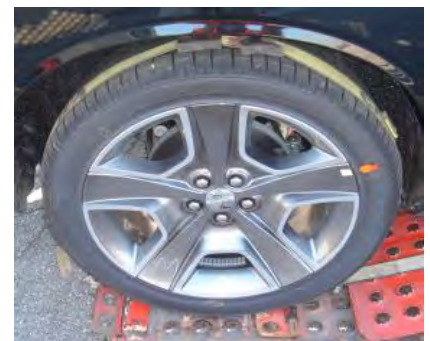
Guidelines for Rail Transport:

- Loading is restricted to Tri-level railcars, unless authorization is given by FCA US/CAN Logistics to load on Bi-level's. If loading on a Bi-level the front chock height should be set in the low position and the rear chock in the high position, always maintain two inches of clearance between the chock and the closest point on the vehicle.
- Vehicles are to be uniformly positioned on decks (A= 4, B=5, C = 5) to maximize the distance between vehicles and between vehicles and end doors.
- A minimum of 3" required between vehicles and 5" between vehicle and end doors.
- Minimum of 3" roof clearance must be maintained.
- Position the vehicle on Tri-levels with tires no closer than half an inch (1/2") to the tie down rail, optimal spacing is 1-2 inches.
- When the chock tie-down track is on right side of vehicle, a spotter is required to assist/guide driver to position vehicle for proper securement application and prevent vehicles tire/rims from contact/rubbing against chock tie-down track.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- A stop condition is required when either entering or exiting the ramp.
- To avoid damage to the front fascia, please drive **VERY SLOWLY** up and down ramps.



Guidelines for Haulaway Transport:

- Hellcat/Daytona/Scat Pack units are restricted to only 4 units per load.
 - The last 3 positions on the upper deck.
 - The last position on the bottom deck.
 - Use only certified equipment (Next Gen / or models that ensure NO damage for low profile vehicles).
- Strap/Soft tie securement only on these models.
- The SRT models utilize very low front fascia, it is imperative to properly set skid position to prevent front fascia damage.
- High potential for rocker panel damage, make sure all ramps / decks are as level as possible.
- A stop condition is required when entering or exiting the ramp.
- Front fascia clearance is minimal. Please drive **VERY SLOWLY** while loading vehicle.



*****NEVER DRIVE UNIT IN REVERSE ONTO TRAILER*****

This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp and grounding the unit.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low clearance vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

ACCELERATED SERVICES
AUTO TRANSPORTERS

Wheel lash in front



Wheel lash in rear



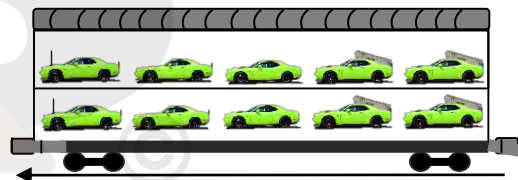
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Vehicle	Length		Width		Height W/O Accessories		SAE Cargo Area (Behind 2nd Row)		SAE Cargo Area (max)		Curb Weight
	Inches	Millimeters	Inches	Millimeters	Inches	Millimeters	Cu. Ft.	Liters	Cu. Ft.	Liters	
LA DR22 SRT Widebody	197.9	5027	78.5	1993.9	57.0	1448	16.2	458	NA	NA	4,334 RWD V8

Guidelines for Rail Transport - Widebody:

- Widebody Hellcats CANNOT load on ANY tri-level equipment. Loading is restricted to **Bi-level railcars ONLY**.
- A minimum of 3" required between vehicles and 5" between vehicle and end doors.
- When loading **Bi-Level**, use **ONLY** the **Holden Grip Lock Chock**.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- A stop condition is required when either entering or exiting the ramp.
- To avoid damage to front fascia, drive **VERY SLOWLY** up & down ramp.



Guidelines for Haulaway Transport - Widebody:

- **Bottom Deck:** restricted to last position only.
 - Widebody is never to load into the belly.
- **Top Deck:** restricted to last three positions only.
- The SRT models utilize very low front fascia, it is imperative to properly set skid position to prevent front fascia damage.
- High potential for rocker panel damage, make sure all ramps/decks are as level as possible.
- A stop condition is required when entering or exiting the ramp.
- Front fascia clearance is minimal. Please drive **VERY SLOWLY** while loading vehicle.



*****NEVER DRIVE UNIT IN REVERSE ONTO TRAILER*****

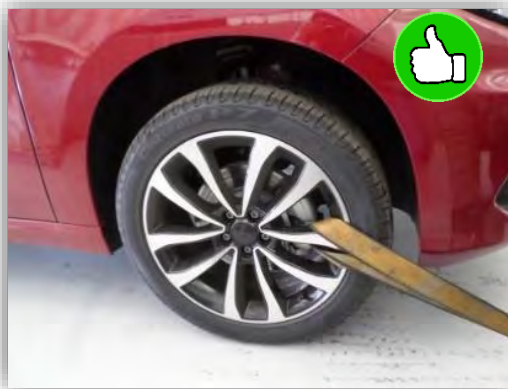
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Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



Rail & Truck Handling, Loading and Securement Standards for Shipping

Dodge Challenger Red Eye and 1320 with Drag Radials

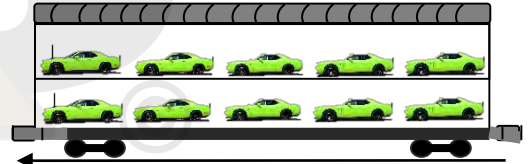
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Vehicle	Length		Width		Height W/O Accessories		SAE Cargo Area (Behind 2nd Row)		SAE Cargo Area (max)		Curb Weight
	Inches	Millimeters	Inches	Millimeters	Inches	Millimeters	Cu. Ft.	Liters	Cu. Ft.	Liters	
Dodge Red Eye	197.9	5027	75.7	1923	57.5	1460	16.2	458	NA	NA	4,451 RWD V8
1320 Challenger with Drag Radials	197.9	5027	75.7	1923	57.5	1460	16.2	458	N/A	N/A	4,232 RWD V8

Guidelines for Rail Transport

- Restricted to enclosed haulaway during inclement weather conditions.
- Red Eye Challengers CANNOT load on Tri-level equipment.
- Loading is restricted to **Bi-level railcars ONLY**.
- A minimum of 3" is required between vehicles and 5" between vehicles and end doors.
- When loading **Bi-Level**, use **ONLY** the **Holden Grip Lock Chock**.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- A stop condition is required when either entering or exiting the ramp.
- To avoid damage to front fascia, drive **VERY SLOWLY** up & down ramp.



Guidelines for Haulaway Transport - Widebody:

- Black Matte Red Eye and the 1320 Drag Radials are restricted to enclosed carriers during inclement weather conditions.
- Bottom Deck: restricted to last position only. Widebody is never to load into the belly.
- Top Deck: restricted to last three positions only.
- High potential for rocker panel damage, make sure all ramps / decks are as level as possible.
- A stop condition is required when entering or exiting the ramp.
- Front fascia clearance is minimal. Please drive **VERY SLOWLY** while loading vehicle.



*****NEVER DRIVE UNIT IN REVERSE ONTO TRAILER*****

Ocean Handling, Loading and Securement Standards for Shipping the Dodge Challenger Red Eye and 1320 with Drag Radials

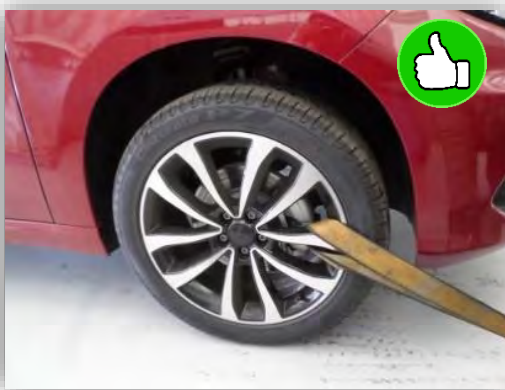
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



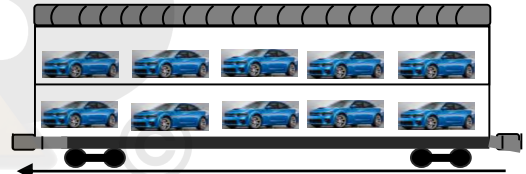
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Vehicle	Length		Width		Height W/O Accessories		SAE Cargo Area (Behind 2nd Row)		SAE Cargo Area (max)		Curb Weight
	Inches	Millimeters	Inches	Millimeters	Inches	Millimeters	Cu. Ft.	Liters	Cu. Ft.	Liters	
Charger Widebody	201	5105.4	78.3	1988.8	58.4	1483.6	N/A	N/A	N/A	N/A	4,591.2 RWD V8

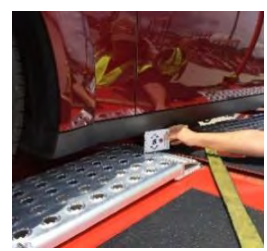
Guidelines for Rail Transport - Widebody:

- Widebody Chargers CANNOT load on ANY tri-level equipment. Loading is restricted to **Bi-level railcars ONLY**.
- A minimum of 3” required between vehicles and 5” between vehicle and end doors.
- When loading **Bi-Level**, use **ONLY** the **Holden Grip Lock Chock**.
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- A stop condition is required when either entering or exiting the ramp.
- To avoid damage to front fascia, drive **VERY SLOWLY** up & down ramp.



Guidelines for Haulaway Transport - Widebody:

- **Bottom Deck:** restricted to last position only. Widebody is never to load into the belly.
- **Top Deck:** restricted to last three positions only.
- The SRT models utilize very low front fascia, it is imperative to properly set skid position to prevent front fascia damage.
- High potential for rocker panel damage, make sure all ramps / decks are as level as possible.
- A stop condition is required when entering or exiting the ramp.
- Front fascia clearance is minimal. Please drive **VERY SLOWLY** while loading vehicle.



