

Segment 1: Nelson

Castlegar - Nelson Active Transportation Corridor Vision Plan

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Segment 1 proposes connecting the City of Nelson to the community of Taghum with a separated multi-use pathway. Depending on the routing option selected, this may also connect to the community of Blewett via Granite Road. This segment may also connect to Grohman Narrows Provincial Park, Morning Mountain Regional Park, and Taghum Beach Regional Park. This segment also provides access for multiple neighbourhoods outside the City of Nelson. The City of Nelson acts as the northern terminus of this project area with the corridor terminating at the Nelson Visitor Information Centre.

There are two primary routing options for this segment:

- Routing Option 1 (**Green** and **Pink**): Follows Railway Street and Government Road exiting the City of Nelson. Continues along adjacent to Highway 3A west towards the Taghum bridge. This routing option connects to Grohman Narrows PP, transit stops, and multiple residences and businesses. The pathway would then exit onto Granite Road connecting to Taghum Beach Regional Park towards a newly proposed pedestrian and cycling bridge over the Kootenay River.

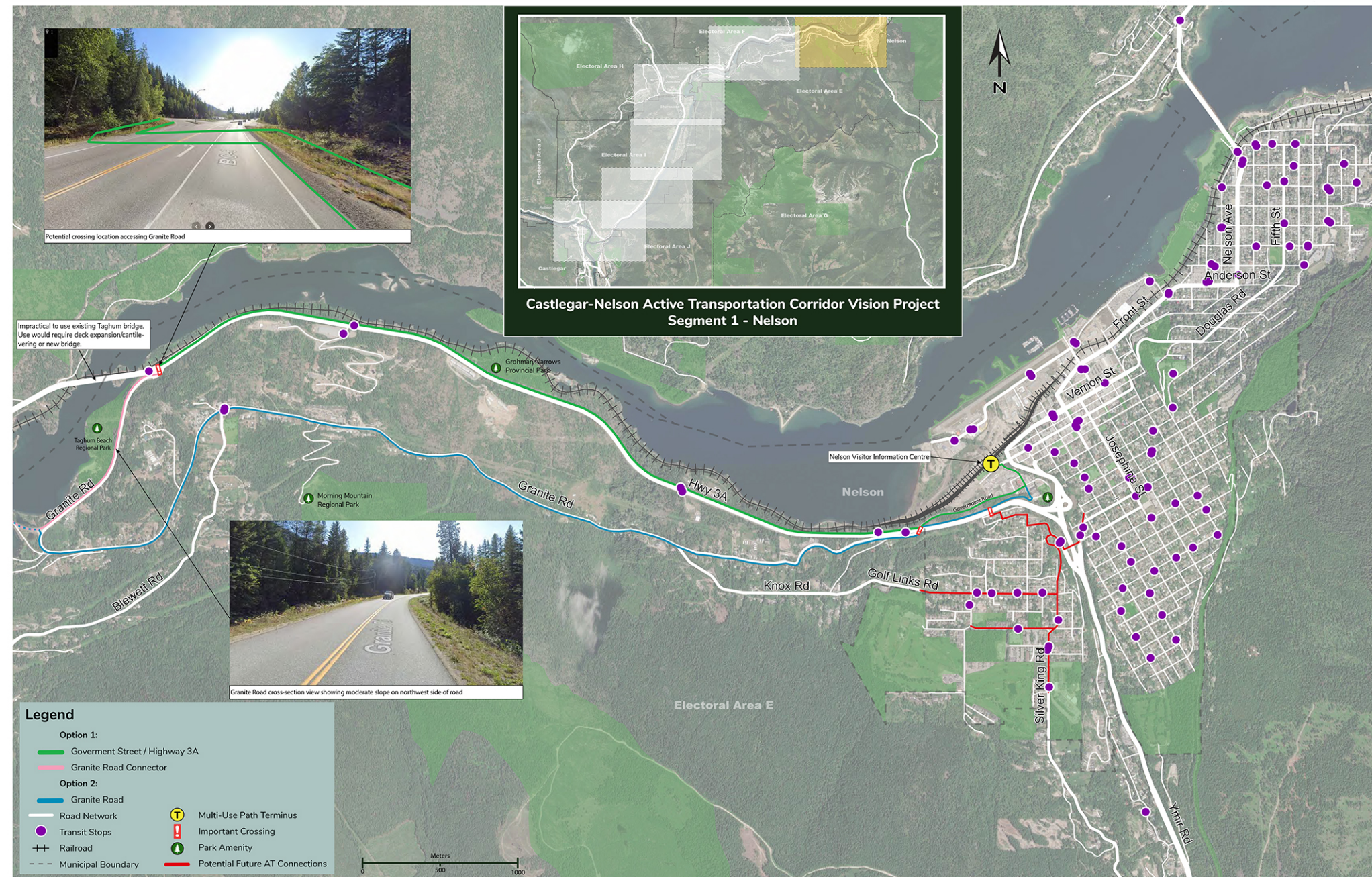
Key Considerations:

- Most direct routing
- 1 crossing of Highway 3A
- Connects to transit stops
- Decreased level of comfort adjacent to Highway

- Routing Option 2 (**Blue**): Follows the same routing as Option 1 exiting the City of Nelson but then crosses Highway 3A over to Granite Road. Continuing along Granite Road, this routing would connect to the community of Blewett and Morning Mountain Regional Park. The pathway would then connect to a newly proposed pedestrian and cycling bridge over the Kootenay River.

Key Considerations:

- Less direct routing
- 1 crossing of Highway 3A
- Connects to Blewett
- Potentially higher comfort away from Highway
- More grade change than Option 1



Tell Us What You Think!
Which option do you prefer? How would you improve them?

Segment 2: Taghum & Bonnington

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Segment 2 proposes a multi-use pathway connecting Taghum and Bonnington. This segment would provide access for multiple residents and businesses within and surrounding these communities. This segment proposes a new pedestrian and cycling bridge across the Kootenay River near Taghum bridge.

There are two primary routing options for this segment:

- Routing Option 1 (**Green** and **Orange**): Crosses the proposed new bridge and follows the Highway 3A alignment on the south side of the Highway. It then enters the Fortis/WK Power hydropower Right-of-Way which runs parallel to the Highway.

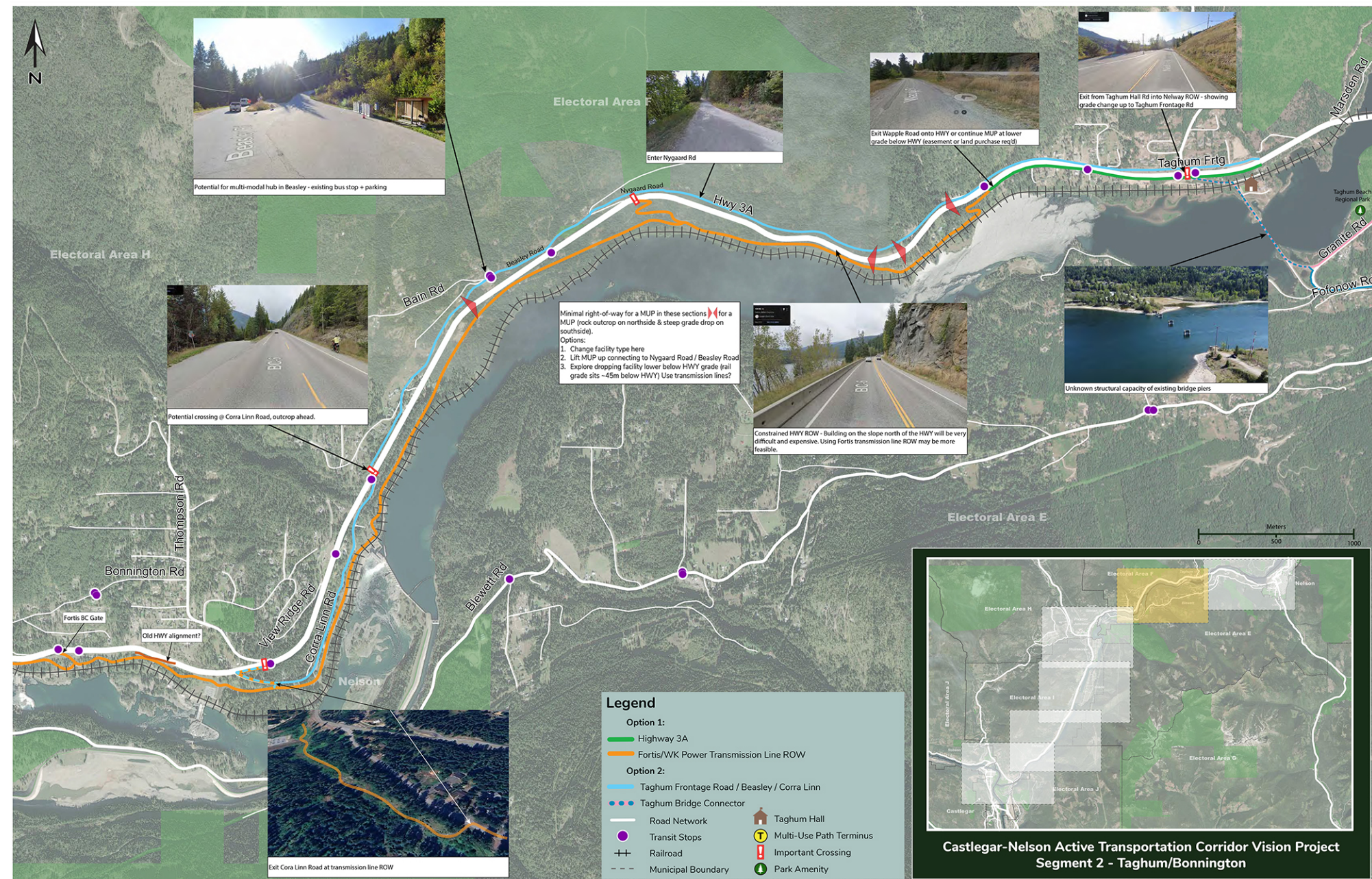
Key Considerations:

- Utilizes existing Right-of-Way
- Highway crossing required for access to Taghum
- Highway crossing required for access to Bonnington
- Highway crossing required for access to Beasley

- Routing Option 2 (**Blue**): Crosses the proposed new bridge and crosses Highway 3A to Taghum. Follows Taghum Frontage Road through the community and then enters a new alignment on a bench above Highway 3A. The pathway would enter Nygaard Road and the community of Beasley. The pathway would cross the Highway, entering Cora Linn Road and then entering the Fortis/WK Power hydropower Right-of-Way as per Option 1.

Key Considerations:

- Less direct routing
- Greater grade changes
- Significant new construction of the new Right-of-Way
- Requires private property easement along the bench
- 2 crossings of Highway 3A
- Highway crossing required for access to Bonnington



Tell Us What You Think!
Which option do you prefer? How would you improve them?

Segment 3: Slocan Junction

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Segment 3 proposes a multi-use pathway connecting Bonnington, Slocan Junction, and Shoreacres. This segment would provide access for multiple residents and businesses within and surrounding these communities. This segment would take advantage of the existing crossing under Highway 3A at the terminus of the Slocan Valley Rail Trail. This segment proposes a new pedestrian and cycling bridge at Shoreacres across the Slocan River. This bridge may be on the east or west side of the existing highway and railway bridges.

There are two primary routing options for this segment:

- **Routing Option 1 (Orange):** Follows the Fortis/WKPower Right-of-Way throughout the segment. This route would pass directly through Slocan Junction and cross the highway further south near Shoreacres. It would then cross the proposed new bridge and follow the Highway 3A alignment on the north/west side of the Highway.

