

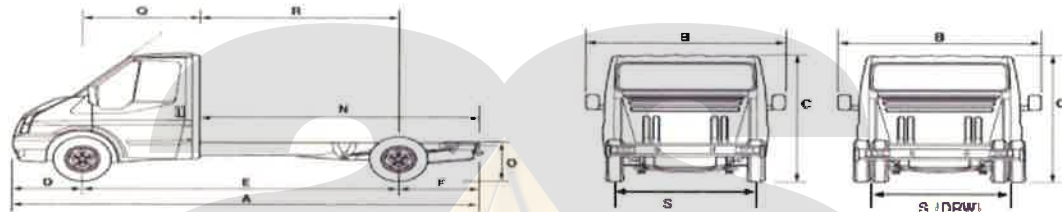
Transit Chassis Cab and Cut-Away Key Dimensions

Summary

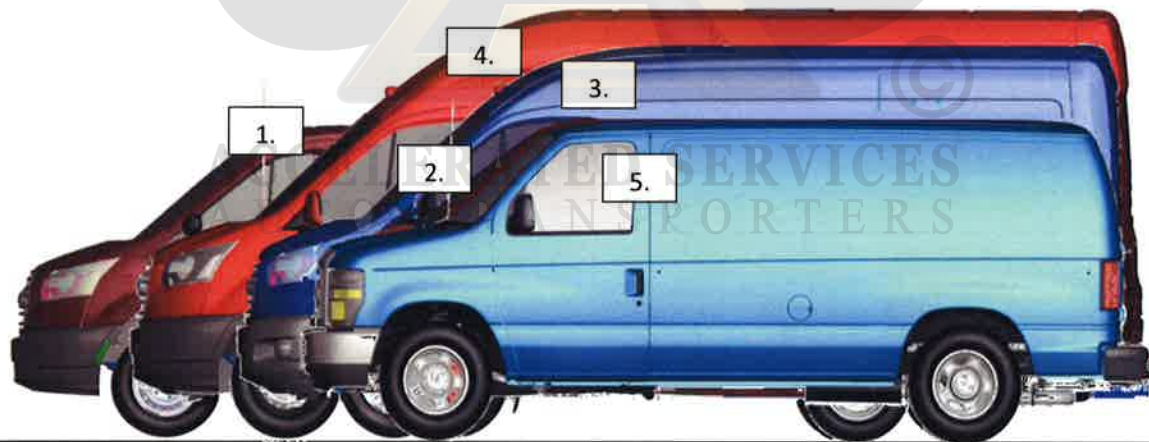
DIMENSION	DESCRIPTION	Transit (Van only, Chassis Cab not included)		
		2.4%	8.6%	3.8%
		MWB	LWB	ELWB-EL
A	Overall length (w/o float)	219.4	237.1	298.4
B	Overall width (w/o float) SRW/DRW no mirrors	79.8	79.8	79.8
C	Overall height (rwd)	86.7	86.7	86.7
E	WB *	138	155.7	178.1
S	Front track	68.8	68.8	68.8
S	Rear track (SRW) (rwd)	68.7	68.7	68.7
S	Rear track (DRW) **	64.7	64.7	64.7

Notes:

S - Track is defined as the distance between 'contact patches' of each tyre
for SRW this is coincident with the tyre centres, for DRW it is the distance between the 'average contact patch' of the 2 tyres, which is coincident with the centreline of the 2 tyres.



V363 Visual Reference Picture



1. Maroon- Extended Long Wheel Base-Extended Length "Super Jumbo"

2. Maroon- Medium Wheel Base-Low Roof

3. Light Purple- Long Wheel Base- Medium Roof

4. Red- Long Wheel Base- Extended Length-High Roof

5. Blue- E-Series Van

Ford Transit Van Haulaway Instructions:

All Transit Models:

Front Tie Down Slots for All Models:

Slotted hole behind the front tire on the transmission cross member:



A-Pull or V-Pull Tie

Non-Trailer Tow Hook Transit Models:

Right Hand Rear Tie Down Slots:

Use outboard slots on frame rail:



A-Pull or V-Pull Tie

Left Hand Rear Tie Down Slots:

Use inboard slots on frame rail:



A-Pull or V-Pull Tie

With Trailer Tow Hook Transit Models:

Right Hand Rear Tie Down Slots:

Use inboard slots on trailer hitch:



A-Pull or V-Pull Tie

Left Hand Rear Tie Down Slots:

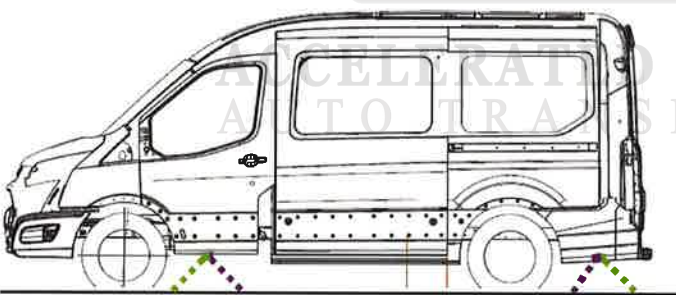
Use outboard slots on trailer hitch:



A-Pull or V-Pull Tie

Transit Chassis Cab and Cut-Away Haulaway Instructions:


Soft straps only are to be used on front and rear tires



For Haulaway, there are no load position restrictions on Transit Van models. For chassis and cut-away units, use soft straps only. Do not load Transit cutaways in backward orientation on the B deck. Do not adjust the seat back cushion to the point where it makes contact to the plastic closure.