*****NEVER DRIVE UNIT IN REVERSE ONTO TRAILER*****

This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front

Wheel lash in rear



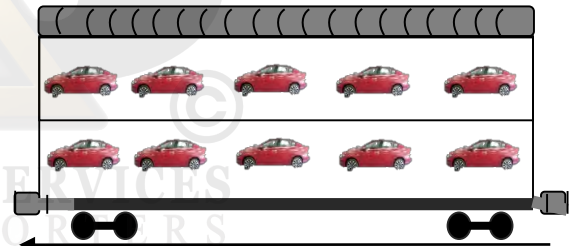
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



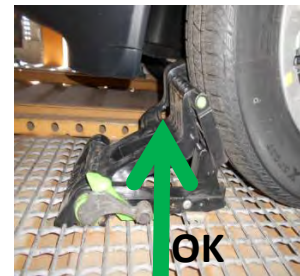
Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors (in)	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight (lbs.)	Max* Weight (lbs.)	Approach Angle	Departure Angle
					Highest	Lowest					
Neon SE/ SXTAT	178.6	71.2	71.8	70.6	59.0	59.0	103.9	2657	3759	17°	17°
Neon SE/ SXTMT	178.6	71.2	71.8	70.6	59.0	59.0	103.9	2535	3638	17°	17°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- A minimum of 3” roof clearance must be maintained.
- Units must be positioned 3” bumper to bumper and 5” between bumper and end door to allow for a load factor of 6 / deck and to maintain adequate spacing. This is very critical, as there is no room for error.
- Vehicles are to be uniformly positioned on decks (A = 5, B = 5) to maximize the distance between vehicles and between vehicles and end enclosures.
- Front and rear chocks must be placed in the high setting.
- All chocks must be carefully positioned from the side of the vehicle, never from the front (see photo).
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- Do not exceed 5 mph / 8 kmh speed limit when loading and unloading and utilize caution inside the railcar to avoid damage.



Make sure to install chock from the side.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.
- Make sure all decks are as level as possible to prevent rocker panel and / or front fascia damage.
- Properly set skid position to prevent front fascia damage.
- **SLOW** speed is essential because this is a low profile model.



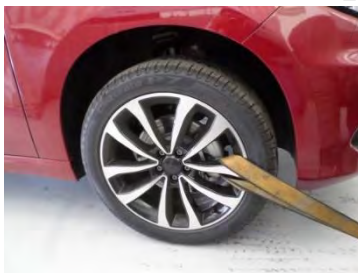
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

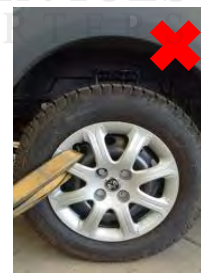
Wheel lash in front



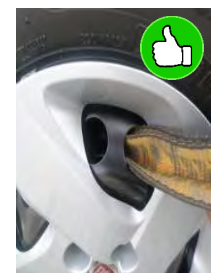
Wheel lash in rear



Wheel lash for plastic rim cover



NO



OK

- Hooks and lashings should **NEVER** touch the wheel cover.
- Lashing strap should be installed going from the inside through the wheel arm and come out towards the operator.

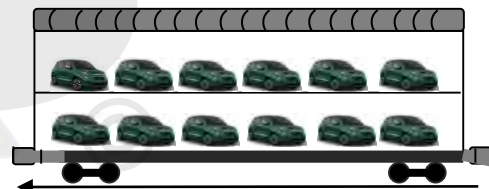
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Vehicle	Length		Width		Height W/O Accessories		SAE Cargo Area (Behind 2nd Row)		SAE Cargo Area (max)		Curb Weight
	Inches	Millimeters	Inches	Millimeters	Inches	Millimeters	Cu. Ft.	Liters	Cu. Ft.	Liters	
Fiat 500L	167.4	4253	70.9	1801	65.5	1663	22.4	634	68	1924	3,222 ATX Trekking
Fiat 500 X	167.2	4248	70.7	1796	63.1	1602	14.5	410	40.8	1155	3,113 ATX 2.4L

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Units must be positioned 3” bumper to bumper and 5” between bumper and end door to allow for a load factor of 6 per deck and to maintain adequate spacing.
- A minimum of 3” roof clearance must be maintained.
- Vehicles are to be uniformly positioned on decks (A = 6, B = 6) to maximize the distance between vehicles and between vehicles and end doors.
- Chocks should be set as followed:
 - 500L - Front on low setting, rear on mid setting
 - 500X - Front on low setting, rear on high setting
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided due to the tight spacing requirements between vehicles and rail car end doors.



Guidelines for Haulway Transport:

- Strap/Soft tie securement only on these models.
- Make sure all decks are as level as possible to prevent rocker panel and/or front fascia damage.
- Properly set skid position to prevent front fascia damage.
- **SLOW** speed is essential because this is a low profile model.



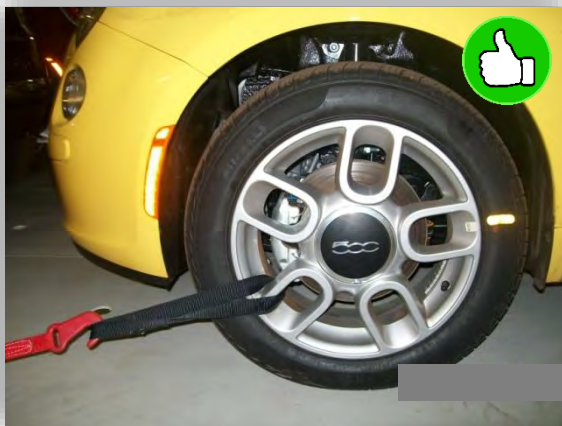
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



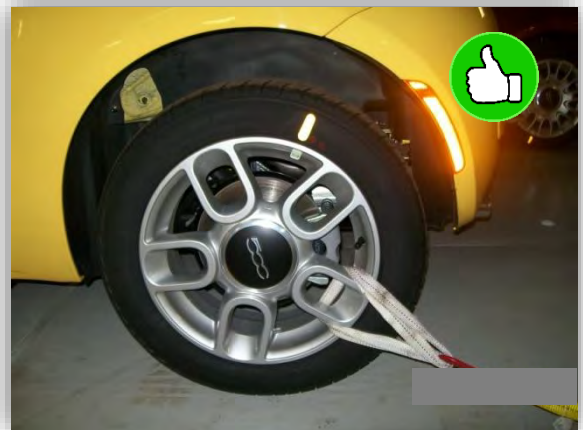
Guidelines for Ocean Transport:

- A stop condition is required when either entering the ramp or off loading and grounding the unit.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



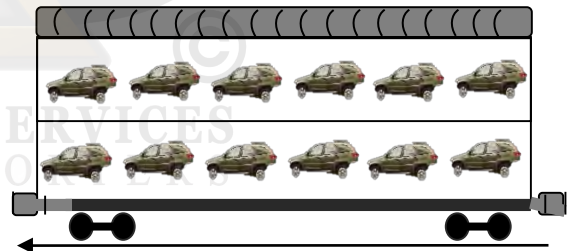
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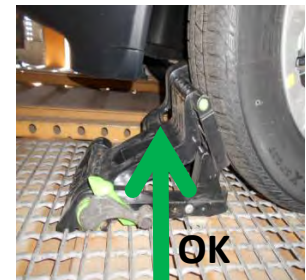
Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors (in)	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight (lbs.)	Max* Weight (lbs.)	Approach Angle	Departure Angle
					Highest	Lowest					
Fiat Palio Sporting MT	152.7	67.1	67.6	65.8	59.4	59.4	95.4	2502	3384	22.2°	23°
Fiat Palio Sporting Dualogic	152.7	67.1	67.6	65.8	59.4	59.4	95.4	2502	3384	21.6°	22.3°
Fiat Palio Adventure MT	169.6	69.0	69.9	67.8	64.7	64.7	97.2	2646	3527	26°	26°
Fiat Palio Adventure Dualogic	169.6	69.0	69.9	67.8	64.7	64.7	97.2	2646	3527	23°	23°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- A minimum of 3" roof clearance must be maintained.
- Units must be positioned 3" bumper to bumper and 5" between bumper and end door to allow for a load factor of 6 / deck and to maintain adequate spacing. This is very critical, as there is no room for error.
- Vehicles are to be uniformly positioned on decks (A = 6, B = 6) to maximize the distance between vehicles and between vehicles and end enclosures.
- Front and rear chocks must be placed in the high setting.
- All chocks must be carefully positioned from the side of the vehicle, never from the front (see photo).
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- Do not exceed 5 mph / 8 kmh speed limit when loading and unloading and utilize caution inside the railcar to avoid damage.

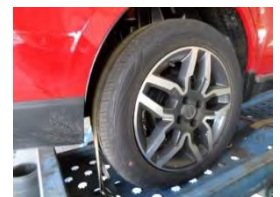


Make sure to install chock from the side.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.
- Make sure all decks are as level as possible to prevent rocker panel and / or front fascia damage.
- Properly set skid position to prevent front fascia damage.
- **SLOW** speed is essential because this is a low profile model.



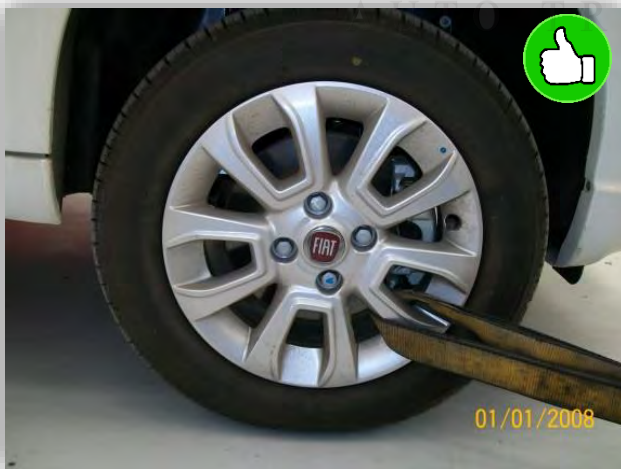
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



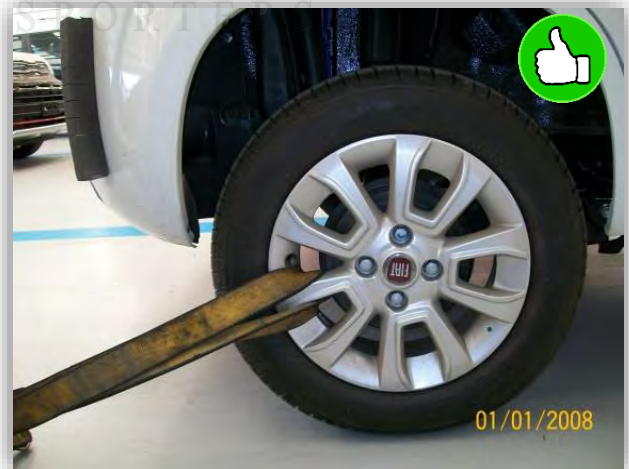
Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Vehicle	Length		Width		Height W/O Accessories		SAE Cargo Area (Behind 2nd Row)		SAE Cargo Area (max)		Curb Weight
	Inches	Millimeters	Inches	Millimeters	Inches	Millimeters	Cu. Ft.	Liters	Cu. Ft.	Liters	
RT—Grand Caravan	203.8	5176	78.7	1999	69.3	1761	78.7	2227	133	3764	4,548

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Climate control comes on automatically on Dodge Caravan models.
- Front chocks should be set in the medium setting.
- Rear chocks should be set in the highest setting.
- Supplemental chocks must be used when present – Supplemental Block Chocks cannot be used on the front of the rear tires when running boards are present.
- AVR Supplemental Restraints cannot be used on the front wheels due to lack of clearance.
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia and the rocker panels.
- Vehicles are to be uniformly positioned on decks (A = 5, B =5) to maximize the distance between vehicles and between vehicles and end doors.
- A minimum of 3” is required between vehicles and 5” between vehicle and end doors.
- A minimum of 3” roof clearances must be maintained.

