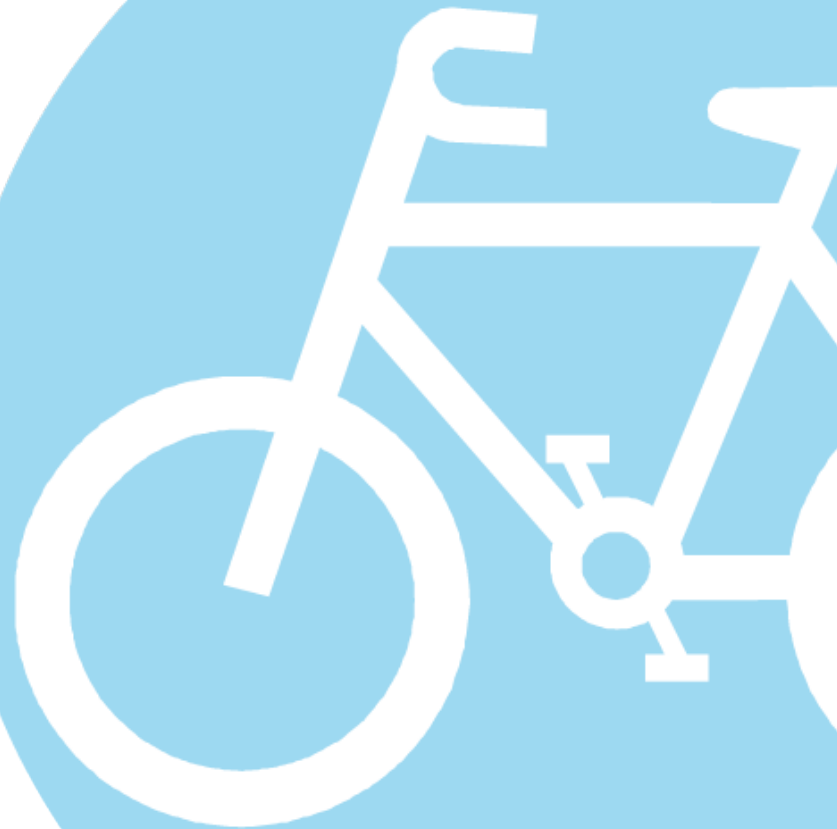


# **5** TRANSPORTATION





PLAN ELEMENT:

# Transportation

# 5

## INTRODUCTION

Ensuring the East End has a functional and appropriate transportation network is a significant goal of this plan. This includes providing access and circulation for all user types, including vehicles, pedestrians and bicycles. This plan element focuses on how all aspects of the transportation system in the East End work together. The following details outline each component.

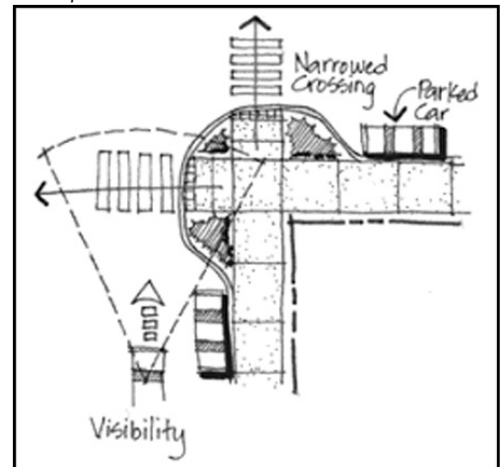
## ROADWAY NETWORK

The most dominant form of transportation currently in the East End is vehicular. The current street network functions properly and no major improvements are proposed to roadways within the study area. However, additions to the network are recommended which would increase pedestrian safety and enhance the visual character of the East End. In addition, speed of traffic along Main and Washington Streets is not inviting to the pedestrian and/or bicyclist. In order to reduce speed and make drivers aware of street activity, a few techniques are recommended. These measures include traffic calming techniques, decorative pavement, parking recommendations, and additional pedestrian crossings.

