



V8 Stock Cars Tour
Car specification rule book
&
Guide
2019

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V8 Tour Series

“A V8 powered racing series that covers the whole country and is open to all V8 stock car drivers, with cheap affordable parts that are reliable and easily accessible.”

The plan is to be inclusive of all the current drivers without a formula or dis-enchanting by their current promoter or organisation.

The overall goal is to have all cars built and powered within a set rule book by 2022, this will mean some cars will need to be modified or replaced but as a club we will work together to help wherever we can. The rules will need to be finalised prior to the beginning of the 2019 season but will not come into effect until 2022. BUT any new car being built to enter the formula prior to 2022 MUST be built to the spec set out in the rules for 2022.

18-20 meeting season.

Season runs from 1st March until 1st November ONLY

A series that at its core is 8-12 meetings on the Northern tracks, 8-12 meetings on the southern tracks with good spacing of events and hopefully weekend meeting to encourage travelling for drivers based further away from events. We have made contact with the F2 Outlaws and they have agreed to help us with the formula and have us run with them at a certain number of meetings. They have been very supportive and have offered to guide us in this new venture.

It will be a Pay to Race formula but that is only because we are a new series and we will need to build a reputation with promoters and tracks. As a club we will be looking to attract sponsors and partners to offset some of the costs of pay to race.

This will be YOUR club, OUR club, it will be in all of our interest to make this work and because of this A certain amount of in house promotion by the drivers and their teams will be required.

Tracks that have shown an Interest

Of the tracks in the Northern area, all the tracks that currently run the F2 Outlaws and Barford have had conversations with the F2 Outlaws said they would be willing to run a small amount of dates.

We are looking to run alongside the F2 Outlaws at roughly 1 meeting per month to begin with. These are, Barford, Brampton, Scunthorpe, and Hunmanby.

Midlands based tracks, Trent is one of the F2 Outlaw tracks, hopefully we will have a meeting or two there. There is also a possibility of a future track in the midlands.

Of the tracks in the South, the promoter at Ringwood has offered us 6 dates next year at Ringwood, and has also said that we are welcome to run at Bourne raceway of his also dates. Bourne is a field track currently, BUT they are looking to lay a shaleway with Armco down in the adjacent field if talks go well in the next few months.

TMC Tongham have also given us 1 date this year.

What will drivers have to do?

Possibly register themselves with the IOPD (International Organisation or Professional Drivers) This will cost £20-£25 and they provide a defence for you the drivers if anyone get hurt through your actions on track that may injure or cause damage to the public.

There is also a movement to form a National Stockcar Association, as a direct comparison to the ORCi, But with a more inclusive, open attitude to oval racing. This may happen in the next few months, so we won't know anymore until later on this possibility.

We recommend that all drivers investigate personal Insurance with a company like Total SPORTS insured, they provide cover for all types of extreme sports stars.



Are now an Affiliate partner of the V8 Stock car Tour and are offering favourable personal injury quotes to our drivers and members. Please ask for the link to use to get the v8 catered policy quote.

Please take the time to get a quote

<https://www.totallysportsinsurance.co.uk/contact.php?vclq0y73>

They are supporting us throughout 2019.

This is because we do not have Ben-fund like the ORCi or FORDS care to support any of us if we have an accident. (Please be aware that the ORCi/BRISca/Spedeworth do not pay you if you get injured, these non-profit charities do)

RACE!

HAVE FUN, ENJOY YOURSELVES. TREAT EVERYONE INSIDE THE GROUP AND OUTSIDE WITH THE SAME RESPECT YOU WISH TO RECEIVE.

Let's build an atmosphere that we can enjoy staying a couple hours after a meeting together with our families for food and drinks. Something we can be proud of. Tough racing on track and friendly off.

Keep in mind the 2022 rule book, we don't want to lose anyone from the series because they were not aware the rule changes were coming and how they would affect them.

We may need to invest in different transponders

We may need to invest in race receivers

Be open minded when it comes to rule changes, these will not be to suit everyone but will be for the best of everyone. There are two – three types of cars possibly going to be used in this series to begin with, each with their own pros and cons, but we will need to make some small alterations to all cars from the outset.

What will we have to do for you?

"WE" the group who are putting this plan together are not perfect, we will make mistakes along the way, BUT we do not plan on making the same mistake twice. We have support from the Chairman of the Outlaw F2's, Promoters and officials from other formulas and more importantly drivers. As a group we will succeed with this project.

Officially Start a club appoint the necessary people for the managing of key roles, or LTD Company

Insure the club

Licensing drivers

Appoint scrutineers

Decide what the best and cheapest way is to bring all cars together on a level playing field initially. (Tyres) (Restrictor plates if needed) (Weight)

Write rules for 2022

Book meetings with tracks

Find partners for the series

Help drivers

Promote the series

Build relationships with the other independent formulas

Build a web presence, social and website that reflects the racing and the product.

Arrange driver meetings, dinner dance and awards evenings.