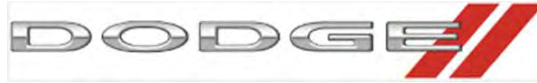


Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.
- There is a high potential for rocker panel damage, so make sure all ramps/decks are as level as possible.
- Properly set skid position to prevent front fascia damage.
- A stop condition is required when entering or exiting the ramp.
- **SLOW** speed is essential because this is a low profile model.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- **SLOW** speed is essential when loading and unloading if damage is to be avoided.
- All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width with Folded Mirrors	Overall Width with Mirrors (in)	Overall Width without Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight	Max* Weight	Approach Angle	Departure Angle	Breakover Angle
					Highest	Lowest						
Cherokee DOM	182	75.1	82.1	73.3	65.8	64.3	106.6	3,579	4,260	17.0°	20.5°	15.0°
Cherokee BUX	182	75.1	82.1	73.3	65.8	64.3	106.6	3,591	4,281	17.0°	20.5°	15.0°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Climate control comes on automatically on Jeep Cherokee models.
- Front chocks should be placed in the mid setting, except for Trailhawks which should be placed in the high setting
- Rear chocks should be placed in the Mid setting.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- Vehicles are to be uniformly positioned on decks (A = 5, B = 5) to maximize the distance between vehicles and between vehicles and end doors.
- A minimum of 3" is required between vehicles and 5" between vehicles and end doors.
- A minimum of 3" roof clearances must be maintained.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.
- 4 Straps/unit.
- Make sure straps are not frayed or twisted.
- Straps should not come into contact with any part of the vehicle except the tire.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

Jeep



Cherokee



Guidelines for Ocean Transport:

- **SLOW** speed is essential when loading and unloading if damage is to be avoided.
- All 4 wheels must be lashed
- When wheel lashing:
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



Do not use fixed tow hooks in the front or rear for tie down.



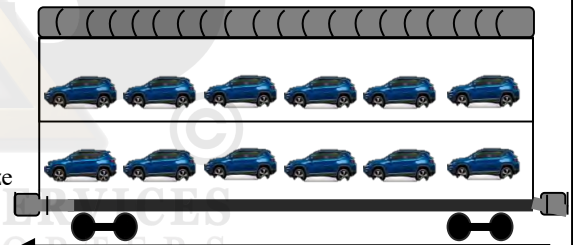
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors (in)	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight (lbs.)	Max* Weight (lbs.)	Approach Angle	Departure Angle
					Highest	Lowest					
Compass Latitude	173.0	76.5	81.05	71.61	64.6	64.6	103.78	3322	4400	16.8°	31.7°
Compass Limited Premium	173.0	76.5	81.05	71.61	64.6	64.6	103.78	3371	4400	16.8°	31.7°
Compass Trailhawk	173.0	76.5	81.05	71.61	64.6	64.6	103.78	3708	4700	16.8°	31.7°

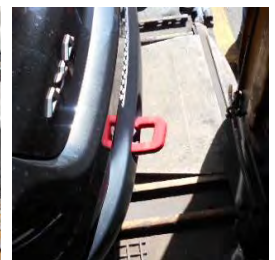
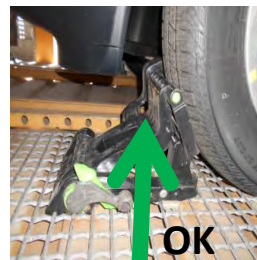
Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- A minimum of 3" roof clearance must be maintained.
- Units must be positioned 3" bumper to bumper and 5" between bumper and end door to allow for a load factor of 6 / deck and to maintain adequate spacing. **This is very critical, as there is no room for error.**
- Vehicles are to be uniformly positioned on decks (A = 6, B = 6) to maximize the distance between vehicles and between vehicles and end enclosures.
- Front and rear chocks must be placed in the high setting.
- All chocks must be carefully positioned from the side of the vehicle, never from the front (see photo).
- Do not exceed 4 Trailhawks per deck due to possible damage from the tow hook (see photo).
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- Do not exceed 5 mph / 8 kmh speed limit when loading and unloading and utilize caution inside the railcar to avoid damage.



Make sure to install chock from the side.

Trailhawk w/ protruding hooks.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- Do not use tie down hooks as lashing points.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.
- Do not exceed 5 mph / 8 kmh during loading and unloading to avoid damage.

Wheel lash in front



Wheel lash in rear



Do not use fixed tow hooks in the front for tie down.



Do not use fixed tow hook on left side rear for tie down.



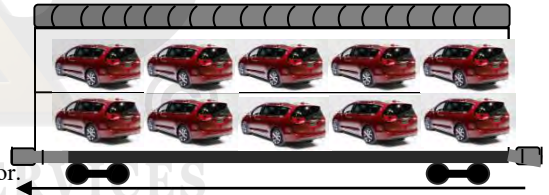
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width with Folded Mirrors	Overall Width with Mirrors (in)	Overall Width without Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight	Max* Weight	Approach Angle	Departure Angle	Breakover Angle
					Highest	Lowest						
2017 RU	203.8	83.0	90.4	79.6	68.7	70.3	121.6	4,094	4,543	13.9°	19.0°	12.3°
2017 RU Electric (Dom / BUX)	203.8	83.0	90.4	79.6	68.7	70.3	121.6	4,845	5,050	13.9°	19.0°	12.3°
2017 RU BUX	203.8	83.0	90.4	79.6	68.7	70.3	121.6	4,278	4,468	13.9°	19.0°	12.3°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Supplemental and AVR chocks are not to be used on the front of Pacifica models due to clearance of less than 2".
- Climate control comes on automatically on Pacifica models.
- Vehicles are to be uniformly positioned on decks (A = 5, B = 5) to maximize the distance between vehicles and between vehicles and end doors.
- Units must be positioned 3" bumper to bumper and 5" between bumper and end door.
- A minimum of 3" roof clearances must be maintained.
- Front chocks should be set in the medium setting.
- Rear chocks should be set in the highest setting.
- Supplemental chocks are not to be used, except block chocks on the rear tires.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia and the rocker panels.



Guidelines for Haulaway Transport:

- **ONLY 7 units are to be loaded due to length and clearance concerns. See picture for correct vehicle positioning.**
- Strap/Soft tie securement only.
- There is a high potential for undercarriage / rocker panel damage, please ensure all ramps / decks are as level as possible.
- Properly set skid position to prevent front fascia damage.
- A stop condition is required when entering or exiting the ramp.
- **SLOW** speed is essential because this is a low profile model.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- **SLOW** speed is essential when loading and unloading if damage is to be avoided.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not carry any tools, straps, etc., between or around vehicles while on the vessel.

ACCELERATED SERVICES
AUTO TRANSPORTERS

Wheel lash in front



Wheel lash in front



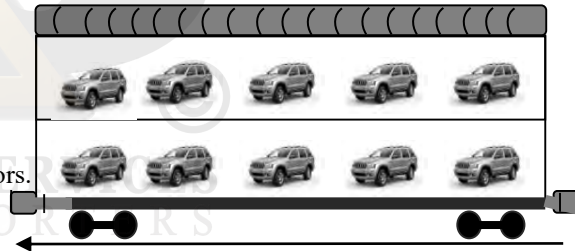
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width with Folded Mirrors	Overall Width with Mirrors (in)	Overall Width without Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight	Max* Weight	Approach Angle	Departure Angle	Breakover Angle
					Highest	Lowest						
Grand Cherokee DOM	189.8	76.5	84.8	76.5	73.0	70.0	114.8	4,548	5,384	26.2°	24.0°	19.0°
Grand Cherokee BUX	189.8	76.5	84.8	76.5	73.0	69.9	114.8	4,626	5,260	26.2°	24.0°	19.0°
Durango DOM	199.8	77.1	85.5	75.6	71.6	70.9	119.8	4,598	5,304	16.3°	21.5°	17.9°
Durango BUX	199.8	77.1	85.5	75.6	71.6	70.9	119.8	4,677	5,303	16.3°	21.9°	17.9°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Climate control comes on automatically on Jeep Cherokee and Dodge Durango models.
- Vehicles are to be uniformly positioned on decks (A = 5, B = 5) to maximize the distance between vehicles and between vehicles and end doors.
- Units must be positioned 3” bumper to bumper and 5” between bumper and end doors.
- A minimum 3” roof clearance must be maintained.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- **SLOW** speed is essential when loading and unloading if damage is to be avoided.

• **Grand Cherokee**

- Do not use Tow Hooks in Front.
- Tow hook in rear.
- When wheel lashing front tires and rear tires (if tow hook in rear is not available).
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.

