

### **3.0 PLAN DEVELOPMENT & PUBLIC INPUT**



The development of the *Shelby County Bicycle and Pedestrian Master Plan* was a collaborative process facilitated by the project team from the Kentuckiana Regional Planning and Development Agency (KIPDA) working closely with key representatives from the Shelby County community. **Figure 3.1** is a flowchart that details the planning process.



The first step in the planning process was to develop a scope of work and to collaborate with the Shelby County Multi-Use Joint Pathway Committee to steer the project through its development. The Pathway Committee was engaged to assist the project team by serving as a clearinghouse for all information relevant to the plan while also providing a local perspective. The Pathway Committee is comprised of representatives from Shelby County, the City of Shelbyville, the City of Simpsonville, Shelby County Parks, the Shelby County Tourism Commission and the Triple S Planning Commission.

A kick-off meeting was held with the Pathway Committee on September 13, 2016 to introduce them to the project and to elicit their views and insight regarding bicycling and walking needs throughout Shelby County. The Pathway Committee identified key destinations, opportunities, challenges, and amenities for bicycling and walking within Shelby County.

The next step in the planning process was to complete a system evaluation, utilizing all existing data to establish a baseline of current bicycle and pedestrian

accommodations and activity, and to identify opportunities and constraints for future improvements in Shelby County. These steps allowed the project team to develop a comprehensive menu of recommendations aimed at increasing walking and bicycling within the County.

The project team's data collection efforts included a review and analysis of existing relevant reports, resources, mapping, new developments, city and county ordinances, and related projects provided by City, County and State agencies. Key sources of information used in the development of the *Shelby County Bicycle and Pedestrian Master Plan* included the [Shelby County Comprehensive Plan](#), the [Shelbyville Seventh Street Corridor Plan](#), the [Simpsonville Small Area Plan](#), the [Shelbyville East End Study](#) (all of which can be found on the Triple S Planning Commission website), GIS Data (including a facilities inventory conducted by KIPDA), KYTC [Bike Walk](#) resources, and the Federal Highway Administration's [Bicycle and Pedestrian Program Guidance](#).

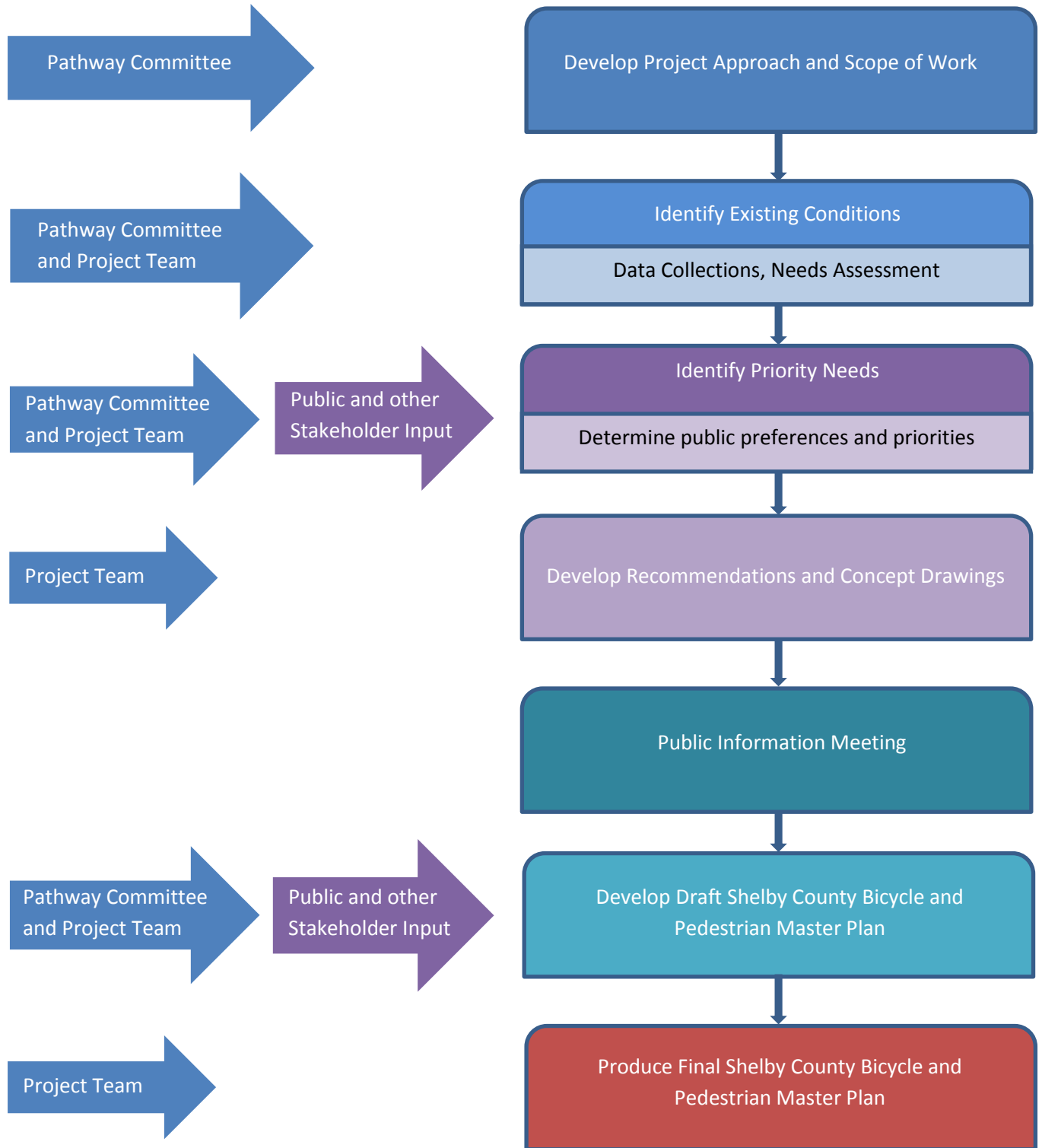
The needs assessment of the County's bicycle and pedestrian transportation system was conducted by reviewing the existing conditions and classifying opportunities for improvement. This evaluation and analysis identified barriers, gaps in connectivity, substandard conditions, and assessed the overall suitability of the existing infrastructure for bicycling and walking.

