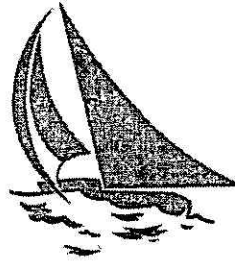


City of
Melbourne
The Harbor City



Site Assessment Report
Fire Station No. 72 Replacement

Prepared by:

Engineering Department

with support by Fire Department and Community Development Department

October 14, 2025

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Introduction & Scope

The City of Melbourne Engineering Department, with support from the Fire and Community Development departments, performed a preliminary site assessment on five (5) selected parcels for the potential replacement of the current, obsolete Fire Station No. 72, which is located at the southeast corner of Wickham and Sarno Roads within the confines of Jimmy Moore Park. Fire Station No. 72 (FS 72) was built in 1966. This station was originally built to support a much smaller community and to house only three (3) fire personnel. Present staffing includes five (5) fire personnel, one (1) fire engine, and a five-ton brush truck. Upon completion of the new station, it will house a minimum of eight (8) personnel and three (3) fire apparatus (one fire engine, one heavy rescue, and one brush truck). Future staffing models suggest this station should be sized to house 10 to 12 personnel and four (4) apparatus. The structure has been remodeled multiple times over several decades; however, the footprint has not changed. Current call volumes and projected development have proven the current site, and configuration, is no longer able to provide the level of service that is warranted.

The site assessment includes site mapping and identification, analysis of available environmental documentation and permitting requirements, fill planning, identification of site planning considerations, traffic impacts and response time analysis, identification of potential land acquisition costs and other related information pertinent to the development and construction of a replacement fire station.

Overview of Site Considerations

Exhibit A provides a quick glance at the location of five (5) parcels identified as potential options for the replacement of FS 72. Additional details for each site are further outlined in this document.

Minimum Site Standards

Acceptable locations for the replacement of Fire Station No. 72 must be able to accommodate the construction of a facility that meets the following standards:

- 2021 NFPA 1500 Standards for Fire Stations
- Designed to withstand 170-mph wind speed requirements
- Provide four (4) engine bays to accommodate current and future apparatus
- Sized to accommodate a 50-year expected lifespan, including future expansion
- Sized to accommodate staff privacy consistent with changes in professional staffing of the station.

Emergency Response Timing

As part of the siting process the replacement FS 27 must be located to ensure an average response time of less than six (6) minutes and ideally less than five (5) minutes to satisfy the City's commitment to meeting national standards. The current response time provided from Fire Station No. 72 is less than five (5) minutes.

While there is growth occurring west of Wickham Road and within the response area of FS 72, as depicted in Exhibit B, more than 70 percent of the calls for service from Fire Station No. 72 occur to locations east of the Wickham Road corridor. Additionally, the average apparatus response speed is approximately 30-35 mph or 44 feet-per-second. This allows City staff to estimate and compare response times between the existing and proposed locations. Impacts to response times based on proposed sites can be estimated using the current observed call response splits using Wickham Road as the center point of the response area.

The City of Melbourne is currently working to obtain Emergency Vehicle Preemption (EVP) capabilities for use in all City's fire apparatus. This system is primarily a software-based method for providing preferential traffic signal coordination for emergency response vehicles to reduce response times. Generally, EVP will reduce the overall transit times in an emergency response; however, the actual anticipated reductions in average response will vary based on the local roadway network and cannot be observed until implemented. The City of Melbourne Fire Department anticipates that this technology will help reduce response system-wide but the estimated reduction in time is still to be determined.

Future Development & Other Site Conditions

As part of this assessment, City Staff identified several factors which may need to be considered, to include future development, wetlands, brownfields, costs related to dirt/fill, and the like; however, the impacts of these factors cannot be quantified into specific impacts for each site at this time.

The City is aware of proposed development and entitled development projects within the Fire Station No. 72 response area. The new fire station is intended to represent a long-term investment in the community and is being sized and ideally located to accommodate future and proposed development projects. A substantial area of undeveloped land exists within the western portions of Fire Station No. 72's response area. As provided in Exhibit B, there are eight (8) entitled development projects which have begun or recently completed the land development process with the City of Melbourne. Upon receipt of the certificate of occupancy, these development projects will increase the number of calls that occur in the western response areas, which could

cause an overall shift to response time averages. Similarly, redevelopment within the response area, specifically redevelopment proposing higher intensity/higher density could similarly increase the number of calls and cause shifts in the average response times for Fire Station No. 72.

As part of staff's review of each site, the City has identified and outlined additional considerations related to the conditions of the site, to include whether the site has existing water and sewer, existing or mitigated wetlands, the existence or perceived existence of brownfields contamination, and other factors such as soils condition or gradation requiring the need for additional dirt/fill to stabilize the site for construction -- all of which can add additional costs and time to the development of a site for the purpose of a fire station. The details of each site are outlined in the pages to follow.

Site # 1

Site Location & Description

This subject parcel is located approximately 3,900-feet west of the existing City of Melbourne Fire Station No. 72 on the south side of Sarno Road. The subject parcel is identified as Parcel ID 27-36-24-00-509 (Tax Account 2747149). The parcel is approximately 3.0 acres in size and is located along Sarno Road adjacent to privately-owned parcels and within close proximity to the Brevard County landfill site and a privately owned recycling and landfill operation.