Things to change straight away

No more grade colours – reason behind this is "tape on wings to change roof grade looks poor, we would rather the cars all look professional all the time. " Drivers will still line up with the highest

graded drivers at the rear but no roof grades. HOWEVER GOLD and SILVER wing colours are reserved for future champions IF wanted.

No other “special roof grades for minor championships” because there will be no other Championships. Apart from each track will have a track Champion.

Wheels

15” x 6” OR 15”x 6.5” width maximum

Tyres

1 make tyre and type, no buffing/ softening/ cutting / pricking or altering in any way from the ones supplied. Tyre to be available from V8 Stock cars tour for £30 each including VAT.



Chase the GOLD

The Gold roof will be awarded to the winner of a One off race at the end of the year, there will be a qualifying system and this race will be started in points order, (highest points at front). Any International entries from our partners will be seeded on to row 3, 6 and 9 respectively.

International drivers will be placed on the grid position from either standings in home country or time trials.

The current idea is for at every meeting the last race will be a “chase for the Gold” qualifier. The points will be totalled up throughout the year leading to a Championship race at the last meeting of the year.

The race will be run over 2X25 Lap races and the first 25 laps start in points order. The second 25 laps start from running order, with only fuel to be added to the car in between the twin 25’s.

This will be our BIG race of the year

The winner of the “chase for the Gold” will be able to carry the gold roof for the whole following year.

SILVER roof

The silver roof will be the overall series point’s winner. And get to carry the silver roof for the whole of the following season. On the occasion that a points champion is numerically crowned before the end of a season, that driver will still have to wait until the following year to carry the silver roof. At no point should there ever be two silver roofed cars on track.

Possible point's breakdown

Below is the points breakdowns, Idea is to give point to all finishers to promote racing all the way through the race.

meeting with up to- 27 cars in attendance					meeting with more than 28 cars in attendance heat split					
finishing position	heat 1, 10 laps	heat 2, 16 laps	final 20 laps	chase for the gold 16 laps	finishing position	heat 1, 16 laps	heat 2, 16 laps	consolation , 10 laps	final 20 laps	chase for the gold 16 laps
	points	points	points	points		points	points	points	points	points
1	25	50	200	100	1	30	30	5	100	50
2	22	45	180	90	2	27	27	4	88	44
3	20	40	160	80	3	25	25	3	80	40
4	19	35	140	70	4	22	22	2	72	36
5	18	30	120	60	5	20	20	1	64	32
6	17	25	100	50	6	19	19	1	60	30
7	16	20	80	40	7	18	18	1	56	28
8	15	19	76	38	8	17	17	1	52	26
9	14	18	72	36	9	12	12	1	48	24
10	13	17	68	34	10	11	11	1	44	22
11	12	16	64	32	11	10	10	1	40	20
12	11	15	60	30	12	9	9	1	36	18
13	10	14	56	28	13	8	8	1	32	16
14	9	13	52	26	14	7	7	1	28	14
15	8	12	48	24	15	6	6	1	24	12
16	7	11	44	22	16	5	5	1	20	10
17	6	10	40	20	17	4	4	1	16	8
18	5	9	36	18	18	3	3	1	12	6
19	4	8	32	16	19	2	2		8	4
20	3	7	28	14	20	1	1		4	2
21	2	6	24	12	21				2	1
22	1	5	20	10	22				1	0
23	1	4	16	8	23				0	0
24	1	3	12	6	24					
25	1	2	8	4	25					
26	1	1	4	2	26					
27	1	1	2	1	27					
28					28					
29					29					
30					30					

Rules up to January 2022 , simply- any v8 engine powered car 15" diameter wheels either 6" OR 6.5" wide wheels, Tyres as per 2022 rule can race in this championship, Any new car built should be done so too the rules set out for 2022, NO short term specials taking advantage of the loose rules to begin with. Any Chevy powered car other than ct350 may incur a restrictor plate if deemed too powerful. Any leaf sprung car can use an adjustable shocker. Cars should be a minimum weight of 1050kgs, and a maximum of 1180kgs.

Lighter cars should be strengthened with bracing to bumpers and nerf rails evenly rather than ballast added. But ballast can be added if necessary.

The car must be front-engine with rear wheel drive. The engine, gearbox, and prop-shaft must be generally on the centre-line of the car and the driver must be seated in the centre, behind the engine and in front of the rear axle. Front and rear axles may be off-set.

Cars must be constructed from steel box section or tube minimum 50x50 4mm thick. All cars must identify the gauge and thickness of their chassis before racing. If a car is deemed too weak/strong, heavy or light the scrutineers hold the right to refuse participation at the event. Please consult a member of the technical team if you are unsure.

