

3 TRUCKAWAY POLICIES AND PROCEDURES

3.1 General

To avoid any risk of damage when vehicles are being transported on open equipment, the following driving instructions are to be adhered to:

Dispatching:

The route selection to the desired destination is to be selected, taking the vehicle headroom into account (i.e. the maximum extent of height and width) to ensure that construction sites, bridges, tree-lined avenues, roadside structures or foliage cannot impair or damage the load.

Note for passenger vehicles: Loaded transport vehicles must be parked on fenced-in secured company sites belonging to the service provider or on manned car parks/rest stops during the night, at weekends and on public holidays, but also during the day during longer rest periods.

Consideration for special weather conditions:

In special weather conditions (e.g. snow, hail, extreme wind, falling trees and branches or ice), the route or driving operation is to be adjusted accordingly and every risk to the cargo is to be avoided.

Driving-related effects:

The driving style must be adapted and must contain an element of foresight to ensure that driving-related effects (such as flying stones) do not pose a risk to the cargo.

Note: All transporters (single and double-decker) are required to be fitted with a stone-guard above the wheels. Other-wise, they are not suitable for transporting MB trucks, MB commercial vehicles or MB passenger vehicles, due to the risk of damage.

Dangerous goods:

Regulation and instructions regarding dangerous goods see chapter 2.4 (Dangerous Goods Regulations).

Transshipments:

Transferring loads between vehicles is forbidden. Exceptions are laid out in writing in the haulage contract. Reloading brought vehicles within the plants is not allowed. Transporters can only take additional load, when brought load will not be changed/alterd.

Note: AMG SLS and AMG GT/GTs

For transporting the SLS AMG and AMG GT/GTs, refer to the separate description in the chapter passenger car specific "Regulations for the handling of new Maybach and Mercedes-Benz SLS AMG vehicles and for special transports".

Driving & Loading at Daimler Locations

The driving and loading personnel must be familiar with the instructions laid out section "3.1 General". During the loading and unloading process, the engine of the auto transporter must be switch off; this does not apply to operations utilizing the hydraulics. Transporting materials of any kind in the vehicles being transported and on the loading surface is strictly forbidden.

3.2 Equipment Guidelines

The condition of the transport equipment (truck & trailer) must be of such a design that there is no risk of damage to the vehicles to be transported. Below are the required equipment guidelines:

- Vehicles should only be transported on equipment that can be adjusted so that loading skids and interior ramp and deck angles do not exceed 8° elevation during loading and unloading.
- Ground skids and trailer tread way areas having sharp or jagged edges must be repaired or replaced to prevent damage to vehicles and injury to personnel.
- Use only flat skids to prevent front spoiler or underbody damage.
- Stone guards must be installed between truck and trailer.
- Repair or replace trailers that exhibit stress cracks, metal fatigue or other structural weaknesses.
- Wash trailers weekly to avoid accumulation of dirt, oil and grease.
- Tie-down straps and deck safety pins must not be bent, cracked, broken, excessively worn or showing signs of structural integrity being compromised.
- Inspect hydraulic hoses for possible leaks or loose connections.
- Locking devices for ramps must be lubricated, rust free and fully operational.
- Hand holds and ladders must be well maintained in order to prevent injury to personnel.
- Trailers should be equipped with fall guards above the cab of the truck.
- Make certain that all other loading and unloading equipment is in proper working condition.
- Never use salt to melt snow or ice in order to gain traction on skids or ramps. Alternate substances to be considered are UREA or CMA.
- The road safety of the tires must be guaranteed. Any damage that arises is to be fixed immediately.
- All loading and unloading equipment as well as load securing equipment shall be stowed and secured properly and safe for road traffic.
- Rusted vehicle and add-on parts as well as rusted loading surfaces on transport vehicles may cause paint damage and must therefore be avoided. Any damage that results is the responsibility of the service provider.

3.3 Shipping Documentation and Inspection Procedures

Main article please see 2.5 Vehicle Hand-over/Inspection.