### Traffic Calming

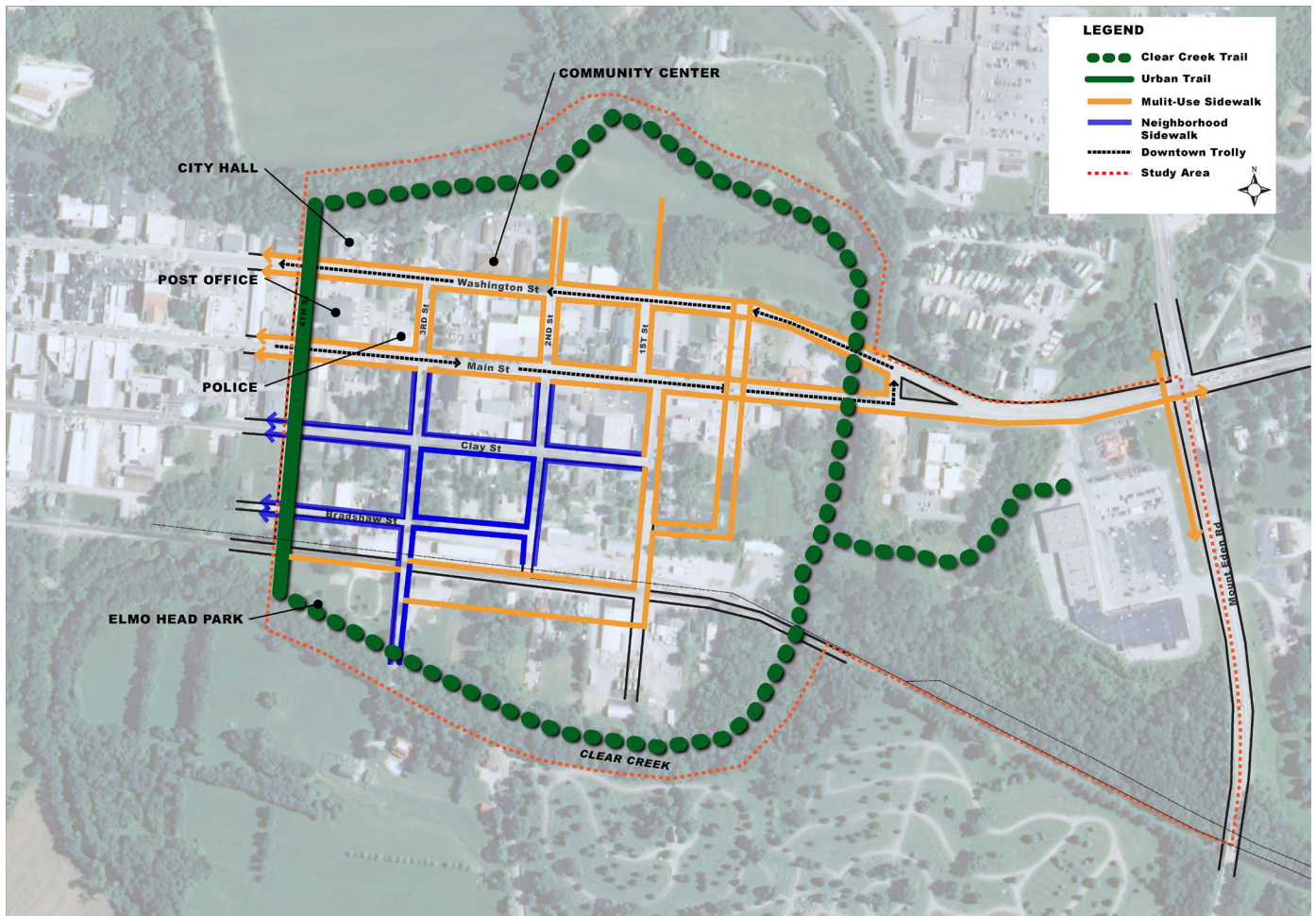
The first is constructing curb extensions or bump-outs where on-street parking is proposed. A curb extension expands the sidewalk into the street at corners or pedestrian street crossings. This traffic calming measure is intended to slow the speed of traffic by physically narrowing the roadway and increasing awareness of pedestrians. It also allows pedestrian and vehicles to see each other when on-street parking is permitted.