The third step in the planning process was to conduct public outreach. A public "visioning" workshop was held on October 3, 2016 to inform the public about the plan and to elicit their input for the development of the plan. Participants were presented information by way of mapping stations and were offered the opportunity to comment on the strengths and weaknesses of the existing infrastructure. A public survey was also conducted at the public meeting that addressed the needs, desires, and possible changes needed to enhance bicycle and pedestrian access throughout the community.

Recommendations gathered from those who attended the meeting are represented in **Figure 3.2** and can be located by ID Number in **Appendix A** on **Map 3.1**. Input from the Pathway Committee gathered at the same point in time is represented in **Figure 3.3** and in **Appendix A** on **Map 3.2**.

**Figure 3.1**

### PLAN DEVELOPMENT PROCESS



**Figure 3.2****PUBLIC MEETING COMMENTS**

<b>ID#</b>	<b>Location (see Appendix A - Map 3.1)</b>	<b>Comment</b>
1	KY-53 / I-64	BIKE / PED ACCESS ACROSS I-64
2	KY-53 / I-64	BIKE / PED ACCESS ACROSS I-64
3	KY-53 NORTH OF CITY	NO CONNECTIVITY, HARD TO WALK BETWEEN WASHINGTON STREET AND FREEDOM'S WAY
4	US-60 - MIDLAND TRAIL FROM FREEDOM'S WAY/KY 55 TO FAIRGROUNDS	NO CONNECTIVITY, HARD TO WALK, GAPS IN SIDEWALKS
5	US-60 - MASONIC HOME TO ROCKET LANE	BIKE / PED ACCESS
6	7TH STREET US-60 TO CLEAR CREEK PARK	BIKE / PED ACCESS
7	7TH STREET, US-60 TO CLEAR CREEK PARK	BIKE / PED ACCESS
8	7TH STREET, US-60 TO CLEAR CREEK PARK	BIKE / PED ACCESS
9	7TH STREET, US-60 TO CLEAR CREEK PARK	BIKE / PED ACCESS
10	7TH STREET, US-60 TO CLEAR CREEK PARK	BIKE / PED ACCESS
11	7TH STREET, US-60 TO CLEAR CREEK PARK	BIKE / PED ACCESS
12	7TH STREET, US-60 TO CLEAR CREEK PARK	BIKE / PED ACCESS
13	7TH STREET, US-60 TO CLEAR CREEK PARK	BIKE / PED ACCESS
14	KY-53, FROM RAILROAD TRACKS TO BYPASS	NO CONNECTIVITY, HARD TO WALK
15	KY-53, FROM RAILROAD TRACKS TO BYPASS	NO CONNECTIVITY, HARD TO WALK
16	ST REGIS DRIVE	NO SIDEWALK BETWEEN US-53 AND OSPREY COVE AVENUE
17	BRASSFIELD BOULEVARD	CONNECT TO EXISTING PATH TO GREENWAY
18	KY-55, NORTH OF SHELBYVILLE	SIDEWALK NEEDED FOR SAFE WALKING
19	BAYNE AVENUE	CONNECT NEIGHBORHOOD TO US-60 FOR SAFE WALKING
20	BENSON PIKE, ROCKET LANE TO MT EDEN ROAD	CONNECT NEIGHBORHOOD TO SCHOOLS
21	DISCOVERY BOULEVARD TO FREEDOM'S WAY/KY 55	CONNECT SCHOOLS TO EXISTING SIDEWALK