- All shipping documents must be signed and dated before vehicles will be released except when units have been pre-released, inspected and signed for by your terminal personnel.
- Locate and identify all vehicles assigned to you by matching your shipping order to vehicle window label AND Vehicle Identification Number (VIN) located on the driver's side door pillar or on the lower front windshield.
- It is the responsibility of the carrier to inspect all vehicles prior to moving from parking location and loading. Any exceptions noted during inspection should be brought to the attention of the shipping department when at MBUSA locations for disposition prior to loading. At other than MBUSA locations, an authorized inspection agent must be contacted for damage verification. Carriers will be held responsible for damages noted upon delivery unless previously noted.
- If a proper inspection cannot be executed due to weather conditions this must be noted as well.

NOTE: Be sure to note shortages of spare keys, fuse packets, literature, first aid kits, floor mats, lighter, tool kit, telephone, CD, etc.

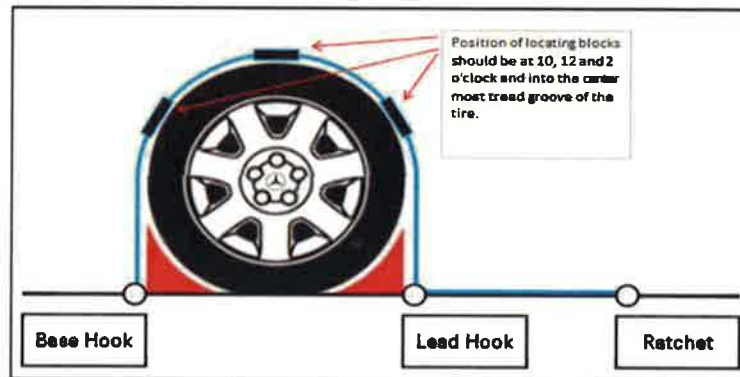
3.4 Load-securing equipment

All passenger car vehicles and vans should be secured with soft tie down straps on all wheels (4 per vehicle) when loaded by truck. Chains are strictly prohibited. Tie down straps should be in proper working condition. Straps should not show any signs of wear and tear or signs that structural integrity could be compromised. Straps should form an upside down U shape over the wheel ensuring no strap contact with the vehicle and no hook contact with the tire.

Tie-down straps with locating blocks

Most tie-down straps come with locating blocks. Straps should be positioned in the center most tread groove and locating blocks should be placed at 10, 12 and 2 o'clock position on each wheel.

Fasten base hook to truck/trailer attaching point. Hook must be fastened vertically below (Thus the best power transmission is guaranteed and no body parts are touched). Run lead hook and ratchet mechanism away from tire. Fasten lead hook into truck/trailer attaching point, also vertically below. Tighten strap with ratchet mechanism (or when using a pan truck, the built in mechanism). Strap must not deform tire and must not make contact with any part of the vehicle except the tire.



Three point tie-down strap with strap controller

Another approved tie-down strap is the three-point tie-down strap with strap controller. The strap to be used must be 2.2 m in length and have maximum expansion of 4%. The lashing force (LC) must be at least 1500 daN in a straight line. The strap must be equipped with a continuous and variable anti-skid band (see fig. Strap controller below). This must be designed in such a way to prevent twisting during transport. It also must be guaranteed that the strap controller is at least as long as half the circumference of the wheel, as the three-point tie-down strap may not touch the tire.



Tie-down strap with continuous, studded strap controller

3.5 Loading and Unloading Preparation

- During the entire loading and unloading process the engine of the truck has to be switched off. This does not apply while using the hydraulic systems.
- Determine the load configuration and position of each vehicle in relation to the number and order of deliveries to be made.
- Selection of loading and unloading areas must have adequate drainage and be level.
 - Ensure that the area is free of vegetation, debris, potholes and low overhangs.
- Set all brakes on tractor/trailer during loading and unloading operations.
- Observe the following guidelines to avoid damage to the front spoiler, bumper, tires and underbody components of vehicles:
 - Clear skids and trailer tread ways of straps, hooks, ratchets, etc. (See pictures below)
 - Ensure that jump skids, ground skids and deck sections are correctly positioned and secure.
 - Do not use loading skids with protruding inside flanges.