*Example Curb Extension*



*Source: Maricopa Association of Governments*

Figure 4-1: Transportation Element Map



### Decorative Paving

The second proposed technique to slow traffic and increase the awareness of nearby pedestrians is changing pavement or using decorative concrete/pavers. This includes vehicular areas within the entertainment district and along the urban trail on Fourth Street. Decorative pavement acts as a visual cue to drivers that something is different in the area and to be aware of pedestrians. This type of pavement treatment is also used at pedestrian crosswalks.



*Decorative pavement used at crosswalks to slow traffic and cue motorists of crossing pedestrians*

### Parking

Parking plays an important role in every downtown. If there is a perception of a parking shortage, it will detract visitors and residents to utilize local attractions. Throughout the East End two types of parking are identified: on-street parking and shared parking lots. Both play an important role in accommodating user needs.

On-street, parallel parking provides direct and easy access to storefronts and is important for business development, expansion and retention. This type of parking also visually narrows the roadway and cues drivers tend to slow down, thus creating a safer pedestrian environment. By providing a physical





obstacle between moving traffic and the pedestrian, it can also provide a streetscape that is more inviting and comfortable. This can encourage users of downtown to walk more between short distances rather than driving. On-street parking is currently used in downtown Shelbyville as well as in certain areas of the East End. This parking type is proposed to be extended from downtown along Main Street and Washington Street from Fourth to First Streets.

Shared parking lots are the second proposed parking strategy for the East End. In higher density areas and areas that maintain a zero lot line setback, such as Main Street and Washington Street, parking lots should be limited to the rear of buildings in order to maintain the desired downtown character. Current parking requirements need to be analyzed to ensure new development does not result in a large amount of under-utilized parking. Shared parking among neighboring business or uses should always be encouraged. All rear parking lots should have a sufficient and safe pedestrian connection to the adjacent major roadway.

Parking lots within parks, shopping center or other uses that do not maintain a zero lot line do not have to provide rear parking. However, these parking lots should be required to maintain a higher landscaping requirement and buffer. All parking lots should be well lit to increase safety and attractively landscaped and buffered to minimize their visual impact. Screening between sidewalks or streets (perimeter landscaping treatments) should be encouraged to reinforce the pedestrian environment.

## Main and Washington Proposed Street Cross Section

The cross section of Main Street and Washington Street in the East End are currently two lane, one-way streets with on-street parking in limited areas. Two 12-foot lanes exist on each roadway and some areas have an existing sidewalk in varying physical conditions. Curbs exist in some areas but overall there is a lack of definition between the vehicular roadway and the pedestrian sidewalk.

As illustrated in Figure 4-2, the proposed cross section of these roadways does not impact the travel lanes; two 12-foot lanes remain in the current one-way layout. An eight-foot parallel parking area on each side of Main Street and Washington Street is being proposed from Fourth Street to First Street with a curb and gutter. This is followed by a suggested 10-foot sidewalk area (minimum eight foot) sidewalk with buried or relocated utilities. Unified pedestrian amenities, such as decorative street lighting and the use of curb extensions, along Main and Washington Street are also recommended to bring the scale of the street down and unify the corridor.

The sidewalk area consists of three components: the carriage strip, the clear zone and the café space/ encroachment zone. The carriage area is adjacent to the roadway and allows for street trees, plantings, benches, trash receptacles, permanent signs, and street lights. The clear zone is adjacent to the carriage strip and should not contain any physical obstructions which may limit pedestrian flow. Finally, the café area/encroachment zone is the area adjacent to the building façade. This area can be used by the private owner for activities including outdoor dining and/or temporary store displays.