GENERAL RULES

- All races will be run in an anti-clockwise direction
- When a meeting format differs from all-in meeting drivers will be allocated their heat races from the v8 tour notice board in the pit area. If qualification for the feature final is necessary the qualifying places will be announced at the beginning of the meeting. (This may be subject to change in the event of low qualifiers from other races.)
- Starting positions will be in reverse points order in the drivers allotted groups, grid will be on v8 tour notice board in pit area. The only race that will be different is the Chase for the Gold Championship final. This race will be lined up in highest point scoring order from the qualifying races. If there are any invitee from other countries these will be allocated grid position on row three.
- All races will begin with a rolling start. The lead car will control the pace of the rolling start but must keep its distance from the pace car. All cars must maintain the distance from the car (S) in front of them. The green flag to start the race will be dropped when the lead car has passed turn 4.
- RE-starts will be in single file and the lead car will maintain a constant speed until the race re-commences. The green flag to start the race will be dropped when the lead car has passed turn 4.
- The start and finish line will be determined by the individual track and will be pointed out before the meetings.
- Once the car enters the circuit it is deemed race ready, NO alterations or repairs will be allowed until the car returns to the pit area. (With exception to the chase for the gold championship final).
- Each driver will be required to attend drivers meeting to be informed of the flags/ rules/ procedures at each track. They may differ from track to track so attendance is mandatory.

The race way will be clearly marked out at each venue, drivers may incur penalties in the form of place deduction/ disqualification if:

- Unnecessarily cutting across the infield
- Gaining an advantage by leaving the track to pass an opponent.
- Cutting the corner, excessively placing wheels of the car over the inside curb of the raceway.
- Braking on the rolling lap/ re-start to gain an advantage
- Leading the rolling lap too fast
- Deliberate fencing
- Ramming a stationary car on or off the track.
- Slowing down of the car behind by bulking / brake testing.

Each track may also have different guidelines on vehicle technical disqualification this may be from a flat tyre, excessive fluid leak, wheel guard becoming loose. The track stewards' decision will be final in these matters.

- A car will be deemed to be “deliberately fenced” when the car(S) has pushed and still in contact with the other car when it hits the fence.
- The offending driver will be disqualified from the event.
- The offending driver will also be asked to appear at a conduct hearing.
- Any car that hits the fence as a result of a spin will not be deemed as being “deliberately fenced.”
- This is a contact formula but common sense needs to be provable with drivers on track behaviour.

End of race

- A race shall be deemed finished when the last car crosses the line after the chequered flag is waived. *The drivers must continue at a race type speed until the red flag is waived.*
- Finishing positions will be shown on the notice v8 tour notice board in the pit area.
- Any dispute to the result can be lodged at the meeting. And or 48 hours after the meeting has finished. If it is lodged after the meeting please do so with evidence and in writing to v8stockcarstour@gmail.com . Any other form of protest will not be accepted.

Driver and team Standard

Acceptable behaviour

The V8 stock car tour expects that all employees and competitors and teams will conduct

Themselves in a professional manner when interacting with others or with the general public. All members of the series should consider their own behaviour and the impact that this can have on others. In person and through the medium of social media. The v8 stock cars tour recognises that personalities, characters and management styles may differ but, notwithstanding these

Differences, as a minimum standard all peoples are expected to:

- Work co-operatively with others in order to achieve objectives
- Manage performance in an appropriate and fair manner
- Give and receive constructive feedback as part of normal race day activities. Such feedback should be evidence-based and delivered in an appropriate manner
- Consider other people’s perspectives in order to help reach agreement
- Establish good working relationships.
- Actively promote positively yours and our corporate partners together.

By holding ourselves to a high standard we will make ourselves and our product more marketable to the general public and companies looking to advertise and join with us in cooperation. As our attitude and behaviour will always reflect on our partners good and bad.

Unacceptable behaviour

Unacceptable behaviour (including bullying, harassment and victimisation), may involve actions, words or physical gestures that could reasonably be perceived to be the cause of another person’s

distress or discomfort. Bullying or harassment may be by an individual against an individual or involve groups of people.

The V8 stock cars tour defines behaviour as being unacceptable if:

- It is unwanted by the recipient.
- It has the purpose or effect of violating the recipient's dignity and/or creating an intimidating, hostile, degrading, humiliating or offensive environment, and
- Having regard to all the circumstances, including the recipient's perception, it was reasonable for the behaviour to have that effect.

Unacceptable behaviour does not have to be face-to-face, and may take many forms such as written, telephone or e-mail communications or through social media.

If a third party who is not a member of the v8 stock cars tour or a driver/ team member

(for example, a customer, a supplier or a visitor) behaves in an unacceptable manner, this should be reported to the relevant member of V8 stock cars tour staff who will determine an appropriate course of action to deal with the issue. If a V8 stock cars tour staff is not able to resolve the issue, the complaint will be taken forward by the Director of V8 stock cars tour ltd.

Some examples of unacceptable behaviour are:

- Aggressive or abusive behaviour, such as shouting or personal insults
- Spreading malicious rumours or gossip, or insulting someone
- Discrimination or harassment when related to a protected characteristic under the Equality Act 2010
- Unwanted physical contact
- Stalking
- Offensive comments/jokes or body language
- Publishing, circulating or displaying pornographic, racist, sexually suggestive or otherwise offensive material or pictures
- Persistent and unreasonable criticism
- Coercion, such as pressure to subscribe to a particular political or religious belief

Bullying and Harassment

Unacceptable behaviour may contravene equalities and/or other legislation.