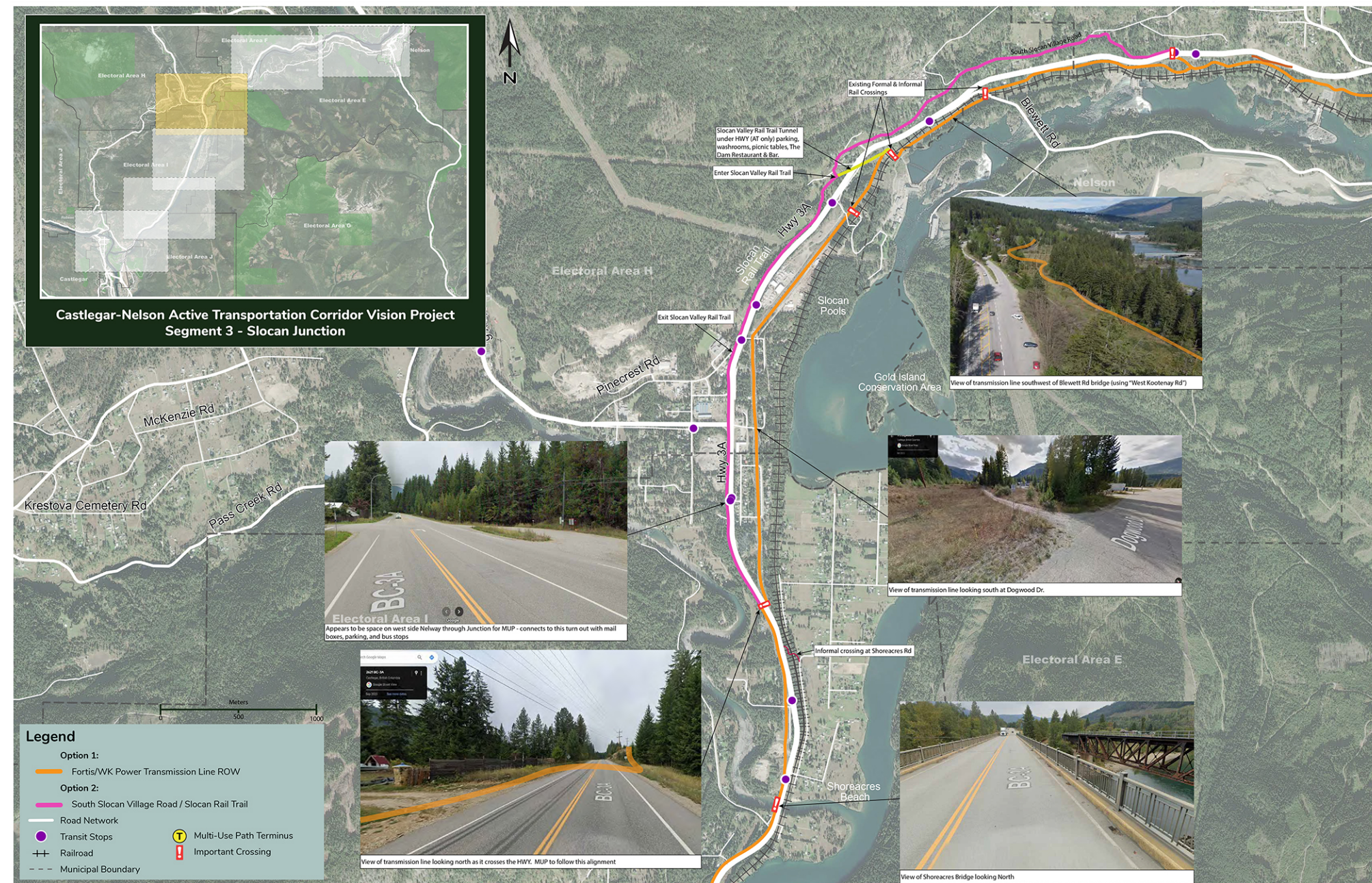
Key Considerations:

- 1 Highway Crossing
- Utilizes existing Right-of-Way
- Connects to Slocan Valley Rail Trail at tunnel crossing
- Railway crossing needed for access to Shoreacres residents
- Other railway crossings require improvements

- **Routing Option 2 (Pink):** Crosses the Highway entering South Slocan Village Road and then enters the Slocan Valley Rail Trail. The pathway would then continue adjacent to Highway 3A on the west side connecting to Option 1 where it crosses.

Key Considerations:

- 1 Highway Crossing
- Utilizes existing Rail Trail
- Better connectivity for Slocan Junction residents
- Decreased level of comfort adjacent to Highway 3A



Tell Us What You Think!
Which option do you prefer? How would you improve them?