This parcel is owned by the City of Melbourne. The current Future Land Use (FLU) is Heavy Commercial and zoning is M-1 (Light Industrial). A fire station is permitted in the M-1 zoning.

Refer to Exhibit C – Site #1 for a detailed map.

Environmental Site Conditions

As depicted in Exhibit C, the subject parcel contains wetlands, which have already been mitigated. The Phase I Environmental Site Assessment (ESA) was commissioned by Daily Bread, Inc. for this site in October 2023 noted no presence of Recognized Environmental Conditions (contamination), and no additional testing or environmental evaluation is recommended. However, given the age of the Phase I ESA and its proximity to the County's former landfill site, it is recommended that an updated Phase I ESA be performed should the City consider proceeding with this site for the development of a fire station. Through the East Central Florida Regional Planning Council's (RPC) brownfields assessment grant, a Phase I ESA performed, saving the City approximately \$3,000-5,000. Should the Phase I ESA identify Recognized Environmental Conditions, a Phase II ESA, providing soil sampling and testing, will be warranted. The City could again utilize the RPC's grant to save up to \$20,000. If contaminants are present on site, remediation methodology and costs would then be determined but are generally quite costly and will significantly delay construction.

Initial geotechnical investigations were performed in 2023 as part of the site assessment conducted alongside the Phase I ESA. The geotechnical report, which can be furnished upon request, revealed that substantial soils replacement would be required to develop the site to support structures and roadways. Per the geotechnical report, as much as 23-feet of soil depth is recommended for treatment within the limits of the building foundations, including removal and replacement of at least two (2) feet of soil and five (5) feet of fill over that, or the use of deep foundations (piles). Additionally, any parking areas will likely require several feet of excavation with soil replacement, in addition to

approximately five (5) feet of fill, to support the proposed pavement installation. This is likely to add more than \$1 million to the development costs of this location.

Traffic & Response Time Considerations

This subject parcel is located on Sarno Road, approximately 3,900-feet west of the intersection at Wickham Road. Average response times from this location could be expected to increase, or deteriorate, response time by approximately 32 seconds.

Other Site-Specific Considerations

There is water and sewer in close proximity to the site. This parcel is currently vacant and allows for commercial, industrial, or institutional uses. Any development of this site by the City would remove this parcel from consideration for private commercial development and therefore, eliminate the opportunity for the City to realize any property tax revenue associated with the development of this site. Conversely, the subject site is currently owned by the City and exempt from property taxes, and the development of the site as a fire station would not change the existing impacts to future property tax revenues.

Site #2

Site Location & Description

This subject parcel is located approximately 1,400-feet west of the existing City of Melbourne Fire Station No. 72 on the north side of Sarno Road. The subject site is identified as Parcel ID 27-36-24-00-11 (Tax Account 2703576). The parcel is approximately 2.6 acres in size and is located along to Sarno Road, adjacent to the private driveway connection that services the American Muscle Car Museum and the Club-52 site.

This parcel is owned by the Otto S Boozer Revocable Trust and currently listed for sale with an asking price of \$1,080,000. There is no current appraisal nor Broker's Price Opinion available. The real estate broker has notified the City that an offer was made on the parcel. It is unknown whether the property owner has accepted the offer and entered the Due Diligence Period.

The current Future Land Use is Heavy Commercial and zoning is C-P (Commercial-Parkway). A fire station is permitted in the C-P zoning district.

Refer to Exhibit D – Site #2 for a detailed map.

Environmental Site Conditions

In accordance with the City's Real Property Acquisition Policy #310, the City shall perform adequate due diligence on the parcel prior to purchase, including obtaining an appraisal and commissioning a Phase I ESA to identify any potential sources of contamination that may be present. Through the RPC's brownfields assessment grant, the City requested a Phase I ESA be performed, saving the City approximately \$3,000-5,000. The Phase I ESA is currently underway and expected to be completed late October; however, preliminary reviews indicate that the findings are unremarkable. Should the Phase I ESA identify Recognized Environmental Conditions, a Phase II ESA, providing soil sampling and testing, will be warranted. The City could again utilize the RPC's grant to save up to \$20,000. If contaminants are present on site, remediation methodology and costs would then be determined but are generally quite costly and will significantly delay construction.

As depicted in Exhibit D, the subject site provides aerial photography that indicates potential wetlands and/or hydric (unsuitable) soils may be present at this site. In accordance with Policy #310, wetland delineation and geotechnical testing of the site prior to purchase shall be performed to determine presence of unsuitable soils and the extent of the soil replacement that would be required to develop the site. If wetland presence is confirmed, mitigation will be required prior to development of the site, which

will increase time and cost of construction and involve both permitting and application times, in addition to potential direct mitigation bank credit purchase costs of approximately \$75,000 - \$100,000 per acre of mitigated area.

Traffic & Response Time Considerations

This subject parcel is located on the north side of Sarno Road, approximately 1,400 feet west of the intersection at Wickham Road. Average response times from this location could be expected to increase, or deteriorate, by approximately 23 seconds.