Whilst bullying and harassment will always be deemed to be forms of unacceptable behaviour, the two terms have distinct and separate legal meanings.

Harassment is connected to anti-discrimination legislation. Therefore if an individual is on the receiving end of unacceptable behaviour which relates to their sex, race, sexual orientation, age, disability, religion or belief or gender reassignment (collectively known as 'the protected characteristics'), this will be deemed to be harassment. Harassment may be established from a single event and a series or pattern of behaviour is not necessary in order to establish that an individual has suffered harassment.

Individuals are also protected from harassment based on someone else's protected characteristic, or based on the perception that they have a protected characteristic.

Harassment, as defined in the Equality Act 2010, is **unwanted conduct related to a relevant protected characteristic which has the purpose or effect of violating an individual's dignity or creating an intimidating, hostile, degrading, humiliating or offensive environment for that individual.**

Bullying is a broader concept which may generally be characterised as: **offensive, intimidating, malicious or insulting behaviour, an abuse or misuse of power through means that undermine, humiliate, denigrate or injure the recipient.** For bullying to be established, there will generally be a series of pattern of events in which one individual has

Demonstrated unacceptable behaviour towards another individual.

With regards to both harassment and bullying, the unacceptable behaviour may be overt (for example, verbal abuse/threats/physical violence) or it may be more subtle and insidious. In either case, unacceptable behaviour is a breach of the University's Dignity at Work Policy and should be reported in accordance with the Dignity at Work Policy,

Victimisation

Victimisation is unfavourable treatment of a person ('the victim') to a detriment because they have:

- brought discrimination (including harassment) proceedings or given evidence or information in connection with such proceedings
- done anything other thing in connection with discrimination (including harassment) proceedings
- made an allegation (whether expressly or otherwise) of discrimination or harassment
- Or because it is suspected that the victim has done or intends to do any of these things.

Unfavourable treatment of a complainant or a Dignity @Work Contact or a witness in relation to a dignity at work complaint which relates to a protected characteristic is likely to be victimisation.

The V8 stock cars tour staff will not tolerate victimisation and a perpetrator will be subject to disciplinary action which may result in action up to and including dismissal or expulsion from the V8 stock cars tour.

What does not constitute Unacceptable Behaviour?

For the avoidance of doubt and for the purposes of this policy,

Invoking V8 stock car tour procedures connected to areas such as conduct, capability or discipline where such procedures are applied reasonably and appropriately will not constitute as

Unacceptable behaviour.

It is important to note that behaviour that is considered bullying by one person may be considered acceptable by another. For instance, legitimate, constructive and fair feedback on a member of staff's performance or behaviour at work is not bullying.

Isolated incidents of unreasonable behaviour such as abruptness, sharpness or rudeness

Whilst unacceptable, will generally not be considered to amount to bullying. However, individuals may want to let the other person know how their behaviour has made them feel in order to avoid a repeat of such behaviour. If the behaviour does continue over a period of time this may be considered to be bullying/harassment.

If a complaint brought under the Dignity at Work Policy is judged to be vexatious or malicious,

Disciplinary action may be taken against the complainant.

However, please note that disciplinary action will not be taken if a complaint made in good faith is judged to be unfounded.

Any driver (or team member) failing to adhere to the above conduct standard will be required to attend a conduct hearing. This will be held between the offending persons and 2 representative of the

V8 Stock cars tour management team. Both parties will be allowed to bring 1 witness each. And if agreed an impartial witness will attend.

The meetings will be recorded and minutes will be kept of the meetings.

If a meeting is not arranged in adequate time the driver will be put on temporary suspension until the hearing is held. **NO DRIVER WILL BE PERMITTED TO RACE IN THE V8 TOUR WHILST ON TEMPORARY SUSPENSION.**

Please remember a driver is responsible for his/her team at all times and a driver's suspension will be put in place because of their teams actions.

The V8 Stock cars tour do not find verbal abuse of any members, track staff, teams, drivers or public acceptable in anyway. This includes via the use of written forms of abuse on social media and or internet forums. Each person involved in the V8 Stock cars tour is an ambassador for the formula and the associate partners and we have a standard to keep too.

Licencing application

When applying for a V8 stock cars tour driver's contract, you will be asked about any previous disciplinary action taken against you in previous formula. It is your responsibility to provide accurate information. We will NOT discriminate against any previous racing penalties you may have received. BUT.

Any ban because of violent behaviour will be viewed on a case by case basis.

Drivers may be asked to put up a behaviour bond if necessary.

Disciplinary against racing indiscretions

Any driver (or drivers) failing to adhere to the racing rules will be required to attend a conduct hearing depending on the severity of the Indiscretion. This will be held between the offending persons and 2 representative of the V8 Stock cars tour management team. Both parties will be allowed to bring 1 witness each. And if agreed an impartial witness will attend.

The meetings will be recorded and minutes will be kept of the meetings.