*Example of curb extension and on-street parking*

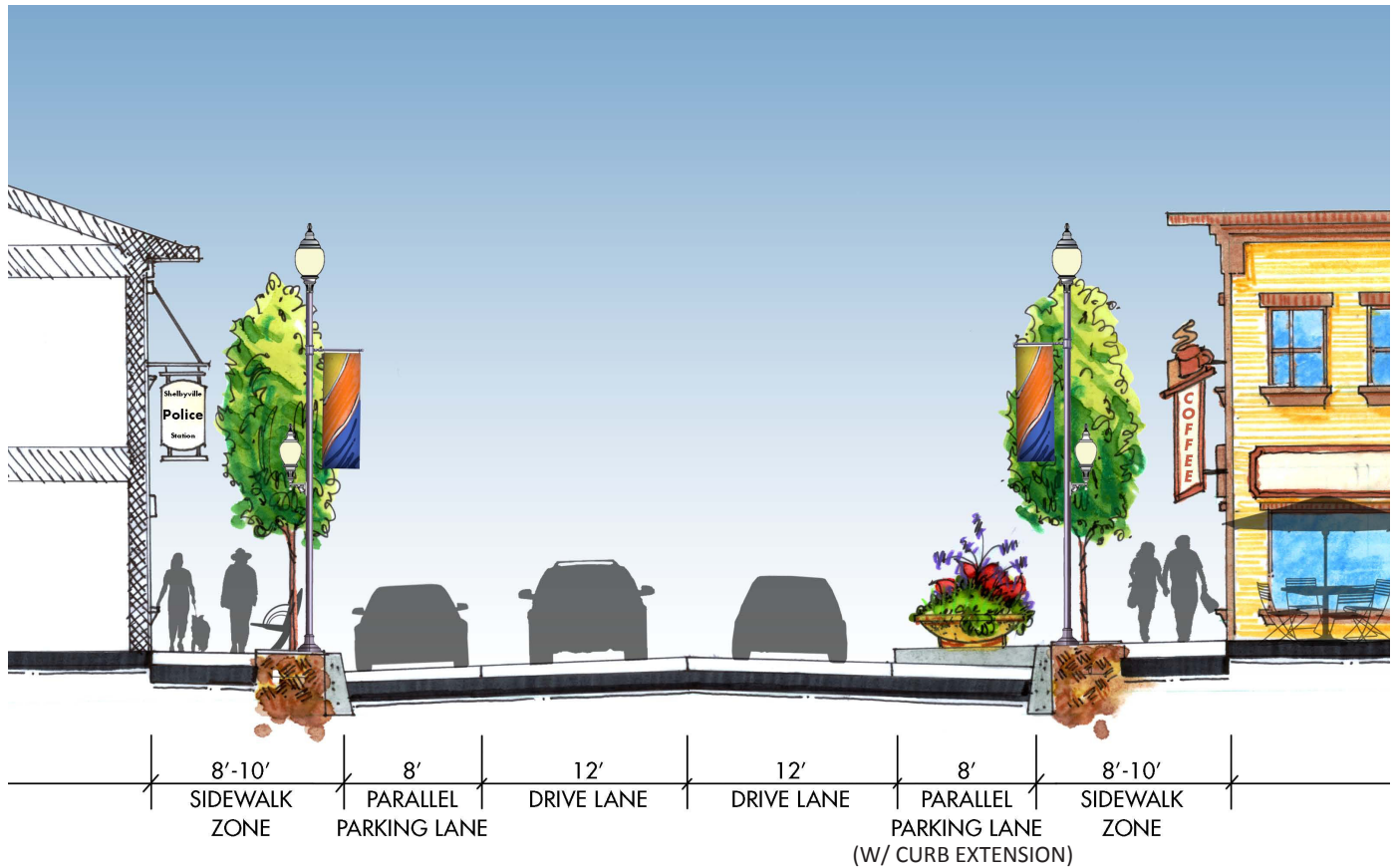


*Example of rear parking lot*



*Existing cross section of street in the East End*

Figure 4-2: Proposed Cross Section - Main and Washington Streets



## SIDEWALK NETWORK

Well defined sidewalks, a continuous sidewalk network, and safe pedestrian crossings are important to the future and vitality of the East End. By defining the separation of the pedestrian and vehicular realm, it will create a more inviting downtown environment. Two types of sidewalks are proposed within the East End: a multi-use sidewalk and a neighborhood sidewalk. All sidewalks and crosswalks should be designed and constructed to be in compliance with ADA requirements.