This location moves the emergency vehicle egress driveway further away from the intersection at Wickham Road and Sarno Road, which ensures that intersection traffic movements do not interfere with the safe departure of a fire apparatus from the site.

Other Site-Specific Considerations

There is water and sewer in close proximity to the site; however, the sanitary sewer main would need to be extended to the site for the property to be developed. Further, there is no existing easement with the Walmart property to the east that would allow for a sewer main extension to occur, preventing cost-effective sanitary sewer connection.

This parcel is currently vacant and zoned for commercial use. Any acquisition and development of this site by the City for the purposes of a fire station would remove this parcel from consideration for private commercial development and therefore, eliminate the opportunity for the City to realize any property tax revenue associated with the development of this site.

Any development of the 53-acre site owned by Brevard County, located along Sarno Road immediately south of the subject site, could create potential response conflicts with the ingress/egress of a fire station at this location. Additionally, depending on the density or intensity of development, traffic signalization could be triggered at the intersection of Sarno Road and Porcella Avenue. If a fire station is developed on the subject site, staff recommends that the emergency vehicle egress driveway be aligned with the Porcella Avenue, which may constrain the building footprint.

Site #3

Site Location & Description

This subject site is the existing location of City of Melbourne Fire Station No. 72. The subject site is identified as Parcel ID 27-37-19-00-501 (Tax Account 2720641).

This parcel is owned/controlled by the City of Melbourne and is part of Jimmy Moore Park. The portion of land under consideration includes the existing location of FS 72 and falls in the northwest corner of the parcel immediately adjacent to Wickham Road and Sarno Road. The subject site backs up to a stormwater pond that falls within the Jimmy Moore Park Complex. While Jimmy Moore Park is 13.44 acres in size, the existing Fire Station No. 72 footprint comprises of approximately 1.16 acres on the northwest corner of the parcel. City staff proposed expanding the footprint of the current location of FS 72 to accommodate a replacement facility by approximately 0.6 acres, or approximately 25 percent of the pond area.

The current Future Land Use of the existing fire station footprint is Public/Institutional Land; however, the additional footprint/land required to construct a replacement fire station would require a Recreation Future Land Use designation. A FLU Map Amendment from Recreation to Public/Institutional Land would be required. The zoning is I-1 (Institutional), and a fire station is permitted.

Refer to Exhibit E – Site #3 for a detailed map.

Environmental Site Conditions

The subject parcel has been under municipal control for more than 60 years. The current Fire Station No. 72 was originally constructed in 1966. As such a Phase 1 ESA is not necessary. Wetlands were not found within the current and anticipated development footprint. Additionally, unsuitable soils are not anticipated under the 'dry' portions of the existing site.

Following City staff review of the existing FS 72 site, an expanded footprint will be required to meet today's National Fire Protection Association (NFPA) standards. Due to the proximity of Wickham Road and Sarno Road, the expanded footprint would require modification (fill) within the existing pond adjacent to the fire station as well as the removal/mitigation of unsuitable soils within the proposed expanded footprint.

Engineering staff have reviewed the subject site and the expanded footprint area and found that roadway drainage from both Wickham Road and Sarno Road is routed into the existing pond at Jimmy Moore Park. Additionally, the pond is connected via a bi-directional flow culvert to Brevard County's bulk water conveyance swale at the southwest corner of the pond. Staff have not been able to identify if these connections were part of the original construction permits for either roadway. The roadway drainage

connections along with the interconnectivity to the County's bulk water conveyance system is likely to make any proposed permitting for the filling of the pond a time intensive and costly process along with carrying the risk of additional mitigation costs associated with permitting.

Traffic & Response Time Considerations

This subject site includes the current location of the existing Fire Station No. 72. Response times are expected to remain the same.

The replacement of Fire Station No. 72 in the existing location would place emergency-egress driveways extremely close to the intersection of Wickham Road and Sarno Road. While this will roughly match the current driveway configuration, it does not account for future traffic volume increases that are anticipated to occur with continued population growth. If the fire station remains in the existing location, concerns remain related to the safe deployment of a fire apparatus given the proximity of the emergency egress at the intersection.

Other Site-Specific Considerations

There is currently water and sewer serving the site. To meet today's NFPA standards for fire stations, the site footprint would need to be expanded. Any reduction in the stormwater pond to accommodate an increase footprint for the fire station would require permits from St. Johns River Water Management District. Additional stormwater treatment may be required or the potential use of alternative forms of treatment which may drive overall project costs higher. Any reduction of the pond footprint, even if permitted, could be perceived as undesirable by the public who frequent Jimmy Moore Park.

The current Future Land Use of the fire station site is Public/Institutional Land; however, the additional land required to construct a new/replacement fire station would require a Recreation Future Land Use designation. A FLU Map Amendment from Recreation to Public/Institutional Land would be required. The zoning is I-1 (Institutional), and a fire station is permitted.

As this parcel is currently owned by the City and exempt from property taxes, the reconfiguration and redevelopment of the site would not have adverse impacts to future property tax revenues.