Apply either a "A-Pull" or "V-Pull" using Ford approved T-hooks OR soft strap.

Transit Loading Aid

	Bi-Level Loading Aid		
	Minimum Requirement is 4 per Deck		
Low Roof 	87" A-Deck Loadable	89" A-Deck Loadable	B-Deck Loadable
Driving through Railcar	Stay centered Obey loading speed limits, under 5 mph Ensure vehicle does not hit Railcar Door Control Arm (photo 1)	Stay centered Obey loading speed limits, under 5 mph Ensure vehicle does not hit Railcar Door Control Arm (photo 1)	Stay centered Obey loading speed limits, under 5 mph
Driver Door	Caution opening Driver Door Railcar Door Edge Bumper may not protect vehicle's door edge from hitting railcar wall Take additional precaution (photo 2)	Caution opening Driver Door Railcar Door Edge Bumper may not protect vehicle's door edge from hitting railcar wall Take additional precaution (photo 2)	Caution opening Driver Door Vehicle Door's Top Corner may hit the railcar ceiling allowing only 7" clearance for egress Take additional precaution
Sliding Side Door	Caution opening Sliding Door may impact angled B-Deck support Take additional precaution (photo 3)	No Concerns	No Concerns
Rear Door (Van only)	Trailing vehicle needs to allow for rear door to open, minimum 30" (photo 4)	Trailing vehicle needs to allow for rear door to open, minimum 30" (photo 4)	Trailing vehicle needs to allow for rear door to open, minimum 30" (photo 4)

Preferred loading is on the B-Deck, with egress through the sliding side door.
 If the unit has Cargo style side doors with clear access to the rear, loading on the B-Deck, with egress through the Rear Door is acceptable.
 If the unit is a Wagon / Bus with Cargo style side doors, preferred loading is on the A-Deck, with egress through the Driver Door.
 Rear Door egress is not an option for the Wagon / Bus variant.
 Low Roof is mixable with other Bi-Level product.




Photo 1




Photo 2




Photo 3





Photo 4

ACCELERATED SERVICES
AUTO TRANSPORTERS

Transit Unloading Aid

Low Roof



Bi-Level Unloading Aid

Minimum Requirement is 4 per Deck

	87" A-Deck Able to Unload	89" A-Deck Able to Unload	B-Deck Able to Unload
Driving through Railcar	Stay centered Obey loading speed limits, under 5 mph Ensure vehicle does not hit Railcar Door Control Arm (photo 1)	Stay centered Obey loading speed limits, under 5 mph Ensure vehicle does not hit Railcar Door Control Arm (photo 1)	Stay centered Obey loading speed limits, under 5 mph
Driver Door	Caution opening Driver Door Railcar Door Edge Bumper may not protect vehicle's door edge from hitting railcar wall Take additional precaution (photo 2)	Caution opening Driver Door Railcar Door Edge Bumper may not protect vehicle's door edge from hitting railcar wall Take additional precaution (photo 2)	Caution opening Driver Door Vehicle Door's Top Corner may hit the railcar ceiling allowing only 7" clearance for entry Take additional precaution
Sliding Side Door	Caution opening Sliding Side Door Top edge may impact angled B-Deck support Take additional precaution (photo 3)	No Concerns	No Concerns
Rear Door (Van only)	Caution opening Rear Door Door may impact trailing vehicle Take additional precaution (photo 4)	Caution opening Rear Door Door may impact trailing vehicle Take additional precaution (photo 4)	Caution opening Rear Door Door may impact trailing vehicle Take additional precaution (photo 4)

Depending on the location of the vehicle and the vehicle type, there are several options for entering the vehicle. Entering through the sliding side door or rear door is the best way to ensure that no damage occurs. When entering through the driver or cargo side door, take caution to ensure that the door does not contact the railcar nor another vehicle. Rear Door entry is not an option for the Wagon / Bus variant.




Photo 1




Photo 3




Photo 4

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Ford Transit Connect

Soft Strap Only

This vehicle can only be shipped using soft strap ties only for car haul. All Transit Connect units will have a speed lip (see photo B below). Extreme care must be used during car haul loading to avoid damage to the speed lip. During rail shipping this unit will ship on bi-level railcars. It will require 4 standard chocks on rail. If the rail cars are equipped with supplemental chocks they should be used.