If a meeting is not arranged in adequate time the driver will be put on temporary suspension until the hearing is held. **NO DRIVER WILL BE PERMITTED TO RACE IN THE V8 TOUR WHILST ON TEMPORARY SUSPENSION.**

At the meeting the V8 Stock cars tour and its management hold the rights at the conclusion of the meeting, If a driver (S) is found guilty of breaking the rules to impose,

- Reprimand in writing with 7 days of the hearing ending
- Fines and or behaviour bonds, notice will be sent in writing within 7 days of the hearing. *The fines and or behaviour bonds will need to be paid in full prior to the driver (S) participation in future V8 stock cars tour events.*
- Suspensions from v8 stock cars tour racing events
- Previous indiscretions may be taken into consideration at the hearing

Technical infringements on v8 stock cars tour race cars

It is the driver's responsibility to enter a race car that adheres to the rules set out in this document. Any infringement on these rules a driver may be required to attend a conduct hearing depending on the severity of the Indiscretion. This will be held between the offending car driver, (if a borrowed car

the car owner ALSO) and 2 representative of the V8 Stock cars tour management team. Both parties will be allowed to bring 1 witness each. And if agreed an impartial witness will attend.

The meetings will be recorded and minutes will be kept of the meetings.

If a meeting is not arranged in adequate time the driver will be put on temporary suspension until the hearing is held. **NO DRIVER WILL BE PERMITTED TO RACE IN THE V8 TOUR WHILST ON TEMPORARY SUSPENSION.**

Penalties for technical infringements will range from

- Result exclusion from results and any of the following.
- Points reduction penalties
- Removal of offending items for evidence at conduct hearing. Items will be marked and documented on removal.
- Suspensions from v8 stock cars tour events.
- Previous indiscretions may be taken into consideration at the hearing.

Appeals

Any conclusion from a conduct hearing may be appealed by the effected parties, this must be lodged in written form either via post and or email within 14 days of the conclusion of the hearing. An appeal will be granted if the parties feel there are other evidence that was not shown and or an unfair penalty was put into place.

Any appeal will be at the cost of the person(S) who have requested said hearing. At a cost of £100.00 to be paid prior to the appeal commencing. This is non-refundable on the result of the appeal.

Protests

Any active driver has the right to formally protest another driver's car if they feel said car is not within the rules set out in this document. The protest must be submitted in written form via post or email. The management or the V8 stock cars tour will then investigate the protest with both parties asked for evidence and reason. This process will be kept confidential between all parties.

The finding of the investigation will be made available to all parties in writing.

Any judgement made by the management will be reviewed at year end.

All protests will automatically be up for discussion to clarify any rules at the end of season AGM, where all drivers will be canvased for their opinion.

Below are the 2022 rules.

DEFINITION

The V8 Stock Car Formula is a purpose-built, open wheel class of racing car, designed for contact racing. The object is to complete the specified number of laps in the shortest time.

- You may push a car from behind, or spin a car to the inside of the circuit.
- You must not deliberately follow a car into the safety fence on the entry to a bend (whether from the rear or the side of the car). This will be deemed to be 'forcible fencing', and carry penalties from load-up to a three-month ban.
- Neither may you lean or spin a car into the safety fence. Unnecessary baulking of a faster car is not permitted.
- Racing is in an anti-clockwise direction.
- The circuit is clearly marked around the in-field, and you are not permitted to let your wheels pass over this demarcation line.

The car must be front-engine with rear wheel drive. The engine, gearbox, and prop-shaft must be generally on the centre-line of the car and the driver must be seated generally in the centre, behind the engine and in front of the rear axle. Front and rear axles may be off-set.

SAFETY EQUIPMENT

CRASH HELMET Helmets must be of a minimum standard as directed by British Oval Racing Safety Executive (B.O.R.S.E).

These are FIA8860-2004, FIA8860-2010, Snell SA2005, Snell SA2010, Snell SA2015, SFI Foundation.

The E2205 European standard helmet may be used in Fibreglass, Carbon or Tri-Composite form only i.e.

NO POLYCARBONATE helmets are allowed. It is important that the helmet fits the driver correctly. Shatterproof goggles/visors must be worn although tinted visors are not advisable.

RACING OVERALLS/FOOTWEAR

You must wear Fire retardant, clean, un-torn and brightly coloured Racing Overalls that are made of a fire retardant material and are clearly marked fireproof. Mechanics are also required to wear clean overalls and will not be permitted on to the track without them being on.

GLOVES

It is Mandatory to wear Fire retardant Gloves also clearly marked fireproof.

Fireproof/flame retardant clothing

It is Mandatory to wear Fire retardant Balaclavas, socks, long sleeved top and long pants clearly marked fireproof. Fire proof footwear at all time in the racing arena.

NECK BRACE

A HANS or equivalent device must be worn whilst racing in the correct manner and fitted correctly.

WET WEATHER CLOTHING

Wet Weather clothing is also recommended and must be worn in addition to, and not instead of the racing overalls

HEAD RESTRAINT NET/WINDOW NETS

You may fit a Head Restraint Net on the on the safety fence side of the race car only. Window nets are optional, if used, they must be of quick release/cloth type only. Net spacing must be 76mm x 76mm maximum.