Site #4

Site Location & Description

The subject site is comprised of two (2) parcels located immediately north and east of the existing City of Melbourne Fire Station No. 72. The subject site is identified as a combination of two (2) parcels 27-37-19-00-258 and 27-37-19-00-258.1 (Tax Accounts 2720403 & 2720404 respectively). The two (2) parcels are 1.81 acres and 2.6 acres in size and are adjacent to each other.

These parcels are owned by Don Bell Incorporated and are not currently available for purchase. City staff understand that the owner is currently in conversation with a private developer regarding redevelopment of the site.

The current Future Land Use is General Commercial, allowing for institutional use, such as a fire station. The zoning is C-2 (General Commercial), and a fire station is permitted use.

Refer to Exhibit F – Site #4 for a detailed map.

Environmental Site Conditions

This subject parcel is currently under private ownership and has historically been used for construction and demolition services and debris/trash hauling businesses. As depicted in Exhibit F, a review of the aerial photography, and known uses of the site, staff can confirm the site is used for outdoor storage and staging of equipment, vehicles and containers. As confirmed by PPM Consultants, Inc., the RPC's environmental consultant under the brownfields assessment grant, this site is expected to be a brownfields site as the current and historical uses have likely resulted in potential contamination of soils.

In accordance with the City's Real Property Acquisition Policy #310, the City shall perform adequate due diligence prior to purchase of this parcel, to include an appraisal and a Phase I ESA to identify any potential sources of contamination that may be present. Through the RPC's brownfields assessment grant, the City could have a Phase I ESA performed, saving approximately \$3,000-5,000. If the Phase I ESA identifies Recognized Environmental Conditions, a Phase II ESA, providing soil sampling and testing, will be warranted. The City could again utilize the RPC's grant to save up to \$20,000. If contaminants are present on site, remediation methodology and costs would then be determined but are generally quite costly and will significantly delay construction.

Traffic & Response Time Considerations

This subject parcel is located on the north side of Sarno Road approximately 800-feet east of the intersection at Wickham Road. Average station response times could be expected to decrease, or improve, response times by approximately 7 seconds.

This location moves the emergency vehicle egress driveway further away from the intersection at Wickham Road and Sarno Road, which ensures that intersection traffic movements do not interfere with the safe departure of a fire apparatus from the site.

Other Site-Specific Considerations

This subject parcel is currently not available for public purchase, which constrains the ability for this site to be considered for development. Any acquisition of this site for the City's use will require a Phase I ESA, which is highly likely to result in a report of perceived contamination, requiring a Phase II ESA. Real contamination identified on site will require remediation, resulting in increased costs, time, and potential future liability for fire personnel who effectively reside on site during their shift. There is currently water serving this site; however, sanitary sewer would need to be brought to the site in order to be developed.

This parcel is currently zoned for commercial uses. Any acquisition and redevelopment of this site by the City for the purposes of a fire station would remove this parcel from consideration for private commercial redevelopment and therefore, eliminate the opportunity for the City to realize any property tax revenue associated with the redevelopment of this site.

Site #5

Site Location & Description

This subject site is located on the northeast corner of Jimmy Moore Park, immediately east of the existing City of Melbourne Fire Station No. 72. The subject site is identified as parcel ID 27-37-19-00-501 (Tax Account 2720641).

This parcel is owned/controlled by the City of Melbourne and is located at Jimmy Moore Park. The portion of land under consideration falls in the northwest corner of the parcel immediately adjacent to Sarno Road and backs up to a drainage canal abutting the perimeter of the Park. The total site is 13.44 acres. The proposed development area for the replacement of Fire Station No. 72 is expected to be approximately 1.75 to 2 acres, depending on the final site configuration.

The current Future Land Use is Recreation and zoning is I-1 (Institutional). A FLU Map Amendment from Recreation to Public/Institutional Land would be required. Under the current zoning, institutional uses such as a fire station are permitted by right.

Refer to Exhibit G – Site #5 for a detailed map.

Environmental Site Conditions

This subject parcel has been under municipal control for more than 60 years and has not been used in a manner which would have potentially contaminated the site. As such a Phase I ESA is not necessary. Additionally, wetlands have not been identified within the proposed development footprint and unsuitable soils are not anticipated on the site.

Traffic & Response Time Considerations

This subject parcel is located on the south side of Sarno Road approximately 1,100-feet east of the intersection at Wickham Road. Average station response times could be expected to decrease, or improve, response time by 10 seconds.

This location moves the emergency vehicle egress driveway further away from the intersection at Wickham Road and Sarno Road, which ensures that intersection traffic movements do not interfere with the safe departure of a fire apparatus from the site.

Other Site-Specific Considerations

At the July 8, 2025 Regular Council Meeting, as part of a Future Land Use Map Amendment, City staff proposed this subject site as the future location for the replacement of Fire Station No. 72. Following public input from adjacent residents regarding the relocation, building size and height, and perceived loss of green space within Jimmy Moore Park, Council denied the FLU Map Amendment rendering the proposed project site moot.

Staff is proposing to scale back the conceptual site of the fire station to ensure the site accommodates today's NFPA standards while also preserving as much recreational space at the Park.