<b>ID#</b>	<b>Location (see Appendix A - Map 3.1)</b>	<b>Comment</b>
22	US-60 - MASONIC HOME TO ROCKET LANE	BIKE / PED ACCESS
23	RED ORCHARD PARK	NEEDS ACCESS TO DOWNTOWN
24	US-60 - MASONIC HOME TO ROCKET LANE	BIKE / PED ACCESS
25	RED ORCHARD PARK	NEEDS ACCESS TO DOWNTOWN
26	SHELBY COUNTY H.S. TO EAST MIDDLE SCHOOL	SIDEWALK IMPROVEMENTS NEEDED
27	NUMEROUS ROADS	BIKE LANES / SHARROWS TO ACCESS TOWN
28	DOWNTOWN SHELBYVILLE	BIKE RACKS NEEDED
29	KY-1848 AT EXIT 28	UNSAFE AND DANGEROUS FOR PEDESTRIANS TO CROSS THROUGH INTERCHANGE
30	GRAND CENTRAL DRIVE	ADD NEW SIDEWALKS
31	US-60 / FAIRVIEW DRIVE TO MERE LANE	ADD NEW SIDEWALKS
32	US-60 / OLD VEECHDALE ROAD TO KY-1848	ADD NEW SIDEWALKS
33	US-60 / KY-1848 TO SIMPSONVILLE ELEMENTARY	ADD NEW SIDEWALKS
34	ROLLING RIDGE WAY	SIDEWALKS NEEDED TO KY-1848
35	OLD VEECHDALE ROAD	SIDEWALKS NEEDED FROM US-60 TO CHAMPIONS WAY
36	TODDS POINT ROAD	SIDEWALKS NEEDED FROM US-60 TO TODDS STATION SUBDIVISION
37	KY-1848 AND I-64	SIDEWALKS NEEDED TO CROSS OVER INTERSTATE
38	US-60 WESTBOUND	CONNECT TO PARKLANDS OF FLOYDS FORK IN JEFFERSON COUNTY
39	KY-1848	HIGH TRAFFIC SPEED WITH POOR VISIBILITY DUE TO HILLS AND CURVES. SHOULDERS NEED TO BE WIDENED
40	US-60 FROM SIMPSONVILLE TO WEBB ROAD	WIDEN SHOULDER
41	BURKS BRANCH ROAD	TONS OF BICYCLE TRAFFIC PASSING HIS HOUSE
42	BROADEN AND ENHANCE EXISTING BIKE / PED PATH	HIGH TRAFFIC AREA, PATH NEEDS TO BE WIDENED AND PROTECTED