• **Durango**

- Wheel lash only.
- When wheel lashing:
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Do not use tow hooks in front



Wheel lash in Front



Tow hook in rear



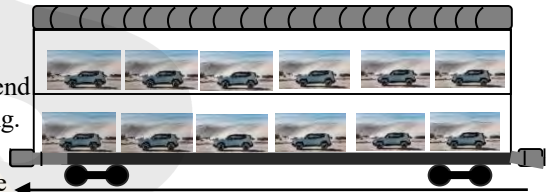
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight	Max* Weight	Approach Angle	Departure Angle
					Highest	Lowest					
Jeep Renegade	166.6	69.4	71.0	N/A	66.5	66.5	101.2	2907	3460	17.9°	29.7°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Units must be positioned 3” between bumpers and 5” between bumper and end door to allow for a load factor of 6 per deck and to maintain adequate spacing. This is very critical, as there is no room for error.
- Vehicles are to be uniformly positioned on decks (A = 6, B = 6) to maximize the distance between vehicles and between vehicles and end doors.
- Supplemental and AVR chocks are NOT to be used on the front of Renegade models due to clearance of less than 2”.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided due to the tight spacing requirements between the vehicles and between the vehicles and end doors on the railcar.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only on these models.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- **SLOW** speed is essential when loading and unloading if damage is to be avoided.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



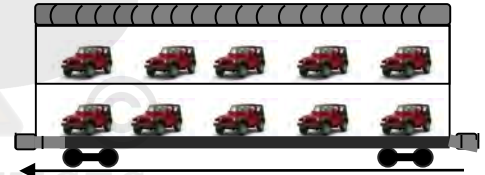
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width with Folded Mirrors	Overall Width with Mirrors (in)	Overall Width without Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight	Max* Weight	Approach Angle	Departure Angle	Breakover Angle
					Highest	Lowest						
2 DR Wrangler DOM	157.5	71	76.6	73.9	71.3	70	95.4	3,785	4,019	38.7°	28.9°	NA
4 DR Wrangler DOM	181.3	71	76.6	73.9	71.3	70.9	116	4,104	4,321	38.6°	28.5°	NA
2 DR Wrangler BUX	157.5	71	76.6	73.9	71.3	70	95.4	3,817	4,212	38.7°	28.9°	NA
4 DR Wrangler BUX	181.3	71	76.6	73.9	71.3	70.9	116	4,076	4,531	38.6°	28.5°	NA
JL 74 Sport	188.4	71.7	77.7	73.8	74.2	72.1	118.4	4,167	4,943	41.4°	30.2°	20.3°
JL 74 Sahara	188.1	71.7	77.7	73.8	74.9	72.8	118.4	4,263	5,002	41.8°	30.8°	21.0°
JL 74 Rubicon	188.6	71.7	77.7	73.8	75.7	73.7	118.4	4,449	5,192	43.9°	32.2°	22.6°
JL 72 Sport (2 DR)	166.8	71.7	77.7	73.8	73.8	72.4	96.9	3,919	4,310	41.0°	29.7°	24.0°
JL 72 Rubicon (2 DR)	166.8	71.7	77.7	73.8	73.8	72.4	96.9	4,193	4,650	44.0°	31.9°	27.8°

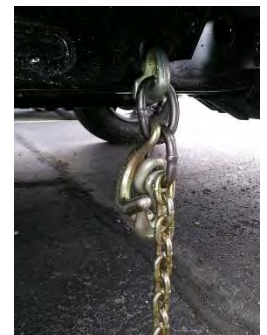
Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- Vehicles are to be uniformly positioned on decks (A = 5, B = 5) to maximize the distance between vehicles and between vehicles and end doors.
- A minimum of 3" is required between vehicles and 5" between vehicle end doors.
- A minimum of 3" roof clearances must be maintained.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.



Guidelines for Haulaway Transport:

- Soft tie method preferred. R-hooks may be used.
- Chain tie locations pictured below.
- Do not load models with soft tops rearward on the head rack to avoid wind damage.
 - All other units can be loaded forward or rearward.
- Use caution when entering / exiting this vehicle to avoid damage to the sill.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- **SLOW** speed is essential when loading and unloading if damage is to be avoided .
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.
- Fixed loop in the front on the right side of vehicle.
- Fixed loop in the rear located on the left side of the vehicle.

Fixed loop in front



Fixed loop in rear



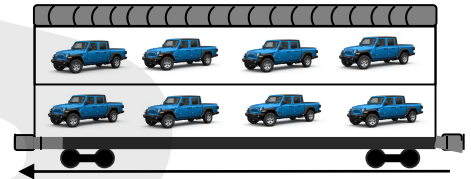
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Weights & Dimensions	Overall Length (in)	Width with Folded Mirrors	Overall Width with Mirrors (in)	Overall Width without Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight	Max* Weight	Approach Angle	Departure Angle	Breakover Angle
					Highest	Lowest						
JT Sport	218.1	71.7	77.7	73.8	75.1	73.1	96.9	4,539	5,294	40.8°	20.7°	18.4°
JT Rubicon	217.7	71.7	77.7	75.6	76.1	74.1	118.4	4,938	5,592	43.4°	21.6°	20.3°
JT Mojave	217.7	71.7	77.7	75.6	76.3	74.3	118.4	4,883	5,223	44.7°	21.3°	25.5°

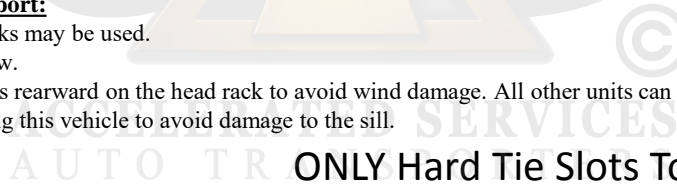
Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Vehicles are to be uniformly positioned on decks (A = 4, B = 4) to maximize the distance between vehicles and between vehicles and end enclosures.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.



Guidelines for Haulaway Transport:

- Soft tie method preferred. R-hooks may be used.
- Chain tie locations pictured below.
- Do not load models with soft tops rearward on the head rack to avoid wind damage. All other units can be loaded forward or rearward.
- Use caution when entering/exiting this vehicle to avoid damage to the sill.

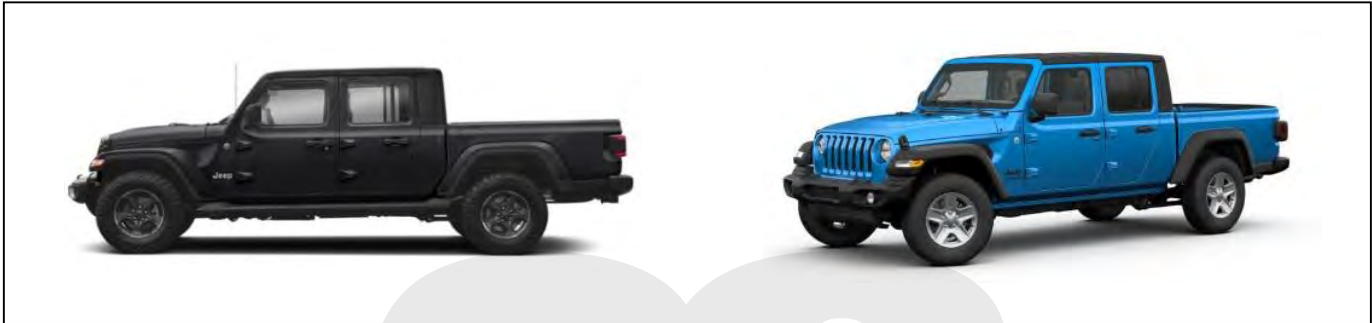


ONLY Hard Tie Slots To Be Used

Only Soft Tie Strapping Method To Be Used



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

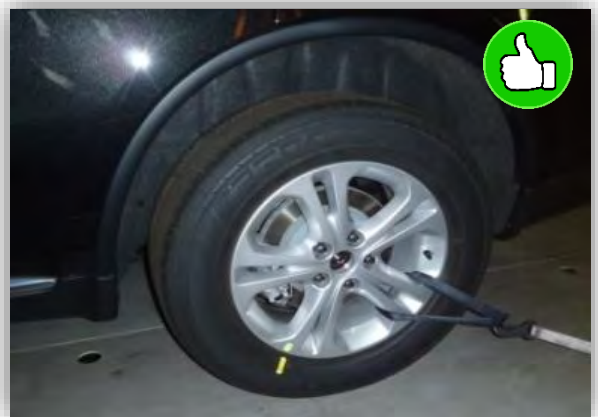
- **SLOW** speed is essential when loading and unloading if damage is to be avoided .
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.

ACCELERATED SERVICES
AUTO TRANSPORTERS

Wheel lash in front



Wheel lash in rear



Rail & Truck Handling, Loading and Securement Standards for Shipping the FIAT 500, FIAT 500 BEV, & FIAT 500 ABARTH

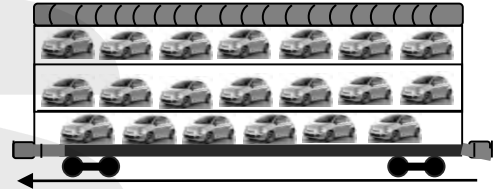
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight	Max* Weight	Approach Angle	Departure Angle	Breakover Angle
					Lowest	Highest						
FIAT 500	139.6	65.6	73.5	64.1	59.3	60.3	90.6	2294	2569	10.8°	30.5°	14.8°
FIAT 500 BeV	142.4	67.1	73.5	64.1	59.3	60.3	90.6	2961	2961	10.0°	31.5°	14.8°
F500 Abarth	144.4	65.6	73.5	64.1	59.3	60.3	90.6	2477	2645	10.8°	30.5°	14.8°

Guidelines for Rail Transport:

- Loading is restricted to Tri-level railcars, unless authorization is given by FCA US / Mexico Logistics to load on Bi-level's. If loading on a Bi-level the front chock height should be set in the low position and the rear chock in the high position to maintain two inches of clearance between the chock and the closest point on the vehicle.
- Vehicles are to be uniformly positioned on decks (A = 6, B = 7, C = 7) to maximize the distance between vehicles and between vehicles and end doors.
- A minimum of 3" is required between vehicles and 5" between vehicle end doors.
- A minimum 3" roof clearance must be maintained.
- Position the vehicle on Tri-levels with tires no closer to the tie-down rail than half an inch (1/2"). Optimal spacing is 1-2", no further than 3".
- Spotter is required on A-Deck and B-deck when the chock tie-down track is on right side of vehicle to assist/guide driver to position vehicle for proper securement application and prevent vehicles tire/rims from contact/rubbing against chock tie-down track.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low clearance vehicle.

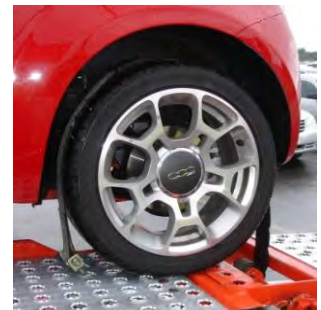


Special Cautionary Notes:

- There is a notably tight clearance between securement devices and the unit.
- The tire width to unload through a railcar and over the bridge plates is very tight. See above picture for reference. Also, due to the width of the tires, on some older Tri-level railcars the unit can rub up against the inner chock rail causing possible tire damage. Please be aware of this while loading and unloading.