- Skids should be long enough and angled to such a degree as to maintain an approximate 8° approach angle for adequate ground clearance during loading and unloading operations.



3.6 Loading Guidelines

- Obey the General Vehicle Handling Guidelines provided by your terminal.
- Height and weight laws set by the DOT must be followed
- Trailer components and tools should never make contact with vehicle body parts.
- Use only low or reverse gears when loading and unloading.
- Keep hands inside vehicle to avoid injuries.
- Vehicle speed must not exceed 5 mph when driving on transport equipment.
- Avoid sudden and excessive application of brakes during loading and unloading.
- Center vehicles on loading ramps and trial tread ways to prevent sidewall chafing and to permit optimal soft-tie angle and tension.
- Jump skids must be utilized when vehicles are traversing distances > 5" between decks.
- Open doors with extreme caution, avoiding contact with trailer structure and components.
- Always exit unit using three-point stance for maximum stability.
- The stack angle must not exceed 25° in order to avoid damages to e.g. engines and oil ducts.
- The following minimum clearances are to be strictly observed for the vehicles to be transported:

Clearance between the vehicles	4" (10 cm)
Deck clearance	2" (5 cm)
Roof clearance	4" (10 cm)
Overlap clearance	4" (10 cm)
Vehicle clearance between the tractor and trailer	6-8" (15-20 cm)



Rule of thumb: One fist clearance

- Vehicles with roof spoilers may only be loaded in the direction of travel.
- The following vehicle key and transmission policies must be strictly obeyed:
 - Place automatic transmission in "Park" and manual transmission in "1st gear".
 - Firmly set the parking brake.
 - Turn off engine by turning ignition key to "LOCK" position.
 - Remove ignition key, lock vehicle and place key in secured area in the cab of tractor.
- Beware of vehicle contact with the upper deck while making load adjustments.
- After loading, driver must measure height of truck to ensure legal height has been achieved.

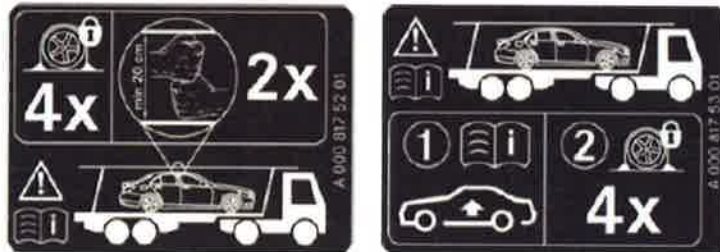
3.7 ADS+ Vehicle Handling/Loading

Special attention is needed when ADS-vehicles (Adaptive Suspension System) are being loaded. With this suspension the vehicle tends to swing/bounce during transport. This will influence the driving behavior of the truck compared to the conventional transports and therefore special lashing requirements are needed.

How do I recognize a vehicle with ADS chassis?



starting from 07.2016: 2. ADS adhesive label at the B-pillar (inner door frame on driver side)



How to load an ADS vehicle?

- No more than 4 ADS vehicles per truck/load
 - Approved for 4 ADS on bottom deck of Cottrell Next Gen Trailers
 - Other trailer manufacturers are approved for 3 ADS vehicles on bottom deck
- No ADS vehicles can be loaded on upper deck
 - Vehicles must be loaded on bottom deck
- Roof clearance must be doubled from 10cm to 20cm (2 fist)

Use of Subcontractors

If subcontractors are employed, it shall be ensured that they are also familiar with the loading instructions and implement them fully. Original tendered carrier will be responsible for subcontractors handling.

Checks during transit

- Load securing for ADS vehicles has to be checked, like all other vehicles, before each departure.
- Lashing must be checked after first 30 miles to ensure vehicle is secured with the proper force.
- Afterwards, check the straps in regular intervals, especially after extraordinary driving situations: emergency braking, swerving, etc. Straps may become loose or break.

Due to these requirements the load factor might decrease. Please make sure to have this documented on the freight documents.

3.8 Key Management

If not otherwise stipulated, all Mercedes Benz passenger vehicles should be locked after being loaded onto the auto hauler. The truck driver is responsible for handling the keys. During transport, the keys of the vehicle being transported are to be kept in a safe/clean place in the cab of the truck. In order to ensure the keys are not exposed to oil or dirt during the loading/unloading and kept in one safe singular place, we specify the use of a key box. Please see image below.