<b>ID#</b>	<b>Location (see Appendix A - Map 3.1)</b>	<b>Comment</b>
43	KY-53 / I-64	MULTI-USE SIDEWALK / BIKE PATH, UNSAFE AREA
44	AIKEN ROAD	NEEDS PATH FOR BIKERS
45	AIKEN ROAD TO LONG RUN PARK	HEAVY BIKE TRAFFIC, NEEDS PATH
46	KY-53 AT BONNIE BRAE DRIVE	UNSAFE AREA, NEEDS TO BE WIDENED OR ADD SIDEWALK / BIKE TRAIL
47	US-60 EAST OF SHELBYVILLE	CONNECT SCHOOLS TO EXISTING SIDEWALK
48	US-60 WEST OF SHELBYVILLE	EXISTING SIDEWALK ENDS IN FRONT OF BIG O TIRES, CONNECT SCHOOL TO EXISTING SIDEWALK
49	KY-53 NORTH OF SHELBYVILLE	NEEDS WIDENING WITH PAVED SHOULDER
50	BURKS BRANCH NORTH TO FOX RUN RD	HARD TO SEE OVER HILL IF TRAFFIC COMING, ADDED PATH OR ROAD WIDENING WOULD BE HELPFUL
51	KY-53 / I-64	BIKE / PED ACCESS ACROSS I-64
52	7TH STREET, US-60 TO CLEAR CREEK PARK	NO SAFE PED / BIKE LANE TO CLEAR CREEK PARK
53	BURKS BRANCH ROAD	NO SAFE BIKE LANE, ESPECIALLY FROM CLEAR CREEK PARK TO BYPASS
54	KY-55 NORTH OF SHELBYVILLE	SAFER BIKE-WAYS FOR THESE CORRIDORS; SHARE THE ROAD SIGNS, BIKES USE SHOULDERS ON KY-55 TO RIDE BETWEEN EMINENCE AND SHELBYVILLE
54A	NORTHERN SHELBY COUNTY	SAFER BIKE-WAYS FOR THESE CORRIDORS; SHARE THE ROAD SIGNS IN NORTHERN SHELBY COUNTY
55	KY-148 WEST	SHELBY TO JEFFERSON COUNTY TO ACCESS PARKLANDS OF FLOYDS FORK, MULTI-USE OR JUST BIKE
56	KY-148 TO KY-44	SHARED ROAD SIGNAGE
57	ZARING MILL ROAD	SHARED ROAD SIGNAGE
58	BONNIE BRAE DRIVE	NEEDS SIDEWALK TO CONNECT TO NEIGHBORHOOD
59	KY-53 FROM I-64 TO US-60	NEEDS SIDEWALK AND WIDENING OF ROADWAY

**Figure 3.3**

## PATHWAY COMMITTEE COMMENTS

ID#	Location (see Appendix A - Map 3.2)	Comment
60	KY-55 / I-64 OVERPASS	NO PEDESTRIAN CROSSING
61	KY-53 / I-64	NO PEDESTRIAN CROSSING
62	KY-53 AT HEN PECK LANE AND RR CROSSING	PEDESTRIAN SAFETY ISSUE
63	MACK WALTERS ROAD AND RR CROSSING	PEDESTRIAN SAFETY ISSUE
64	MACK WALTERS RD AT OLD MILL ROAD AND JONATHAN DRIVE	PEDESTRIAN SAFETY ISSUE AT THE RR CROSSING
65	GORDON LN TO WEISSINGER GOLF COURSE	MULTI-USE PATH THAT CONNECTS GORDON LN TO WEISSINGER HILLS G.C.
66	MACK WALTERS TRAIL TO KENTUCKY STREET	MULTI-USE PATH CONNECTING KENTUCKY STREET TO THE MACK WALTERS TRAIL
67	MACK WALTERS ROAD TO GINKGO TRAIL	MULTI-USE PATH THAT CONNECTS MACK WALTERS ROAD TO GINKGO TRAIL
68	KY-53, FAIRWAY CROSSING TO US-60	NEEDS SIDEWALK ALONG KY-53 FROM FAIRWAY CROSSING TO US-60
69	KY-55, US-60 TO HEARTHSIDE CIRCLE	NEEDS SIDEWALK ALONG KY-55 FROM US-60 TO HEARTHSIDE CIRCLE
70	ST REGIS DRIVE	NO SIDEWALK BETWEEN THORNHILL DRIVE AND KY-55
71	CHAPEL HILL ROAD	NEEDS SIDEWALK BETWEEN CLEAR CREEK ELEMENTARY AND OLD MT EDEN ROAD
72	OLD JAILHOUSE BRIDGE	CONVERT OLD JAILHOUSE BRIDGE TO A PEDESTRIAN BRIDGE
73	NEW CONNECTOR ROAD	NEW CONNECTOR RD FROM OLD FINCHVILLE ROAD TO KY-1871
74	WARRIORS WAY	NEEDS SIDEWALK ALONG WARRIORS WAY FROM PAINTED STONE ELEMENTARY TO WEST MIDDLE SCHOOL
75	KY-53, WARRIORS WAY TO US-60	MULTI-USE PATH THAT CONNECTS WARRIORS WAY TO US-60 ALONG KY-53
76	US-60, DISCOVERY BOULEVARD TO KY-55	NEEDS SIDEWALK FROM DISCOVERY BOULEVARD TO KY-55
77	KY-55X (BYPASS)	MULTI-USE PATH ALONG KY-55X FROM HARRINGTON MILL ROAD TO US-60