*Striped crosswalks and decorative pavement*

Multi-use sidewalks are illustrated in orange on the Transportation Element Map (Figure 4-1). The purpose of these sidewalks is to provide access along major connections in the East End. The recommended width is 15 feet with a minimum of 10 feet in areas that are physically constrained. These larger sidewalks accommodate increased pedestrian flow and also allow for amenities such as tree planting, lighting, and street cafes. Pedestrian amenities, including benches, trash receptacles, street lights, signage, landscaping, planters, street banners, and bicycle racks, should be used throughout the East End on all multi-use sidewalks and should be consistent with those used currently in downtown.

Neighborhood sidewalks are proposed along streets with less pedestrian traffic. These sidewalks are smaller in scale, suggested six feet in width, and provide connections to adjacent land uses, multi-use sidewalks, and trails.



Additional sidewalks and pedestrian facilities are needed outside of the East End including connections to downtown to the west and Mount Eden Road to the east.

## TRAIL NETWORK

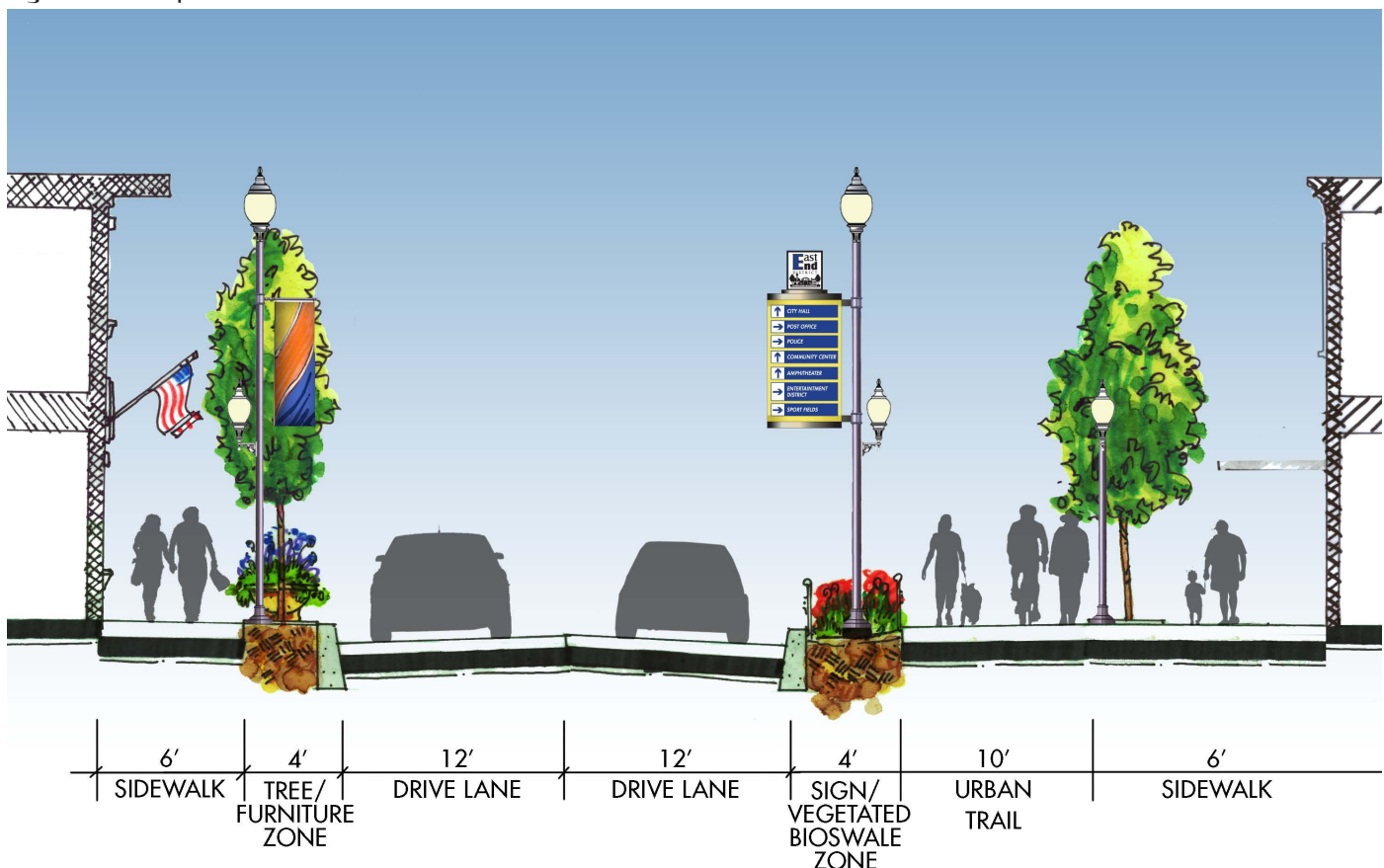
A trail network that reinforces the pedestrian environment and outdoor recreation in the East End is also proposed. As described in Chapter 4, Public Spaces, the Clear Creek trail encompasses the East End and should connect to the larger community, specifically Clear Creek Park.