Guidelines for Haulaway Transport:

- Strap/Soft tie securement only on these models.
- **SLOW** speed is essential because this is a low profile model.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

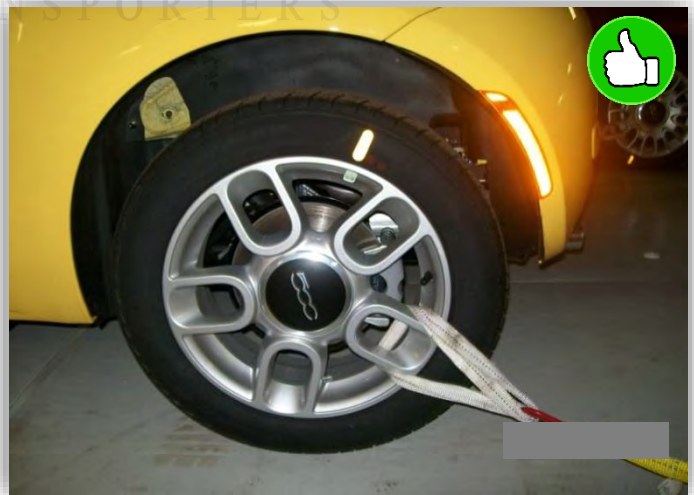


Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp to prevent grounding the unit.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low clearance vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front

Wheel lash in rear



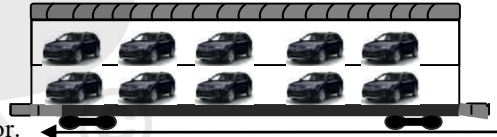
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight	Max* Weight	Approach Angle	Departure Angle	Breakover Angle
					Highest	Lowest						
Journey DOM	193.9	74.6	83.7	73.9	68.0	67.8	113.8	3744	4271	13.4°	22.9°	15.1°
Journey BUX / FIAT Freemont	193.9	74.6	83.7	73.9	67.9	67.1	113.8	3764	4253	13.4°	22.9°	15.1°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Vehicles are to be uniformly positioned on decks (A = 5, B =5) to maximize the distance between vehicles and between vehicles and end doors.
- A minimum of 3” is required between vehicles and 5” between vehicle to end door.
- A minimum of 3” roof clearances must be maintained.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



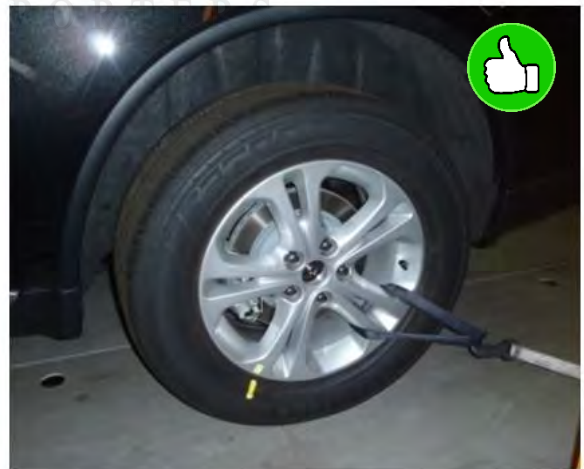
Guidelines for Ocean Transport:

- **SLOW** speed is essential when loading and unloading if damage is to be avoided.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



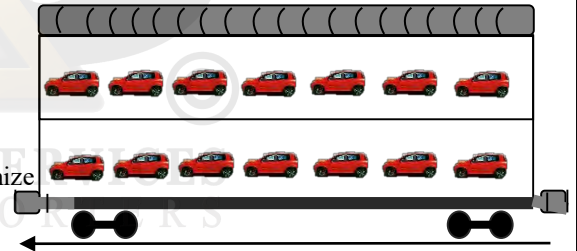
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors (in)	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight (lbs.)	Max* Weight (lbs.)	Approach Angle	Departure Angle
					Highest	Lowest					
Fiat Mobi Easy/ Like	140.5	65.6	66.5	64.3	59.3	59.3	90.8	1951	2833	21.5°	32°
Fiat Mobi Way	141.7	67.6	68.5	66.4	59.8	59.8	90.8	2070	2952	23°	32.9°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- A minimum of 3" roof clearance must be maintained.
- Units must be positioned 3" bumper to bumper and 5" between bumper and end door to allow for a load factor of 7 / deck and to maintain adequate spacing. This is very critical, as there is no room for error.
- Vehicles are to be uniformly positioned on decks (A = 7, B = 7) to maximize the distance between vehicles and between vehicles and end enclosures.
- Front and rear chocks must be placed in the high setting.
- All chocks must be carefully positioned from the side of the vehicle, never from the front (see photo).
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- Do not exceed 5 mph / 8 kmh speed limit when loading and unloading and utilize caution inside the railcar to avoid damage.

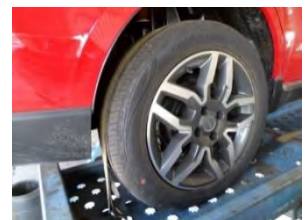


Make sure to install chock from the side.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.
- Make sure all decks are as level as possible to prevent rocker panel and / or front fascia damage.
- Properly set skid position to prevent front fascia damage.
- **SLOW** speed is essential because this is a low profile model.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



Wheel lash for plastic rim cover



NO



OK

- Hooks and lashings should **NEVER** touch the wheel cover.
- Lashing strap should be installed going from the inside through the wheel arm and come out towards the operator.

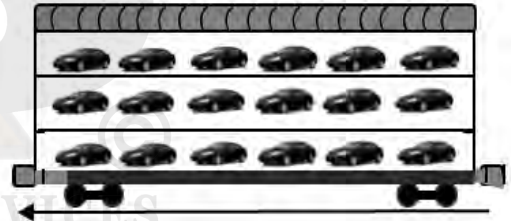
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length (in)	Width with Folded Mirrors	Overall Width with Mirrors (in)	Overall Width without Mirrors (in)	Height (in)	Wheelbase (in)	Min* Weight (lbs)	Max* Weight (lbs)	Approach Angle	Departure Angle	Breakover Angle
Classica AT	159.6	75.5	69.8	68.5	48.5	90.9	2476.2	2490.1	13.1	13	11
Classica MT	159.6	75.5	69.8	68.5	48.5	90.9	2437.2	2451.1	13.1	13	11
Lusso AT 16" tires	159.6	75.5	69.8	68.5	48.5	90.9	2505.3	2541.7	13.1	13	11
Lusso MT 16" tires	159.6	75.5	69.8	68.5	48.5	90.9	2466.1	2498.7	13.1	13	11
Lusso AT 17" tires	159.6	75.5	69.8	68.5	48.8	90.9	2505.3	2541.7	13.1	13	11
Lusso MT 17" tires	159.6	75.5	69.8	68.5	48.8	90.9	2466.1	2498.7	13.1	13	11
Elaborazione Abarth AT 17" tires	159.6	75.5	69.8	68.5	48.8	90.9	2516.4	2553.4	13.1	13	11
Elaborazione Abarth MT 17" tires	159.6	75.5	69.8	68.5	48.8	90.9	2477.1	2512.8	13.1	13	11

Guidelines for Rail Transport:

- Loading is restricted to Tri-level railcars, unless authorization is given by FCA US Logistics to load on Bi-level's. If loading on a Bi-level, the front chock height should set in the low position and the rear chock in the mid position to maintain adequate clearance between the chock and the closest point on the vehicle. Due to insufficient clearance the use of Co-Poly chocks is prohibited.
- Vehicles are to be uniformly positioned on decks (A = 6, B = 6, C = 6) to maximize the distance between vehicles and between vehicles and end doors.
- A minimum of 3" is required between vehicles and 5" between vehicles and end doors.
- A minimum of 3" roof clearances must be maintained.
- Position the vehicle on Tri-levels with tires no closer to the tie-down rail than half an inch (1/2"), optimal spacing is 2-3".
- Spotter is required on A-Deck when the chock tie-down track is on right side of vehicle to assist/guide driver to position vehicle for proper securement application and prevent vehicles tire/rims from contact/rubbing against chock tie-down track.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.



Guidelines for Haulway Transport:

- Vehicle is to be transported **ONLY** on the top deck, excluding the head rack and **ONLY** the last position on the lower deck.
- Strap/Soft tie securement only.
- There is a high potential for rocker panel damage, so make sure all ramps / decks are as level as possible.
- Properly set skid position to prevent front fascia damage.
- A stop condition is required when entering or exiting the ramp.
- **SLOW** speed is essential as this is a low profile model.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front



Wheel lash in rear



Wheel lash for plastic rim cover



NO



OK

- Hooks and lashings should **NEVER** touch the wheel cover.
- Lashing strap should be installed going from the inside through the wheel arm and come out towards the operator.

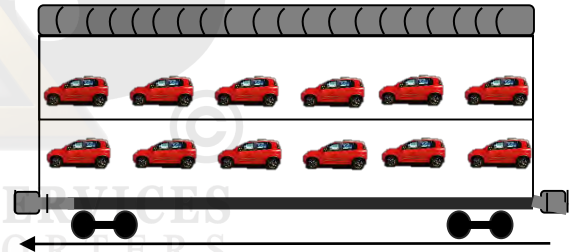
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



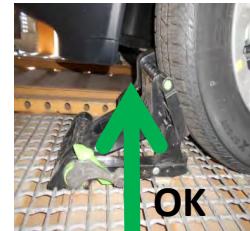
Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors (in)	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight (lbs.)	Max* Weight (lbs.)	Approach Angle	Departure Angle
					Highest	Lowest					
Fiat Uno Like/ Way	150.1	66.6	67.2	65.2	61.3	61.3	93.6	2213	3095	20.2°	29.4°
Fiat Uno Attractive	150.1	65.8	66.4	64.5	58.7	58.7	93.6	2178	3060	21.9°	27.2°
Fiat Uno Sporting	150.1	67.3	67.9	65.9	58.6	58.6	93.6	2242	3124	20.4°	19.3°

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- A minimum of 3" roof clearance must be maintained.
- A minimum of 3" is required between vehicles and 5" between vehicle and end doors to allow for a load factor of 6 / deck and to maintain adequate spacing. This is very critical, as there is no room for error.
- Vehicles are to be uniformly positioned on decks (A = 6, B = 6) to maximize the distance between vehicles and between vehicles and end doors.
- Front and rear chocks must be placed in the high setting.
- All chocks must be carefully positioned from the side of the vehicle, never from the front (see photo).
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- Do not exceed 5 mph / 8 kmh speed limit when loading and unloading and utilize caution inside the railcar to avoid damage.