Segment 4: Glade & Tarrys

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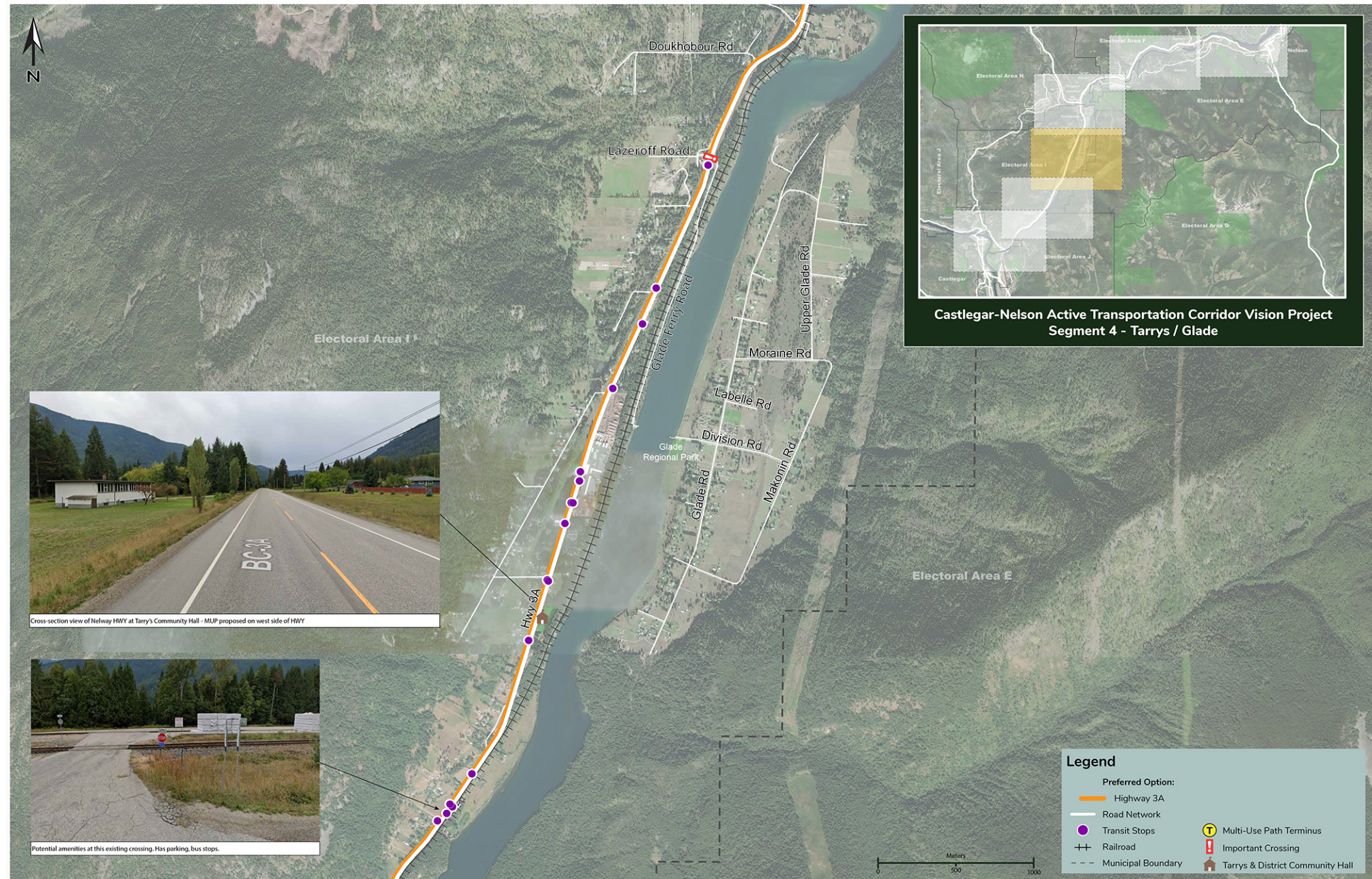
Segment 4 proposes a multi-use pathway connecting the previous segments to the communities of Glade and Tarrys. This segment would provide access for multiple residents and businesses within and surrounding these communities.

One routing option is proposed for this segment:

- Routing Option 1 (**Orange**): Follows the alignment of Highway 3A running adjacent to the roadway. A crossing could be added across Highway 3A at Glade Ferry Road / Lazeroff Road to provide access to the Glade ferry and to the Tarrys Community Hall.