A unique portion of this trail system is the Fourth Street Urban Trail as illustrated in Figure 4-3. The Urban Trail connects City Hall on the north to Elmo Head Park/Community Park on the south through a ten-foot wide pathway. The trail follows the east side of Fourth Street and is separated with plantings from an six-foot sidewalk. The importance in separating the path from the sidewalk is to also allow bicyclists, runners, and faster pedestrian traffic to move up and down Fourth Street more easily. The street paving material would also consist of decorative paving or pavers to alert drivers that pedestrians are nearby.



Example of urban trail proposed on Fourth Street

Figure 4-3: Proposed Cross Section - Fourth Street Urban Trail







A trolley network, similar to Louisville's Trolley Hop Fridays, is proposed for downtown Shelbyville during special events

## TROLLEY NETWORK

Based on input gathered from the Steering Committee and public, there was a strong desire to create a trolley system within downtown Shelbyville. Initially, the trolley will loop on Main Street and Washington Street to provide a connection between downtown and the East End. This service would only run seasonally or on special events. Over time and if demands is sufficient, the trolley system could be expanded to a larger area within Shelbyville or Shelby County.

## WAYFINDING SIGNAGE

Both pedestrian and vehicular wayfinding signage is important to users of the East End. It allows visitors and residents to easily orient themselves. All signage should be consistent in design and color scheme but should depict the unique districts or areas of downtown, such as the East End. As new destinations are established in downtown, the signage system should be updated.

- Pedestrian Signage – A reliable wayfinding signage system is essential to a successful downtown. It can encourage visitors and residents to walk to destinations by providing accurate and reliable prompts and directions.
- Vehicular Signage – The scale of vehicular signage differs from pedestrian and is intended to direct vehicles to attractions such as public parking or event spaces.

## REGULATORY STANDARDS

In addition to enhancements such as wayfinding signage and decorative pavement, all regulatory standards, including AASHTO, ADA, and MUTCD, need to be upheld. For example, federal regulations dictate the placement and number of regulatory signs associated with a roadway (such as speed limit signs). Wayfinding and decorative signage does not replace regulatory signage but is provided to supplement mandated signs and notices. All public spaces, sidewalks and trails should be designed to be ADA standards and within AASHTO guidelines.

Figure 4-4: Example Wayfinding Signage