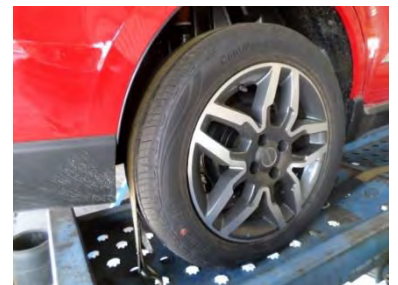


Make sure to install chock from the side.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.
- Make sure all decks are as level as possible to prevent rocker panel and / or front fascia damage.
- Properly set skid position to prevent front fascia damage.
- **SLOW** speed is essential because this is a low profile model.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

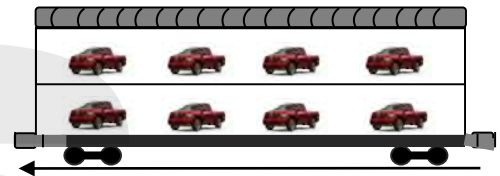


Weights & Dimensions are listed on page 61



Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Vehicles are to be uniformly positioned on decks (A = 4, B = 4) to maximize the distance between vehicles and also the distance between vehicles and end doors.
- A minimum of 3” is required between vehicles and 5” between vehicle and end doors.
- A minimum of 3” roof clearance must be maintained.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided due to the size of the vehicle.
- Standard Cab model has longer doors than Quad Cab.



Chocks:

- All units must be secured using a 6-point chocking system.
- When railcars are equipped with supplemental block chocks they should be placed on the inboard side of the remaining two wheels
 - All supplemental block chocks should be placed between 1” and 3” away from the tire.

Guidelines for Haulaway Transport:

- Soft tie method preferred. R-hooks may be used.
- R-hooks required for front tie down slots.
 - T-hooks to be used ONLY in hitch or lower control bracket.
- Units with a tonneau cover must be loaded forward to prevent wind damage.
- Vehicle can be secured with four tire straps that secure in front and behind the tire.

Soft Tie Strapping Preferred



(If no hitch, T-hook allowed)



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights and Dimensions are located on page 61



Guidelines for Ocean Transport:

- **SLOW** speed is essential when loading and unloading if damage is to be avoided .
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.
- Fixed loop on either side of the front side of the vehicle. Use wheel lashing if bumper hooks not available.
- Fixed bracket in the rear located on the trailer hitch. Use wheel lashing if tow hitch is not available.
- Lashing straps cannot contact any part of the vehicle other than the lashing bracket.

Wheel lash in front
(If no in bumper hooks)



Wheel lash in rear
(If no tow hitch available)



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

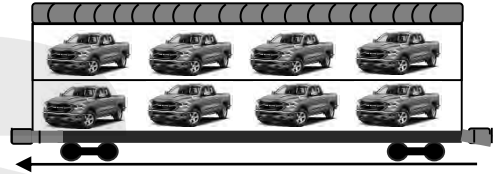


Weights and Dimensions are located on page 61



Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Vehicles are to be uniformly positioned on decks (A = 4, B = 4) to maximize the distance between vehicles and also the distance between vehicles and end doors.
- A minimum of 3” is required between vehicles and 5” between vehicle and end doors.
- A minimum of 3” roof clearances must be maintained.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided due to the size of the vehicle.



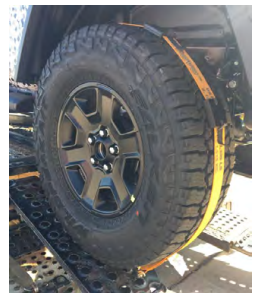
Chocks:

- All units must be secured using a 6-point chocking system.
- When railcars are equipped with supplemental block chocks they should be placed on the inboard side of the remaining two wheels
 - All supplemental block chocks should be placed between 1” and 3” away from the tire.

Guidelines for Haulaway Transport:

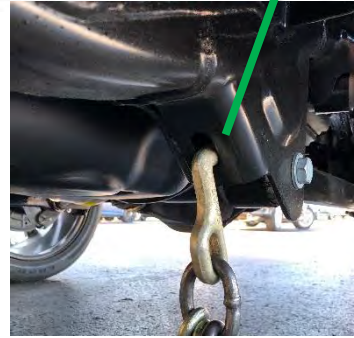
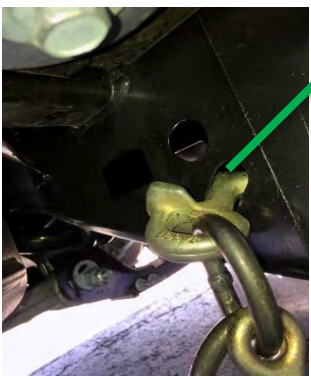
- Soft tie method preferred. R-hooks may be used.
- R-hooks required for front tie down slots.
 - T-hooks to be used ONLY in hitch or lower control bracket.
- Units with a tonneau cover must be loaded forward to prevent wind damage.
- Vehicle can be secured with four tire straps that secure in front and behind the tire.
- Do not mix chains and straps when securing vehicle to the Haul-away Truck.

Soft Tie Strapping Preferred



(R-hook ONLY)

(T-hook allowed)



(If no hitch, T-hook allowed)

This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Guidelines for Ocean Transport:

- **SLOW** speed is essential when loading and unloading if damage is to be avoided .
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.
- Fixed loop on either side of the front side of the vehicle. Use wheel lashing if bumper hooks not available.
- Fixed bracket in the rear located on the trailer hitch. Use wheel lashing if tow hitch is not available.
- Lashing straps cannot contact any part of the vehicle other than the lashing bracket.

Wheel lash in front
(If no in bumper hooks)



Wheel lash in rear
(If no tow hitch available)



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

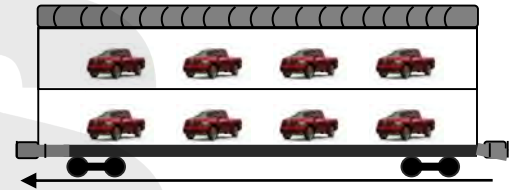


Weights & Dimensions are located on page 61



Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Vehicles are to be uniformly positioned on decks (A = 4, B = 4) to maximize the distance between vehicles and also the distance between vehicles and end doors.
- A minimum of 3" is required between vehicles and 5" between vehicle and end doors.
- A minimum of 3" roof clearance must be maintained.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided due to the size of the vehicle.
- Standard Cab models have longer doors than Quad Cab models.



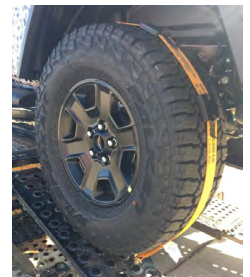
Chocks:

- All units must be secured using a 6-point chocking system.
- When railcars are equipped with supplemental block chocks they should be placed on the inboard side of the remaining two wheels
 - All supplemental block chocks should be placed between 1" and 3" away from the tire.

Guidelines for Haulaway Transport:

- Soft tie method preferred. R-hooks may be used.
- Chain tie locations pictured below.
- Units with a tonneau cover must be loaded forward to prevent wind damage.
- Vehicle can be secured with four tire straps that secure in front and behind the tire.
- Do not mix chains and straps when securing vehicle to the Haul-away Truck.

Soft Tie Strapping Preferred



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

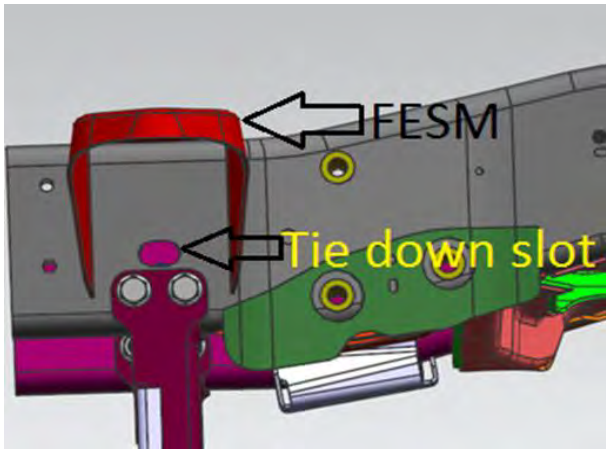
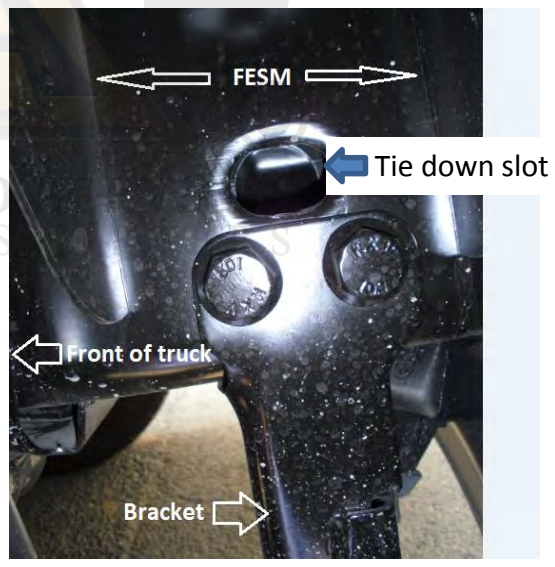


Weights & Dimensions are located on page 61



The present addendum is only for DJ 4X4 2014 truck and later, with bracket on front tie down slot, to be transported by ground:

- To identify a DJ 4X4 truck, examine the wheel. It must have 8 stud bolts.
- When the truck is over the trailer, notice under it a bracket in the front tie down slot.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions are located on page 61



“R” hook



Process of securement

- At the front of the truck, use the tie down slot located below the FESM.
- Insert the “R” hook with the longer leg facing to the rear of the truck and then the hook must be rotated with the longer leg facing upwards.
- The “R” hooks must be placed carefully to ensure proper engagement.
- At the rear of the truck, identify the tie down slots and proceed as per “Vehicle shipping Manual”



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

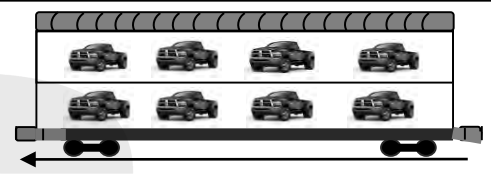


Weights & Dimensions are located on page 61



Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Spotter required for all loading / unloading.
- Vehicles are to be uniformly positioned on decks (A = 4, B = 4) to maximize the distance between vehicles and also the distance between vehicles and end enclosures.
- A minimum of 3” is required between vehicles and 5” between vehicle and end doors.
- A minimum of 3” roof clearance must be maintained.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided due to the size of the vehicle.
- Standard Cab models have longer doors than Quad Cab models.