Key Considerations:

- Utilizes existing Highway Right-of-Way
- Provides access for multiple residences on the west side of the Highway
- Highway crossing potential for access to Glade
- Highway crossing potential for access to Tarrys Community Hall
- Decreased level of comfort adjacent to Highway 3A



Tell Us What You Think!
Do you agree with the preferred option?
How would you improve this segment?

Segment 5: Thrums

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Segment 5 proposes a multi-use pathway connecting the previous segments to the community of Thrums. This segment would provide access for multiple residents and businesses within and surrounding these communities.

There are two primary routing options for this segment:

- **Routing Option 1 (Orange):** The multi-use pathway continues along the west side of Highway 3A connecting to the Thrums Market and various residences on the west side of the Highway. The pathway would follow the existing highway alignment. The pathway would then exit the Highway alignment and enter Lookout Road to avoid the narrowed roadway and cliffs north of the Brilliant Rest Area. The pathway would then exit Lookout Road and rejoin the Fortis/WK Power hydropower Right-of-Way at the Brilliant Rest Area, continuing southward along the bench above Highway 3A.

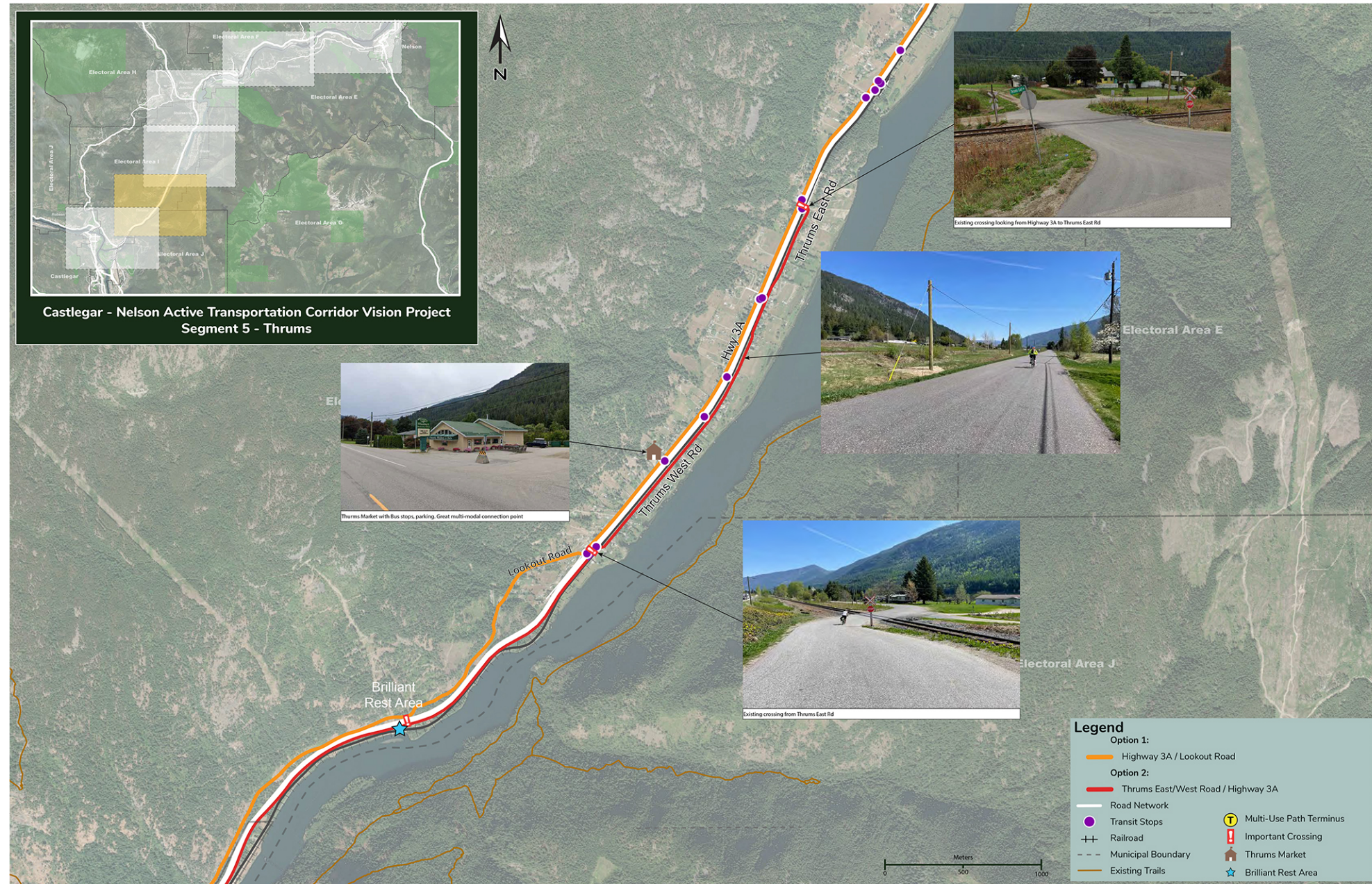
Key Considerations:

- Utilizes existing Highway and road Right-of-Way
- Highway crossing needed for access Thrums East/West Road
- Grade change climbing and descending Lookout Road
- Good connectivity for west side residents
- Private property easements may be required

- **Routing Option 2 (Red):** Follows the same alignment as Option 1 but crosses the Highway onto Thrums East Road and continues on this road south. The pathway would exit Thrums West Road and rejoin Highway 3A continuing south towards the Brilliant rest area.

Key Considerations:

- Less direct routing
- 1+ Highway crossings
- Utilizes existing Highway and road Right-of-Way
- Consistent grade along Highway
- Better connectivity for east side residents
- Decreased level of comfort adjacent to Highway 3A



Tell Us What You Think!
Which option do you prefer? How would you improve them?

Segment 6: Castlegar

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Segment 6 proposes multi-use pathway(s) connecting the previous segments into the City of Castlegar. This segment would provide access for multiple residents and businesses within and surrounding these communities. This segment proposes to take advantage of the existing trail network in Castlegar with the south terminus of the corridor being the CPR Station Museum and/or the Selkirk College Campus.

There are two primary routing options for this segment:

- **Routing Option 1 (Orange):** The pathway continues along the north/west side of Highway 3A via the Fortis/WK Power Right-of-Way connecting to Terrace Road, continuing through the neighbourhood and crossing the Highway to Brilliant Road, entering Option 2 routing. The pathway would continue entering onto the Waldie Island Trail before crossing up, over the Castlegar Robson Bridge, and entering Downtown Castlegar.