3.9 In-Transit Guidelines

- Prior to departure:
 - Store all unused straps, secure skids and loading stands to prevent vehicle damage; make sure all deck sections are pinned.
 - Measure the load height to verify that the load meets state or local height requirements along the route.
 - Recheck all vehicles for correct tie-down.
 - Confirm that all hoods, trunks, windows are closed and that doors are locked. All keys should be secured by the driver in the cab of the tractor.
- Be aware of low tree limbs, signs, wires and other overhead obstructions while in-route.
- Be aware of sharp dips, rises or bends in the road. Traverse railroad crossing with caution.
- If windshields, door glass or rear glass is broken in transit, cover with plastic sheeting.
- Observe safe driving habits and comply with federal, state and local driving regulations.
- Do not park overnight in public lots, private residences or other unprotected areas with loaded vehicles. Failure to comply with this policy will result in a request for disciplinary action.
 - NOTE: Mercedes-Benz does not approve after hour deliveries unless prior arrangements have been made with the dealer or consignee.

3.10 Unloading and Delivery Guidelines

- Verify unloading area with consignee representative, avoiding congested areas and low-lying overhead obstructions.
- Do not park vehicles under trees or lamp posts in order to avoid defilement and staining.
- Confirm that all hooks and straps have been disengaged and removed from trailer tread ways prior to moving vehicles.
- ALL vehicles MUST be locked after unloading and all keys retained by the driver until an AUTHORIZED CONSIGNEE REPRESENTATIVE is present and accept the delivery and ALL KEYS by countersigning delivery receipts.
- Allow representative at delivery site sufficient time to inspect vehicles. Normal allowance is 15 minutes per vehicle. Ensure that damages and deficiencies are clearly noted on all copies of the delivery receipt and that both representative's sign AND date documents before departing.

3.11 GTP (Global Transportation Platform) - Truck Audits

The "Global Transportation Platform" (GTP) system is the central communication channel for our transport service providers. The following functions will be integrated in successive stages: Loading audit and monitoring of measures, as well as KPI measurement, contact data and logistic profiles. In the first stage loading audits are carried out in GTP and the corresponding data stored and processed. At its launch 01.07.2013 the loading audit application has been implemented in GTP. This application supports the structured and uniform execution of the loading audits, as well as storing and processing the corresponding data.

3.11.1 Loading Audit/GTP

With Loading Audit GTP, the trucks of all service providers are subjected to random checks by MBUSA auditors or on behalf of MBUSA for condition and compliance with internal and external regulations.


Loading Audit GTP serves as official proof of compliance with statutory obligations for the loader. Loading Audit GTP is controlled and monitored by the quality management team. It is a decisive parameter for the calculation of the Key Performance Indicators (KPI).

All the auditors are empowered by MBUSA to order measures or loading prohibitions on behalf of MBUSA. Upon request by Headquarters or the auditor, vehicle documents, permits or other identification documents are to be produced.

The service provider will be audited on the following aspects:

- Driver
- State of equipment (truck/trailer)
- Vehicle Handling

Example of audit criteria is listed below:

Checklist GTP USA Version Car 2.0						
Subcontractor:		Location:		Driver Name:		
License Plate:		Carrier:		Auditor Name:		
				Date:		
		Points	yes	no		
1. Driver						
Proper working clothes (no exposed rivets, zippers, buttons)		3				
Driver wearing high visibility safety vest		3				
Clean working clothes		3				
Proper footwear		3				
Gloves worn when operating on truck		3				
No jewelry, watches, rings (unless w/protection)		3				
No sharp objects in pockets (keys, tools, etc)		3				
Total		21				
2. Truck and Trailer						
2.1 Trailer Condition						
Free from extensive rust		3				
Free from hydraulic oil/excessive dirtiness		3				
Free from damages/sharp edges		3				
2.2 Truck/Trailer Safety						
Fall protection are existing (above cab of truck)		3				
Wheels are covered/Rock Guard installed		3				
Tie-downs according to OM (Damage free)		6				
Total		21				
Warning because of condition		0				
Loading Prohibition because of condition		-25				
3. Handling						
Driver does proper inspection of vehicle before taking ownership		4				
Ramps are set at the proper loading angles (< 8 degrees)		4				
Loading platform is prepared according to OM (free of debris, straps, hooks)		4				
Decks are pinned		4				
Ramps and drive surfaces are level		4				
Gloves are removed before entering vehicle		4				
Vehicles are loaded carefully (proper speed)		4				
Driver enters/exits vehicle with extreme caution		4				
Clearances between vehicles are correct		4				
Tie-downs are attached correctly (including locating blocks)		6				
Vehicles are locked and keys are removed from vehicles		6				
Total		48				
Warning because of handling (driving too fast, improper spacing, missing blocks)		0				
Loading prohibition (damaged unit, hydraulic leak, broken trailer equipment)		-25				
4. Miscellaneous						
ADS equipped vehicles loaded in correct positions with correct spacing (bottom deck)		8				
Driver has spill kit		2				
Total		10				
Total Points Overall		300				
Comments:						

3.11.2 Warnings/Loading Prohibitions

Drivers have the potential to receive a warning or loading prohibition during the audit. If a driver receives either, the trucking company will have to provide proof of corrective action. List of warnings and loading prohibitions are listed below:

Warning due to Truck condition ("yellow card")

- Cause: Faults which are to be rectified before the next loading procedure, but in this individual case does not trigger a loading prohibition.
 - Examples: (bad straps on truck, no fall protection, damaged rock guard)
 - Examples that can be fixed immediately or addressed at a repair shop en route without causing damages to cargo
- Procedure: In the Condition category, the truck is to be registered at the plant for a re-audit in good time according to the Operations Manual. If no feedback is giving within the 14-day notice the warning in GTP is automatically converted to a loading prohibition.

Warning due to handling ("yellow card")

- Cause: Incorrect behavior/handling by the driver which requires immediate rectification, but in this particular case warrants no loading prohibition.
 - Examples: (incorrect ramp angles, speeding while loading, improper spacing, improper tie down securement)

- All can be fixed and addressed on-site by auditor and/or loading supervisor with proper training
- Procedure: For the Handling category, a signed training certification is to be sent to Mr. Holden Nuchols, MBUSA. If no feedback is given within the 14-day notice the warning in GTP is automatically converted to a loading prohibition.
- In individual cases, Daimler AG reserves the right to keep the loading prohibition in place despite proof of training. When changing a driver or vehicle due to a loading prohibition (handling), this shall be reported to Mr. Holden Nuchols, MBUSA.

Loading prohibition due to handling

- Cause: Incorrect behavior/handling by the driver (reckless driving/loading)
- Measure: Truck abandons factory without load
- Procedure: Early training of the driver before the next loading. Once this has been carried out, proof of handling measures must be sent to Mr. Holden Nuchols, MBUSA.

Loading prohibition due to Truck condition

- Cause: Leak (Hydraulic, Oil, etc.) , technical deficiencies, extensive rust or unsafe equipment
- Measure: Truck abandons factory without load
- Procedure: Early repair before the next loading. Once this has been carried out, proof of repair measures must be sent to Mr. Holden Nuchols, MBUSA.

Note: There is also the possibility – after 24h advance notification - for the respective auditors to perform a post-audit in the production facilities. If the auditor should find the condition of the truck to be acceptable, the existing loading prohibition in the GTP system will be annulled. This is not valid for loading prohibitions/ warnings concerning Handling.

On-site training of service provider by loading trainers may be carried out by agreement. Should a truck attempt, in spite of the existence of a loading prohibition, to enter the production facility premises, a new loading prohibition shall be issued.

3.11.3 Post Audit Results

After the conclusion of an audit, the forwarding agent shall be immediately notified by email that an audit has been carried out. The audit is immediately available in the GTP database and can be accessed by the service provider at once. Any further measures are to be coordinated *exclusively* with the central QM team according to OM provisions. Dispatch offices are on no account to be contacted!