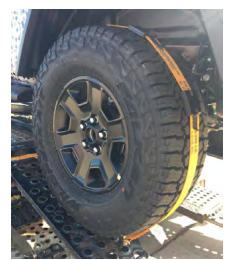
Chocks:

- All units must be secured using a 6-point chocking system.
- When railcars are equipped with supplemental block chocks they should be placed on the inboard side of the remaining two wheels
 - All supplemental block chocks should be placed between 1” and 3” away from the tire.

Guidelines for Haulaway Transport:

- Soft tie method preferred. R-hooks may be used.
- Chain tie locations pictured below.
- Units with a tonneau cover must be loaded forward to prevent wind damage.
- Vehicle can be secured with four tire straps that secure in front and behind the tire.
- Do not mix chains and straps when securing vehicle to the Haul-away Truck.

Soft Tie Strapping Preferred



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

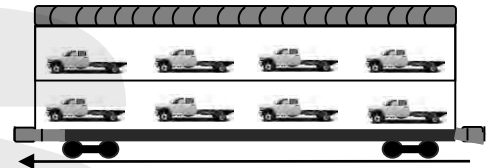


Weights & Dimensions are located on page 61



Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- If upfit with a bed or workbox a spotter is required for loading/unloading
- Vehicles are to be uniformly positioned on decks (A = 4, B = 4) to maximize the distance between vehicles and also the distance between vehicles and end enclosures.
- A minimum of 3" is required between vehicles and 5" between vehicle and end doors.
- A minimum of 3" roof clearance must be maintained.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided due to the size of the vehicle.
- Standard Cab model has longer doors than Quad Cab model.



Chocks:

- All units must be secured using a 6-point chocking system when supplemental block chocks are not available.
- When railcars are equipped with supplemental block chocks:
 - All four tires should utilize the supplemental block chock on the inboard side of the tire, placed between 1" to 3" away from the tire.

Guidelines for Haulaway Transport:

- Soft tie method preferred. R-hooks may be used.
- Chain tie locations pictured below.
- Units with a tonneau cover must be loaded forward to prevent wind damage.
- Vehicle can be secured with four tire straps that secure in front and behind the tire.
- Do not mix chains and straps when securing vehicle to the Haul-away Truck.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

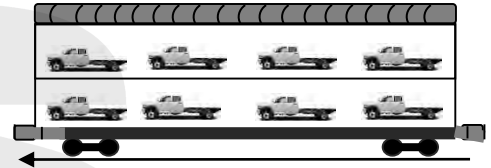


Weights & Dimensions are located on page 61



Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- If upfit with a bed or workbox a spotter is required for loading/unloading
- Vehicles are to be uniformly positioned on decks (A = 4, B = 4) to maximize the distance between vehicles and also the distance between vehicles and end enclosures.
- A minimum of 3" is required between vehicles and 5" between vehicle and end doors.
- A minimum of 3" roof clearance must be maintained.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided due to the size of the vehicle.
- Standard Cab model has longer doors than Quad Cab model.



Chocks:

- All units must be secured using a 6-point chocking system when supplemental block chocks are not available.
- When railcars are equipped with supplemental block chocks:
 - All four tires should utilize the supplemental block chock on the inboard side of the tire, placed between 1" to 3" away from the tire.

Guidelines for Haulaway Transport:

- Soft tie method preferred. R-hooks may be used.
- Chain tie locations pictured below.
- Units with a tonneau cover must be loaded forward to prevent wind damage.
- Vehicle can be secured with four tire straps that secure in front and behind the tire.
- Do not mix chains and straps when securing vehicle to the Haul-away Truck.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

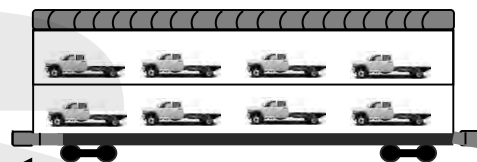


Weights & Dimensions are located on page 61



Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- If upfit with a bed or workbox a spotter is required for loading/unloading
- Vehicles are to be uniformly positioned on decks (A = 4, B = 4) to maximize the distance between vehicles and also the distance between vehicles and end enclosures.
- A minimum of 3" is required between vehicles and 5" between vehicle and end doors.
- A minimum of 3" roof clearance must be maintained.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided due to the size of the vehicle.
- Standard Cab model has longer doors than Quad Cab model.

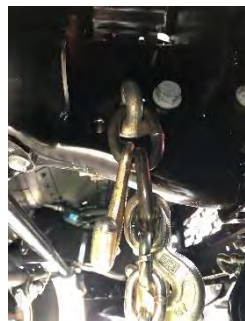


Chocks:

- All units must be secured using a 6-point chocking system when supplemental block chocks are not available.
- When railcars are equipped with supplemental block chocks:
 - All four tires should utilize the supplemental block chock on the inboard side of the tire, placed between 1" to 3" away from the tire.

Guidelines for Haulaway Transport:

- Soft tie method preferred. R-hooks may be used.
- Chain tie locations pictured below.
- Units with a tonneau cover must be loaded forward to prevent wind damage.
- Vehicle can be secured with four tire straps that secure in front and behind the tire.
- Do not mix chains and straps when securing vehicle to the Haul-away Truck.



Weights & Dimensions for RAM Trucks

RAM 1500, RAM, 2500, RAM 3500, RAM 4500 & RAM 5500

Vehicle	Length		Width		Height W/O Accessories		SAE Cargo Area (Behind 2nd Row)		SAE Cargo Area (max)		Curb Weight
	Inches	Millimeters	Inches	Millimeters	Inches	Millimeters	Cu. Ft.	Liters	Cu. Ft.	Liters	
DS 1500 Std Cab 6'4", 120.5" WB	209.0	5308	81.5	2071	75.9	1927	N/A	N/A	12.2	345	4,757 (5.7L 4x2 ST)
DS 1500 Std Cab 8', 140.5" WB	231.0	5867	81.5	2071	75.7	1923	N/A	N/A	12.2	345	4,725 (3.6L 4x2 ST)
DS 1500 Crew 5'7", 140.5" WB	229.0	5817	81.5	2071	76.5	1943	N/A	N/A	50.6	1432	5,383 (5.7L 4x4 ST)
DS 1500 Crew 6'4", 149.5" WB	237.9	6043	81.5	2071	76.8	1951	N/A	N/A	50.6	1432	5,184 (5.7L 4x2 ST)
DS 1500 Quad 6'4", 140.5" WB	229.0	5817	81.5	2071	76.5	1943	N/A	N/A	35.6	1007	5,319 (5.7L 4x4 ST)
DT 1500 Crew 5'7", 144.5" WB	232.9	5916	82.0	2084	75.6	1921	N/A	N/A	-	-	5,317 5.7L 4x4
DT 1500 Crew 6'4", 153.5" WB	241.8	6142	82.0	2084	75.6	1919	N/A	N/A	-	-	5,612 5.7L 4x4
DT 1500 Quad 6'4", 140.5" WB	228.9	5814	82.0	2084	75.6	1919	N/A	N/A	-	-	5,242 5.7L 4x4
DJ 2500 Std Cab 8', 140.5" WB	230.4	5852	81.5	2071	79.4	2018	N/A	N/A			6,321 (5.7L 4x4 ST)
DJ 2500 Crew 6'4", 149.5" WB	237.3	6028	81.5	2071	80.1	2034					7,637 (6.7L Dsl 4x4 ST)
DJ 2500 Crew 8', 169.5" WB	259.4	6588	81.5	2071	79.9	2030					7,818 (6.7L Dsl 4x4 ST)
DJ 2500 Mega 6'4", 160.5" WB	248.4	6310	81.5	2071	80.0	2032	13.5	382			8,048 (6.7L 4x4 SLT)
D2 3500 Std Cab 8', DRW 140.5" WB	230.4	5851	96.4	2449	78.4	1992	N/A	N/A			7,653 (6.7L Dsl 4x4 ST)
D2 3500 Crew 6'4", 149.5" WB	237.4	6030	81.5	2071	80.0	2031					7,628 (6.7L Dsl 4x4 ST)
D2 3500 Crew 8', DRW 169.5" WB	259.4	6588	96.4	2449	78.9	2005					8,167 (6.7L Dsl 4x4 ST)
D2 3500 Mega 6'4", 160.5" WB	248.4	6310	81.5	2071	79.9	2029	13.5	382			8,027 (6.7L Dsl 4x4 SLT)
DP 4500 REG CAB 60" CA	236.3	6002	93.2	2367	81.7	2075			-	-	7,985 (6.7L Dsl 4x2 ST)
DP 4500 REG CAB 60" CA	236.3	6002	93.2	2367	81.7	2075			-	-	7,985 (6.7L Dsl 4x4 ST)
DP 4500 REG CAB 84" CA	260.2	6609	93.2	2367	81.5	2070			-	-	8,137 (6.7L Dsl 4x2 ST)
DP 4500 REG CAB 84" CA	260.2	6609	93.2	2367	81.5	2070			-	-	7,761 (6.7L Dsl 4x2 ST)
DP 4500 REG CAB 108" CA	284.3	7221	93.2	2367	81.5	2070			-	-	7,746 (6.7L Dsl 4x2 ST)
DP 4500 REG CAB 108" CA	284.3	7221	93.2	2367	81.5	2070			-	-	8,256 (6.7L Dsl 4x4 ST)
DP 4500 REG CAB 120" CA	296.3	7526	93.2	2367	81.4	2068			-	-	7,776 (6.7L Dsl 4x2 ST)
DP 4500 REG CAB 120" CA	296.3	7526	93.2	2367	81.4	2068			-	-	7,776 (6.7L Dsl 4x2 ST)
DP 4500 REG CAB 120" CA	296.3	7526	93.2	2367	81.4	2068			-	-	8,317 (6.7L Dsl 4x4 ST)
DP 4500 CREW CAB 60" CA	265.2	6736	93.2	2367	81.4	2068			-	-	8,228 (6.7L Dsl 4x2 ST)
DP 4500 CREW CAB 60" CA	265.2	6736	93.2	2367	81.4	2068			-	-	8,581 (6.7L Dsl 4x4 ST)
DP 4500 CREW CAB 84" CA	289.2	7346	93.2	2367	81.4	2068			-	-	8,368 (6.7L Dsl 4x2 ST)
DP 4500 CREW CAB 84" CA	289.2	7346	93.2	2367	81.4	2068			-	-	8,662 (6.7L Dsl 4x4 ST)
DP 5500 REG CAB 60" CA	236.3	6002	93.2	2367	81.7	2075			-	-	7,761 (6.7L Dsl 4x2 ST)
DP 5500 REG CAB 60" CA	236.3	6002	93.2	2367	81.7	2075			-	-	8,126 (6.7L Dsl 4x4 ST)
DP 5500 REG CAB 84" CA	260.2	6609	93.2	2367	81.5	2070			-	-	7,761 (6.7L Dsl 4x2 ST)
DP 5500 REG CAB 84" CA	260.2	6609	93.2	2367	81.5	2070			-	-	8,126 (6.7L Dsl 4x4 ST)
DP 5500 REG CAB 108" CA	284.3	7221	93.2	2367	81.5	2070			-	-	7,746 (6.7L Dsl 4x2 ST)
DP 5500 REG CAB 108" CA	284.3	7221	93.2	2367	81.5	2070			-	-	8,270 (6.7L Dsl 4x4 ST)
DP 5500 REG CAB 120" CA	296.3	7526	93.2	2367	81.4	2068			-	-	7,776 (6.7L Dsl 4x2 ST)
DP 5500 Reg CAB 120" CA	296.3	7526	93.2	2367	81.4	2068			-	-	8,348 (6.7L Dsl 4x4 ST)
DP 5500 CREW CAB 60" CA	265.2	6736	93.2	2367	81.4	2068			-	-	8,404 (6.7 Dsl 4x2 ST)
DP 5500 CREW CAB 60" CA	265.2	6736	93.2	2367	81.4	2068			-	-	8,734 (6.7L Dsl 4x4 ST)
DP 5500 CREW CAB 84" CA	289.2	7346	93.2	2367	81.4	2068			-	-	8,404 (6.7L Dsl 4x2 ST)
DP 5500 CREW CAB 84" CA	289.2	7346	93.2	2367	81.4	2068			-	-	8,734 (6.7L Dsl 4x4 ST)