Development of the subject site would result in the demolition of the existing FS 72 providing a location for additional paved parking, which would replace the current unlawful, uncontrolled, and unsafe shoulder access car parking that currently occurs within the right-of-way along Sarno Road. The improvements would also allow for reconfiguration of the overall park complex, resulting in a stormwater system that serves as a single system encompassing all improvements. There is currently water and sewer serving the site.

As this parcel is currently owned by the City and exempt from property taxes, the reconfiguration and redevelopment of the site would not have adverse impacts to future property tax revenues.

Summary & Recommendations

Summary

Table 2 below provides a visual summary of the information collected for all five (5) sites identified to serve as potential locations for the replacement of Fire Station No. 72. The summary of sites is listed in order of the best to least average response times. The categories of assessment are:

- **Average Response Time Change:** The estimated impacts to average station response time based upon the observed response locations for recent station emergency call outs.
- **Cost to Develop:** The estimated costs to develop the site based upon anticipated site conditions, land acquisition, design, permitting and construction costs. This only addresses the site construction costs and does not include cost associated with the construction of the fire station as such would be similar across each of the locations.
- **Permitting:** This identifies the anticipated difficulty associated with permitting the site based on site conditions or configuration. The level of difficulty permitting historically results in longer timeframes to receive approval for construction and increased the risk of higher design and construction costs.
- **Tax Impacts:** This category identifies whether the site has any anticipated adverse impacts to property tax revenue based on change in the ownership, zoning, and development of the site as a fire station.
- **Site Availability:** This identifies if the subject site is currently owned by the City of Melbourne, or privately-owned and its availability for purchase.

Table 2:

Site #	Avg. Response Time Change	Cost to Develop	Permitting	Tax Impacts	Site Availability
5	-10 seconds	Standard	Low	N	City-owned
4	-7 seconds	Moderate to High	Medium to High	Y	Not currently for sale
3	No Change	Very High	High	N	City-owned
2	+23 seconds	Moderate to High	Medium	Y	Available for purchase
1	+32 seconds	Moderate to High	Low	Y	City-owned

Recommendations

Based upon staff's review of the five (5) subject sites, three (3) sites have been identified as suitable for further exploration for the replacement of Future Fire Station No. 72. In order, these are subject sites #5, 1, and 2.

Site #3 is not recommended due to the potential difficulty of permitting, time delays, and costs associated with reconfiguration and reduction of the existing stormwater pond at Jimmy Moore Park. Site #4 is not recommended as the site is not currently listed for sale and due to perceived contamination and associated risk, liability, and costs.

Exhibit A - Potential Sites for Fire Station No. 72 Replacement



#GIS

GIS Portal: <https://maps.mtlib.org/arcgis>
 ArcGIS Online: <https://mgis.maps.arcgis.com>