ID#	Location (see Appendix A - Map 3.2)	Comment
78	HARRINGTON MILL ROAD	MULTI-USE PATH ALONG HARRINGTON MILL ROAD FROM KY-55X TO KY-53
79	DISCOVERY BLVD TO MIDLAND INDUSTRIAL DRIVE	MULTI-USE PATH FROM DISCOVERY BOULEVARD AND PARALLEL TO MIDLAND INDUSTRIAL DRIVE TO KY-55X
80	KY-55	NEEDS SIDEWALK ALONG KY-55 FROM OLD BRUNERSTOWN RD TO ISAAC SHELBY DR
81	OLD BRUNERSTOWN ROAD	NEEDS SIDEWALK ALONG OLD BRUNERSTOWN ROAD FROM WINDHURST WAY TO KY-55
82	KY-1848 AT I-64	NEED PEDESTRIAN CROSSING ALONG KY-1848 OVER I-64
83	US 60	SIDEWALK NEEDED FROM US 60 BRIDGE TO KY 1848 (TODDS POINT ROAD)
84	MULTI-USE CONNECTOR	MULTI-USE PATH CONNECTION FROM SIMPSONVILLE TO THE LOUISVILLE LOOP (FROM RR TRACKS NEAR CARDINAL CLUB PARALLEL I-64)
85	TODDS POINT ROAD	SIDEWALKS FROM US 60 TO GRAND CENTRAL DRIVE
86	GRAND CENTRAL DRIVE	SIDEWALKS FROM SIMPSONS STATION DRIVE TO KY 1848
87	OLD VEECHDALE ROAD	SIDEWALKS FROM US 60 TO CHAMPIONS WAY
88	CHAMPIONS WAY	SIDEWALKS ALONG CHAMPIONS WAY FROM BRIGHTVIEW DRIVE TO OLD VEECHDALE ROAD
89	ROLLING RIDGE WAY	SIDEWALKS ALONG ROLLING RIDGE WAY
90	US 60, KY 1848 TO SIMPSONVILLE E.S.	ADD NEW SIDEWALKS
91	US 60	SIDEWALKS NEEDED ALONG US 60 FROM KY 1848 TO KINGBROOK PKWY
92	KINGBROOK PARKWAY	SIDEWALKS ALONG KINGBROOK PARKWAY IN THE KINGBROOK INDUSTRIAL PARK
93	KY-1848	SIDEWALKS NEEDED ALONG KY 1848 FROM ARISTOCRAT COURT TO THE OUTLET MALL
94	NEW SIDEWALK / PATH	BEHIND OUTLET MALL NEAR KY 1399, UNDER I-64 TO ARISTOCRAT CT
95	NEW CONNECTOR ROAD	NEW CONNECTOR ROAD FROM KY 1848 NEAR THE SIMPSONVILLE FLEA MARKET



Based on findings from the prior steps, a list of preliminary recommendations for improved bicycle and pedestrian facilities was prepared by the project team. Its intent was to propose safe walking and bicycling access to schools, neighborhoods, civic centers, parks and recreation facilities.

A “visioning” workshop was subsequently held with the Pathway Committee on December 5, 2016. Committee members engaged in a visioning exercise to develop a vision and a set of goals for the plan that depicted a desired future for bicycling and walking in Shelby County. Utilizing the information gathered from the Pathway Committee and public workshops, the project team prepared a vision statement along with goals and objectives. The statement and its goals and objectives are intended to serve as the guiding principles for Shelby County with respect to addressing the needs of bicycle and pedestrian travel in the community.

After completion of the analysis phase and the development of the preliminary recommendations, the project team hosted a Public Meeting. The purpose of the Public Meeting was to present the findings and conceptual improvements in the draft plan to the public and to solicit their input and comments. The Public Meeting was conducted in an “open house” format, allowing the public to view a series of “stations” illustrating the key elements of the plan. They also were offered the opportunity to have questions answered by either the project team or Pathway Committee members.

The final step in developing the Bicycle and Pedestrian Master Plan for Shelby County was to incorporate the findings from each preceding task into the final plan. The *Shelby County Bicycle and Pedestrian Master Plan* outlines goals, objectives, and recommendations to fulfill the County’s vision for a “safe transportation system that encourages the use of alternative modes of transportation to enrich the quality of life”.

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