This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.

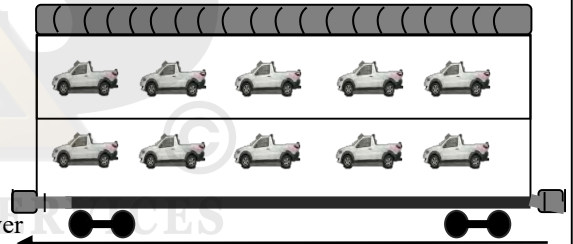


Weights & Dimensions	Overall Length (in)	Width w/ Folded Mirrors (in)	Overall Width w/ Mirrors (in)	Overall Width w/out Mirrors (in)	Height (in)		Wheelbase (in)	Min* Weight (lbs.)	Max* Weight (lbs.)	Approach Angle	Departure Angle
					Highest	Lowest					
Ram SLT/ Club Cab Fleet	174.9	66.7	67.6	65.6	62.6	62.6	107.1	2577	4131	19°	25.6°
Ram Club Cab Adventure	176.2	69.7	70.6	68.6	64.9	64.9	108.5	2754	4187	19°	25.6°

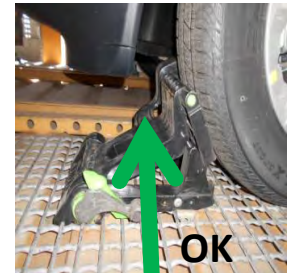


Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- A minimum of 3” roof clearance must be maintained.
- Units must be positioned 3” bumper to bumper and 5” between bumper and end door to allow for a load factor of 6 / deck and to maintain adequate spacing. This is very critical, as there is no room for error.
- Front and rear chocks must be placed in the high setting.
- All chocks must be carefully positioned from the side of the vehicle, never from the front (see photo).
- Exercise caution when entering or exiting the driver’s door on rail due to restricted clearance between door and side panel.
- Do not exceed 5 mph / 8 kmh speed limit when loading and unloading and utilize caution inside the railcar to avoid damage.



Make sure to install chock from the side.



Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.
- Make sure all decks are as level as possible to prevent rocker panel and / or front fascia damage.
- Properly set skid position to prevent front fascia damage.
- **SLOW** speed is essential because this is a low profile model.



This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by ocean. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



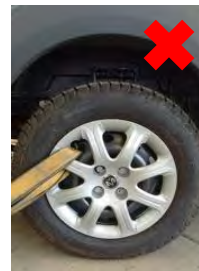
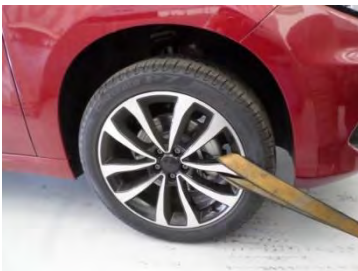
Guidelines for Ocean Transport:

- A stop condition is required when either entering or exiting the ramp.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided to undercarriage/front fascia as this is a low profile vehicle.
- When wheel lashing:
 - All 4 wheels must be lashed.
 - The vehicle should be lashed through the lower quarter of the wheel.
 - Lashing strap can not come in contact with the tire valve.
 - Fix lashing straps to the wheel at an angle of 15 to 45 degrees.
 - Lashing straps are to be run through the spokes of the wheel and cannot be run around the tire.
- Do not lean on or touch unit while securing on vessel.
- Do not carry anything which may cause damage to the units while loading.

Wheel lash in front

Wheel lash in rear

Wheel lash for plastic rim cover



NO

OK

- Hooks and lashings should **NEVER** touch the wheel cover.
- Lashing strap should be installed going from the inside through the wheel arm and come out towards the operator.

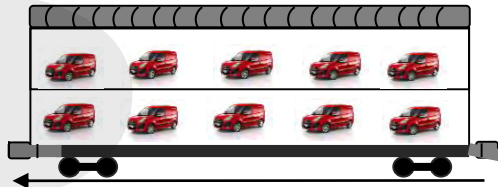
This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by rail or truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Weights & Dimensions	Overall Length	Width with Folded Mirrors	Overall Width with Mirrors	Overall Width without Mirrors	Height	Wheelbase	Min Weight (lbs)	Max Weight (lbs)	Approach Angle	Departure Angle	Breakover Angle
ProMaster City Cargo	186.6"	72.8"	83.5"	72.1	75.9"	108.5	3392	3585	17.2°	29	NA
ProMaster City Passenger	186.6"	72.8	83.5	72.1	74.6"	108.5	3392	3585	17.4	28	NA

Guidelines for Rail Transport:

- Loading is restricted to Bi-level railcars.
- Vehicles are to be uniformly positioned on decks (A = 4, B = 4) to maximize the distance between vehicles and also the distance between vehicles and end doors.
- A minimum of 3" is required between vehicles and 5" between vehicle and end doors.
- A minimum of 3" roof clearances must be maintained.
- Front and rear chocks should be placed in the high setting.
- Exercise caution when entering or exiting the driver's door on rail due to restricted clearance between door and side panel.
- **SLOW** speed is essential when loading and unloading if damage is to be avoided due to the size of the vehicle.



Chocks:

- All units must be secured using a 6-point chocking system.
- Additional chocks should be placed on the inboard side of both the front and rear tire on the same side of the vehicle, alternating sides throughout the railcar.
- When railcars are equipped with supplemental block chocks they should be placed on the inboard side of the remaining two wheels
 - All supplemental block chocks should be placed between 1" and 3" away from the tire.
- When railcars are equipped with supplemental AVR's the 6-point chocking method should still be used.
 - AVR's should only be placed on the outboard side of all four tires and NOT the inboard side.

Guidelines for Haulaway Transport:

- Strap/Soft tie securement only.
- Vehicles are to be uniformly positioned on decks (Lower Deck = 2, Top Deck = 3).
- Be aware of all height restrictions when loading and unloading.




This form outlines the mandatory handling, loading and securing standards for safety and damage free handling when transporting these vehicles by truck. There is a HIGH potential for vehicle damage if this Standard Operating Procedure (SOP) for Loading and Securing is not followed.



Vehicle	Length		Width		Height W/O Accessories		SAE Cargo Area (Behind 2nd Row)		SAE Cargo Area (max)		Curb Weight
	Inches	Millimeters	Inches	Millimeters	Inches	Millimeters	Cu. Ft.	Liters	Cu. Ft.	Liters	
VF Promaster 1500, 118"WB (L1H1)	195.4	4963	80.7	2050	88.7	2254					4568 (3.6L ST)
VF Promaster 1500, 136"WB (L2H1)	213.1	5413	80.7	2050	88.7	2254					4638 (3.6L ST)
VF Promaster 1500, 136"WB (L2H2)	213.1	5413	80.7	2050	99.4	2524					4730 (3.6L ST)
VF Promaster 2500, 136"WB (L2H2)	213.1	5413	80.7	2050	99.4	2524					4781 (3.6L ST)
VF Promaster 2500 Cargo Van, 159"WB (L4H2)	236.1	5998	80.7	2050	99.4	2524					4906 (3.6L ST)
VF Promaster 2500 Window Van, 159"WB (L4H2)	236.1	5998	80.7	2050	99.4	2524					4984 (3.6L ST)
VF Promaster 3500, 159"WB (L4H2)	236.1	5998	80.7	2050	99.4	2524					4920 (3.6L ST)
VF Promaster 3500 Extended, 159"WB (L5H2)	250.5	6363	80.7	2050	99.4	2524					5034 (3.6L ST)

Guidelines for Flatbed Trailer:

- Basket/bikini strap only.
- 4 Straps/unit.
- Make sure straps are not frayed or twisted.
- Straps should not come into contact with any part of the vehicle except the tire.



Guidelines for Haulaway Trailer:

- Basket/lineal strap only.
- Straps must run parallel with tread.
- 4 Straps/unit.
- Make sure straps are not frayed or twisted.
- Straps should not come into contact with any part of the vehicle except the tire.