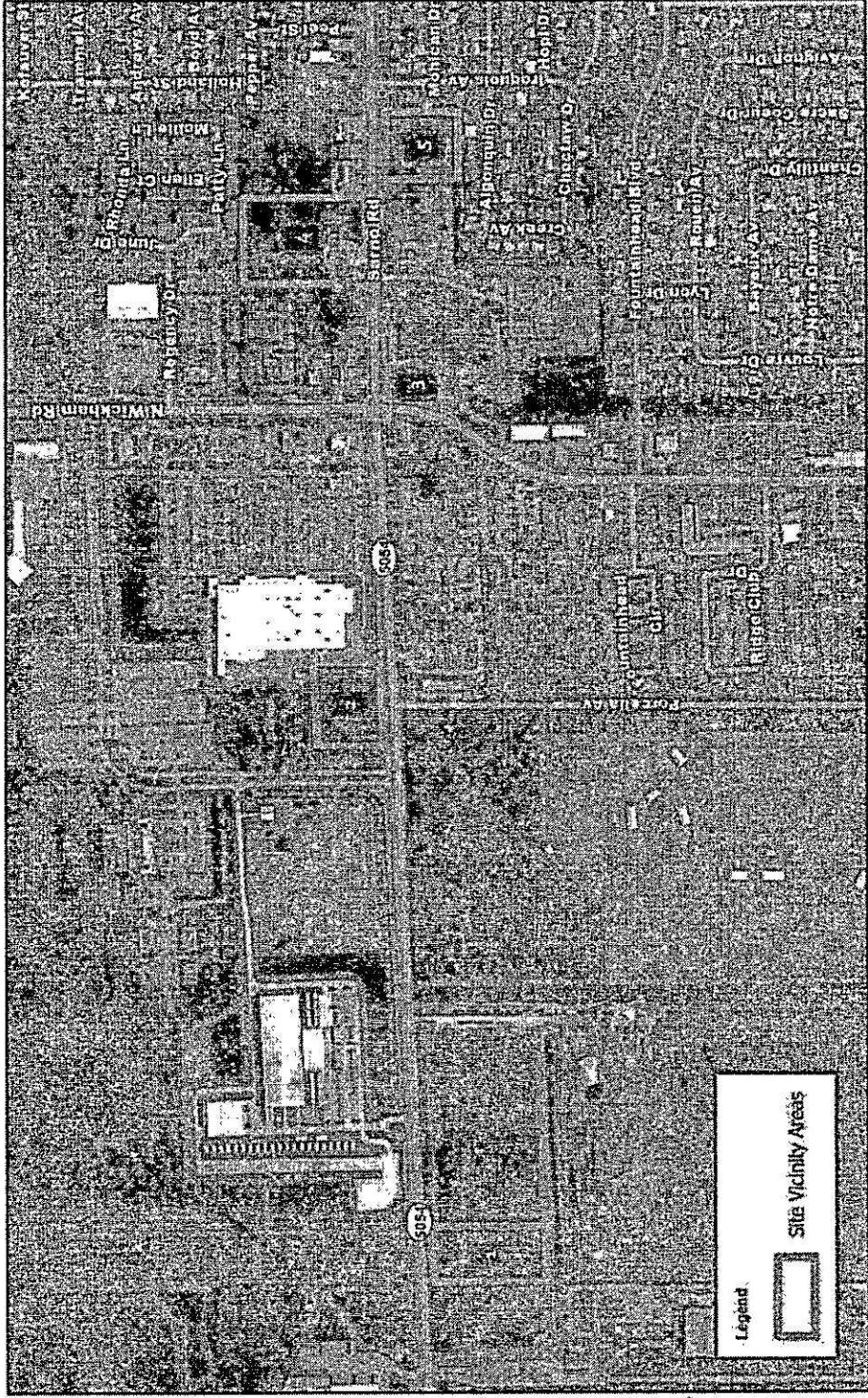


Exhibit B - Development Projects

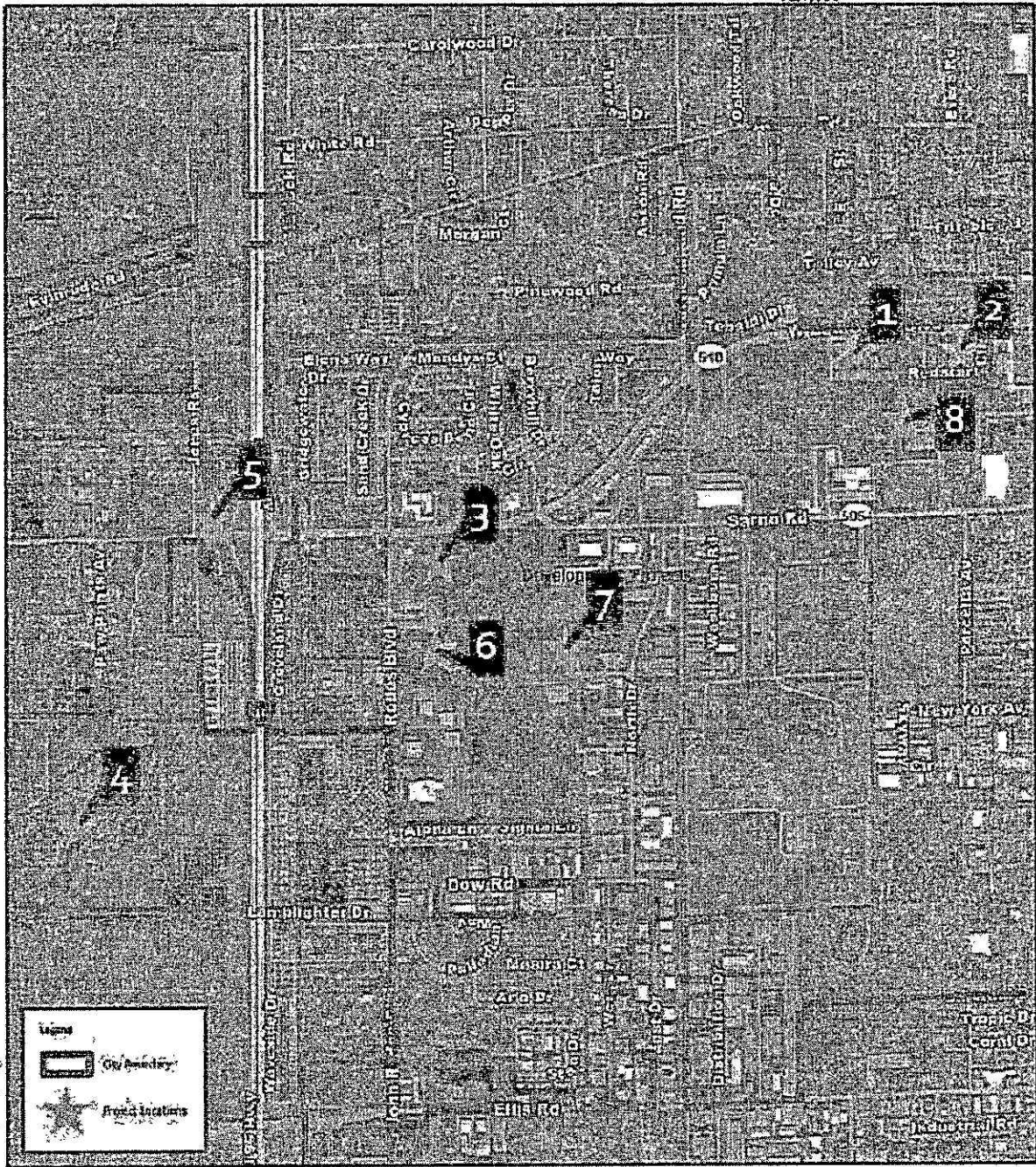
Development Projects
(see Projects Legend attached separately)



GIS Portal: <https://maps.mblf.org/arcgis>
ArcGIS Online: <https://mgis.maps.arcgis.com>

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Map legend on the next page.