FIRE EXTINGUISHER

All tow vehicles, may carry a minimum of a 2kg fire extinguisher dry powder or gas, which must be within easy reach of the driver and mechanics at all times, especially when refuelling.

COMPETITION STYLE DRIVERS SEAT WITH HEAD RESTRAINT

You must have a Competition type Driving Seat with a head restraint. The seat should be securely installed and suitable braced supported at shoulder height and on both of the sides and back, with a suitable framework or seat stiffeners.

SAFETY HARNESS

The lap belt/straps and crotch straps should not pass over the sides of the seat, but through it, in order to wrap and hold the pelvic region over the greatest possible surface. It is mandatory to have a 5 point Safety harness with 3" shoulder straps, the lap & crotch straps can be either 2" or 3" in width. All seatbelt connection points must be visible and no belts must pass through any firewall.

A seat belt designed by the manufacturer to be able to use with a HANS type device with 2" shoulder straps expanding to 3" may also be used. BUT only with the specific HANS device that requires such a seat belt.

RACE RECEIVERS

For driver safety race receivers will be investigated and may become mandatory for all drivers whilst in the racing arena. These will be 1 way communication from race control to drivers only.

FINAL PREPARATIONS & COLOURS

Make sure that, in the construction of your car, you have not included any flammable material. The external painting of the car must, at all times, be of professional appearance — preferably in bright colours. All sign-writing must be of a professional nature. Scrutineers are instructed to refuse any car which they consider unsafe, unsightly, or in any way offensive.

Car Construction Rules 2022

Chassis

All cars must be a purpose built ladder type chassis based upon steel main rails. There may only be 2 X main rails and these must be one piece construction. (Only exception being in the case of repair due to damage) Main rails are also considered to be those between the front and rear bumper at roughly 17-18" from the ground.

The box section must be minimum of 60 X 60mm and 5mm thick, and measure a minimum of 710mm apart internally.

Maximum overall length of car 3660mm (144")

Maximum wheelbase 2440mm (96")

Minimum wheelbase 2260mm (89")

Roll cage

A minimum 6 post roll cage must be constructed using a minimum of 1 7/8" OD red band steam pipe, minimum of 4mm thick, cold formed and must be welded to the chassis. The centre posts of the roll cage must be positioned as near to in line with the driver's body as possible. There should be a minimum of 2" clearance between the drivers head with helmet ON and any part of the roll cage. The minimum width of the roll cage at drivers elbow height should be no less than 813mm. There

must be a complete cross member fitted behind the driver's seat, constructed from minimum 40mm box section. There must be a horizontal piece of roll cage material running from front to rear pillar, and a minimum of 3 X chicken bars running vertically from this to the chassis.

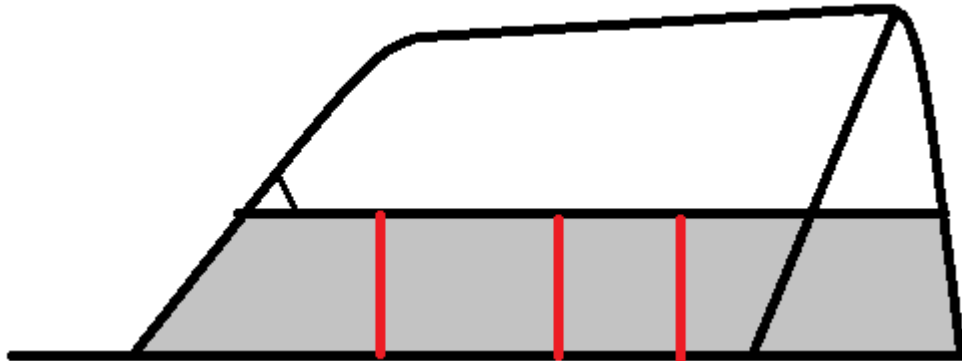


Illustration is for reference and not to scale and is not a guide for overall shape of roll cage,

Cab sides and roof plate must be a minimum of 3mm thick steel plate and must be welded on all four sides. Cab sides must be a minimum of 380mm tall and must cover all 6 legs of the roll cage.

Behind the driver's seat there should be either a "X" or "H" constructed from 40mm box or tube and fully welded into position, the firewall must be welded on all four sides as described below.

Firewalls

A fire wall must be fitted between the driver's seat and the fuel tank, it must be a minimum of 3mm thick. A fire wall must be fitted between the driver's feet and the engine compartment, it must be a minimum of 1mm thick steel or 2mm thick ally. Any holes in these fire wall must be plugged with grommets.

Rear panel

The back panel must be securely fixed in place during race conditions but the fixings used must be of quick release type, i.e. be removable without the use of any tools.

Bumpers

Front bumper should be as wide as the outside measurements of the wheels + 2" maximum.

Rear bumper should be as wide as the rear wheels +4" maximum on the near side and +6" maximum on the offside.

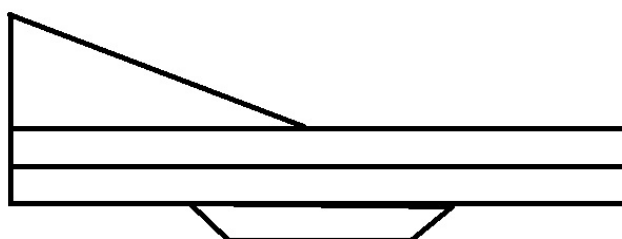
Both bumpers must be a minimum of 140mm deep.