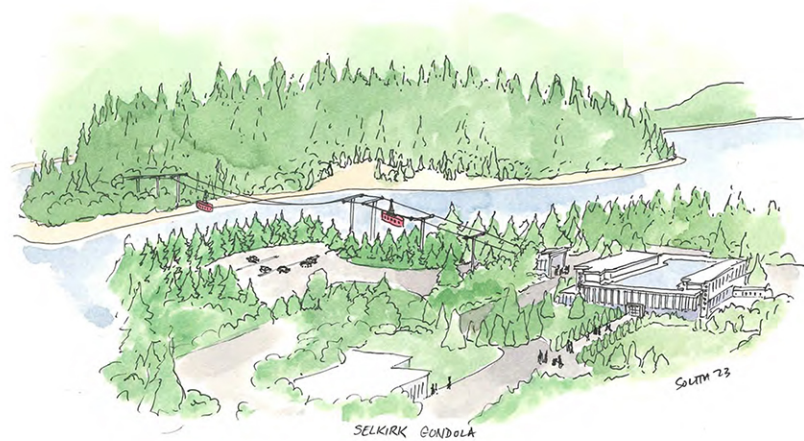
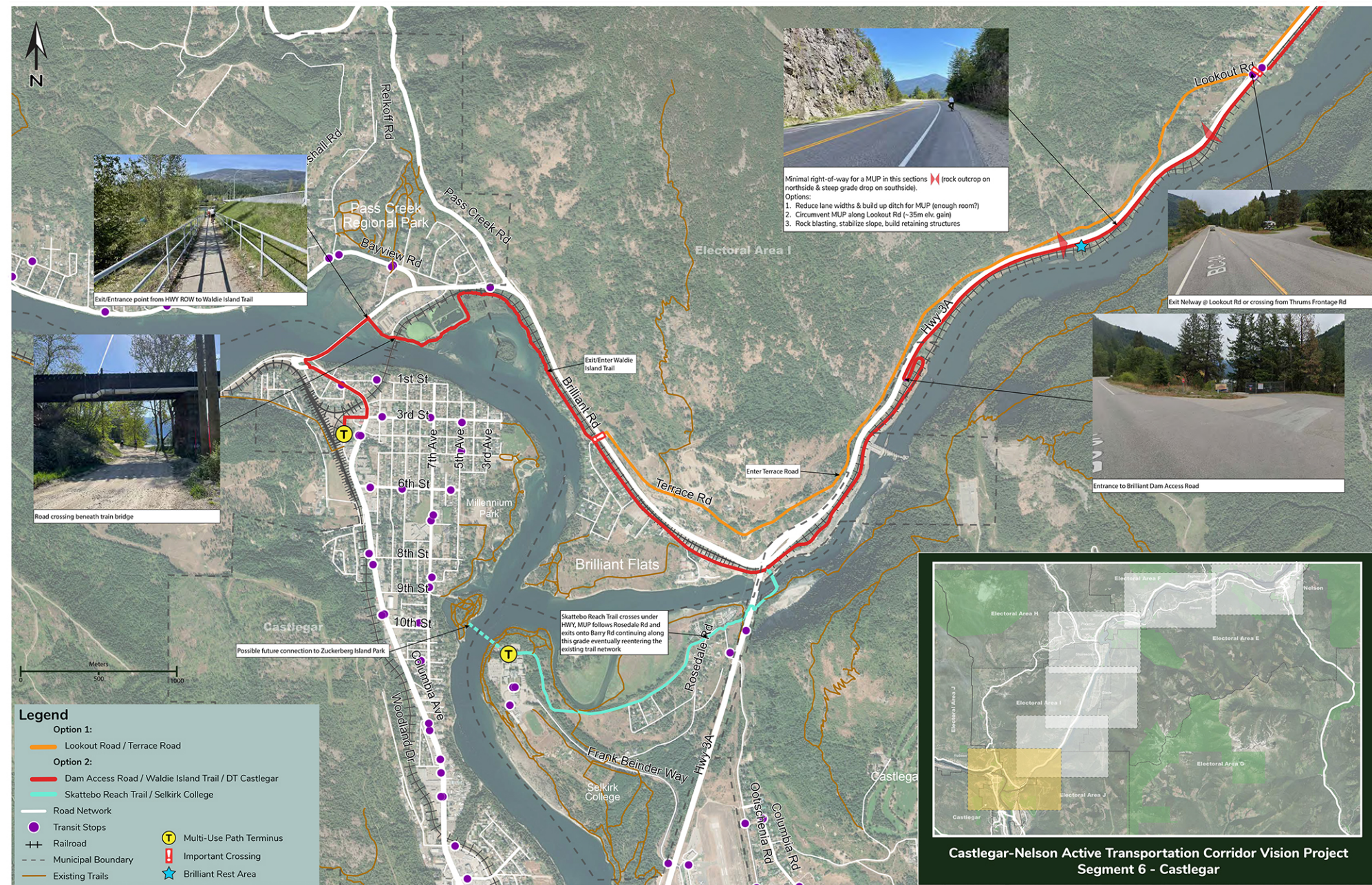
Key Considerations:

- Complicated construction required along the north/east side of Highway 3A
- Utilizes existing Right-of-Way on Terrace Road and Brilliant Road
- Improvements Waldie Island Trail
- Less direct routing and does not connect to Selkirk College

- **Routing Option 2 (Red/Blue):** Pathway continues along south/east side of Highway 3A entering the Dam Access Road. The pathway can continue along the Dam Access Road around under the Brilliant Bridge along Brilliant Road. The pathway then enters the Waldie Island Trail and crosses the Castlegar Robson Bridge as per Option 1. To provide access to Selkirk College Campus, the pathway can for to also cross the Doukhobor Suspension Bridge and enter the Skattebo Reach Trail terminating at the Selkirk College Campus.

Key Considerations:

- Direct routing using existing infrastructure and Right-of-Way
- Improvements to Waldie Island Trail and Skattebo Reach Trail
- Provides access to Selkirk College for students, staff, and faculty
- Decreased level of comfort adjacent to Highway 3A



Tell Us What You Think!
Which option do you prefer? How would you improve them?