EXHIBIT B (continued) - MAP LEGEND

PROJECT #	PROJECT NAME	LOCATION	NUMBER OF UNITS or Gross SF	Timing/ Status
1	The Preserve at Melbourne Eau Gallie Boulevard Apartments	1280 Dunlin Drive 3205 West Eau Gallie Boulevard	240 units Multi-Family Residential 240 units Multi-Family Residential	Built under construction
3	Eau Gallie Town Centre	4429 West Eau Gallie Boulevard/ 801 North John Rodes Boulevard	600 units Multi-Family Residential	under review
4	Eagle Preserve	4905 West Eau Gallie Boulevard	577 Single-Family Lots 800± units future Multi-Family Residential	under review
5	Westside Commons	NW corner of I-95 and West Eau Gallie Boulevard	250 units Multi-Family Residential plus Restaurants & Hotel	under review
6	Harbour Logistics	705 North John Rodes Boulevard	154,000 SF 376,000 SF Distribution/Warehousing	site plan approved
7	Digital Light HVAC & Boat/RV Storage	4050 Digital Light Drive	65,555 SF HVAC building 147-storage bays	construction plan review
8	Club 52	1100 North Wickham Road	37,000 SF parimutuel facility and restaurant	under review

Exhibit C - Site #1 Location & Wetlands Map

Site #1
 Parcel ID 27-36-24-00-509
 Tax Account 2747149



GIS Portal: <https://maps.mlbfl.org/arcgis>
 ArcGIS Online: <https://mgis.maps.arcgis.com>

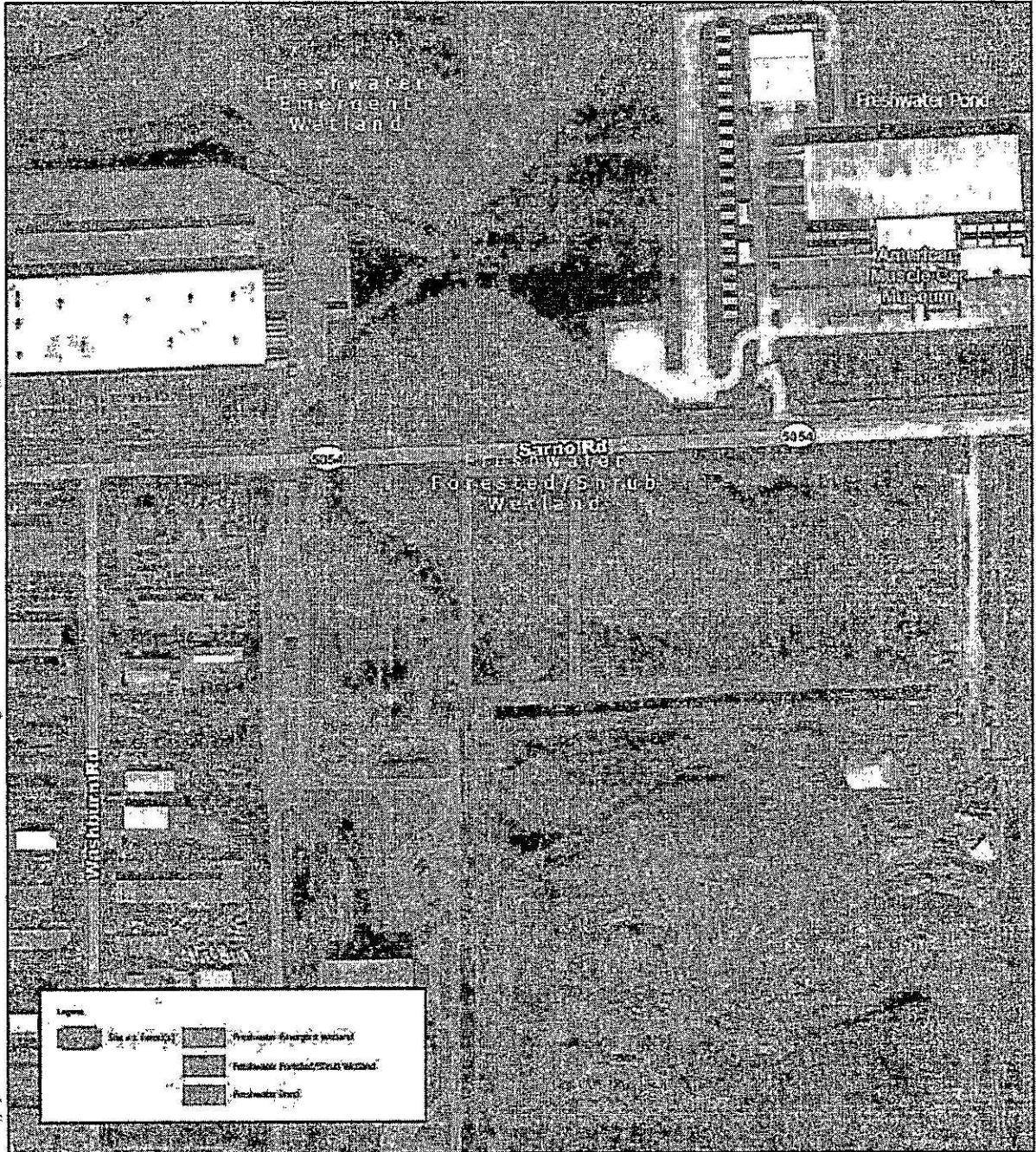
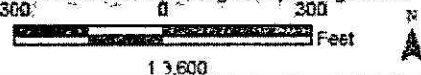