With the car standing on a level surface in race trim with driver sitting in car, the centre line of the bumper measured horizontally should measure 18" +/- 1".

A fence guard must be fitted to both front and rear bumpers.

Must not have any sharp edges!

Illustration is for reference and not to scale:



Nerf rails

All cars must be fitted with nerf rails minimum of 60mm deep should be as wide as the rear wheels +4" maximum on the near side and +6" maximum on the offside.

Must not have any sharp edges!

Wheel guards

A heavy duty spring, or steel bar (or a combination of both), must be fitted to both rear wheels from the inside of the side irons to the rear bumper, and must cover the top of the wheel rim so as to trap the wheel if it were to tear off. The wheel guards can be fitted at both ends with a minimum of 38mm x 13mm high tensile bolts. The rear end of the wheel guard must be bolted or pinned directly to the bumper. The spring must not be heated to bend (cold-bending only if necessary) cars using box tube may have to be welded - the scrutineer's decision is final. The front of the guard can either be bolted or if some form of slider is used a minimum of 38 X 13mm high. Tensile bolt must be fitted to stop it pulling through the slider. Where a guard is attached to the side-rail it must be inside the side rail so that it cannot be removed by other cars or the safety barrier. The security of the spring/bar is the driver's responsibility, and in the event of any breakage to the bar or its fittings during racing, the driver will be excluded. The spring must not be heated to bend (cold bending only, if necessary), welding is not permitted. If they are subsequently damaged and no replacement is available, provided there are no sharp ends, a driver can continue to take part in the meeting without a guard at the discretion of the scrutineer.

Axles

Front axle must be a beam axle. It is permitted to use any standard production Hub modified or any purpose built Hub. Both must be steel. The Cortina Stub Axle is not permitted for use.

Rear axle any standard production axle casing and shafts permitted. Any type of solid axle may be used. No independent rear axles. Axles may be off-set from chassis rails. Axles may have unequal length half shafts. Half shafts, with bearing retaining collars, must be welded.

The only set up allowed on the rear axle is leaf spring and 3 link, of which 1 link bar must be a Panhard rod to run across the rear of the axle casing and join to the chassis. Lengths of the link bars are free but must have no form of springs or cushions built into them.

Leaf springs only, must be fitted parallel to the chassis front and rear.

Differentials must be fully welded. No limited slip or torque slip differentials are allowed.

Springs and shockers

Suspension

Will be up for discussion for all drivers to have input on,

Wheels

(Steel wheels minimum 6mm thick. 14" pressed steel wheel or racing manufactured with replacement centres.

Front wheels and inside rear 14" X 6" maximum) may be removed 2022

15" X 6. OR 15" x 6.5" on all four corners

Wheel spacers are allowed maximum 13mm thick. Wheel stud must completely penetrate standard wheel nut.

(Insert logo for tyre manufacturer here)

Tyres

Kingpin Alpin 195/55/R15 only. This is available for the V8 stock cars tour for £30.00 each inc VAT

Steering

Is free choice

Car weight

Minimum 1050kgs

Maximum 1180kgs

Any ballast must be built into the chassis construction and not be bolt on.



Engine

We have are also in partnership with Chevrolet performance USA.

This means that we are now able to offer all of our Drivers and Members the CT350 circle track engine as specified below cheaper than you will find in speed shops across the globe. We can also offer discount on shipping if we can order multiple motors at once.

Engine Rules

Ct350 small block Chevy engine, must have seals intact from GM.

Engine re-builder will be identified but must be unaffiliated to any one driver and be able to give consistent work and prices to every driver. May be appointed by club and billed through club also.

Club seals will then be applied. Once the engine is opened the driver MUST arrange for club seals to be fitted at the next meeting, this engine will then receive a mandatory strip and dyno test at the end of the year. Any failure to inform of such need for seals and or removal of seals will be deemed as illegal and result in the driver having to attend a conduct hearing.

650 cfm holly carb only part number hly-0-67199BK. Must use jets as supplied and unmodified in any way. Carburettor jetting for this application is #73 primary jets, and #73 secondary metering plate.

Distributor part number will be supplied and must remain unmodified in any way

Air filter is free but will need adapter. Part number all92000

Flex plate part number tci-399173

Starter motor part number pwm9000

Clutch part number will be supplied

No electrical water pumps

Left turn sump as fitted by GM to ct350 8 quarts capacity

The Chevy CT350 motor does not require a MSD box! (Depending on rev limiter findings)

A list of all component parts will be made available.

Throttle must have 2 X return springs fitted.

Engine must be fitted securely in the car in the middle of the main rails. A centre line will be agreed at first inspection and the engine must be within 1" of the centre line measured at the bottom pulley fixing bolt.

BOUNTY PRICE WILL BE MADE FOR ALL ENGINES £4200.00

Driver may only have 1 engine bounty per year. This is where driver (A) is permitted to compulsory purchase a ct350 motor from driver (B) for a set price as long as driver (B) has not already had a "motor bounty" for at least 12 months. And a replacement engine is available so driver (B) can compete at the next event. The "Bounty price" will need to be set to compensate driver (B) for the time it will take to re-install a new motor.

*Bounty motor will only include items included in CT350 crate motor, NOT extra items fitted by driver.ie: headers, water pump, carburettor, flex plates, clutches and flywheels. **There will also have to be the use of common sense during this situation to make sure no-one misses any meetings.***

Gearbox

Free, manual gear box.

Brakes

Must operate efficiently on 3 wheels, and can be of any type. Biased braking is permitted, aluminium callipers are not permitted. Competition pads may be used. Drilling of discs is permitted.

AEROFOILS/REAR WING/ROOF SPOILER

Referred to in this paragraph mean one wing and two fins.

SIZES: Only one rear wing is permitted, not exceeding 1219 x 1219mm (48" x 48"). It may be fitted with one nearside and one offside fin, not exceeding 1524mm (60") horizontally and 609mm (24") vertically. The aerofoil must be constructed in aluminium or fibre-glass only. Steel brackets/bracing can be used to secure the wing and fins. FITTING: There must be a visual gap of 76mm (3") minimum between the roof and the lowest part of the wing when looking from the side elevation.

The aerofoil **must** be on the centre-line of the car, $\pm 50\text{mm}$ (2"). Adjustable wings are permitted, but this must be done externally and not from the cockpit.

An aerofoil may be fitted in front of the engine block and behind the front bumper, central to the chassis, 1.4m (36") wide by 45.7cm (18") in length, and must not obstruct driver's sight-line. Side fins must be of equal size and position, and should not exceed length of aerofoil by more than 50mm either end, and be more than 20.3cm (8") deep. Front and Rear Spoilers/Splitters are permitted below wheel centre-line.

BATTERIES & ELECTRICAL SYSTEM

Battery (ies) must be securely fixed and covered with a rot-proof material if they are not of a sealed type.

A battery box, OR adequate clamps. Should be used to secure the batteries to the chassis

Contact between the safety harness and battery must not be possible.

Battery must be a minimum of 152mm (6") from the fuel tank. The battery position is free and a maximum of two batteries are allowed. Oversized batteries being used as ballast are not permitted. A battery master switch must be mounted on the dashboard and within easy reach of the driver and clearly marked "ON/OFF" or have an Electricity Danger Decal.

IMPORTANT NOTE: Batteries fitted outside the main chassis rail must be fitted against the main chassis with the longer side of each battery against the chassis.

Fuel tanks

All fuel tanks must have a positive means of fixing (metal straps or bolted). Filler caps must not be prone to spillage - no push-on caps. Caps must be metal and secure, or screw-on type. Fuel tanks, including filler caps, must be covered by a metal firewall. Fuel outlet must be from top of tank. A breather pipe, which must incorporate a one-way valve, so it would prevent spillage if inverted) must be fitted. All fuel pipes must be inside vehicle. If there is a floor under the tank, there must be four 50mm holes in the floor, in case of spillage. A fuel shut-off tap must be fitted within easy reach of driver. Fuel lines must be metal, or metal covered. A single electric pump may be used in place of original Fuel tanks must be metal only, or FIA approved. Fuel regulators are permitted. The fuel tank must not exceed 20.5 litre capacity. The tank must be fitted inside the car (between main chassis rails), with adequate protection. . An effective firewall must exist between the tank (including filler cap), and the driver. A single fuel pump with regulator may be used. Secondary fixing on the fuel feed line is required at the carburettor.

A fuel cell is allowed and we will look into the availability and cost, (for safety, these may become compulsory before 2022)

RACING NUMBERS SIDE OF CAR.

(Optional the driver's racing number must appear on both sides of the car. Preferably, these should be in large, American-style numbers on a contrasting background. Background must extend a minimum of 50mm (2") beyond the outline of the number(s).)

AEROFOIL/FIN PLATES.

Numbers should be a minimum of 229mm (9") high by 38mm (1.5") stroke (black on white background), on each side fin so visible from both sides of the car. For V8 Stock Cars with large fin plates: These numbers should preferably be in large, American style numbers on a contrasting background. The background must extend a minimum of 50mm beyond the outline of the number(s). Choice of White on Black OR Black on White.

Drivers name/ nickname

Should be on the front face of the aerofoil/ wing and be easily readable. With your name or (appropriate) nickname clearly sign-written,

Corporate partners

As a tour we have been working very hard to bring down the cost of component parts for the drivers, and teams. We are able to offer discounts, prizes and financial rewards because of these companies and organisations, Therefore we respectfully ask drivers to have on their cars some affiliated partners logos. These may be provided by us. *Any driver wishing to not display these logos are free to choose not to if they wish. But some incentives may not be offered if the logo is not present on the car.*

Rules are correct as of January 7th 2019, any further amendments will be on the website, www.v8stockcarstour.co.uk.