EXHIBIT C - Site #1 Location & Wetlands Map. For information, accuracy or location was provided for the general records and mapping data based on the best and to the best of the City of Melbourne's knowledge. The City of Melbourne assumes no liability for any omissions, errors, or expenses, including but not limited to attorney fees, arising from any user's use or misuse of the property records or mapping data provided herein.

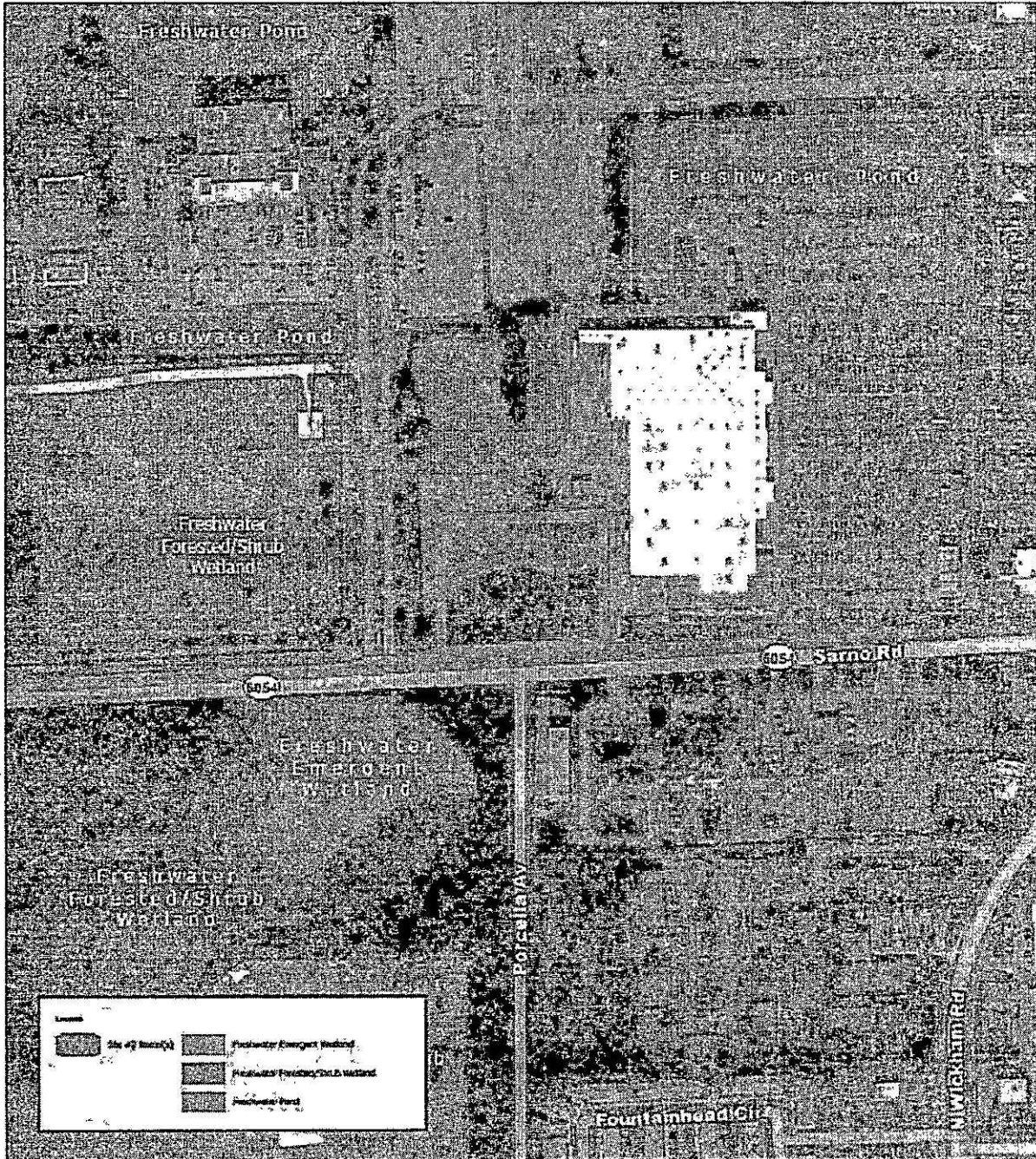
Exhibit D - Site #2 Location & Wetlands Map

Site #2
 Parcel ID 27-38-24-00-11
 Tax Account 2703576



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 ArcGIS Online: <https://mgis.maps.arcgis.com>

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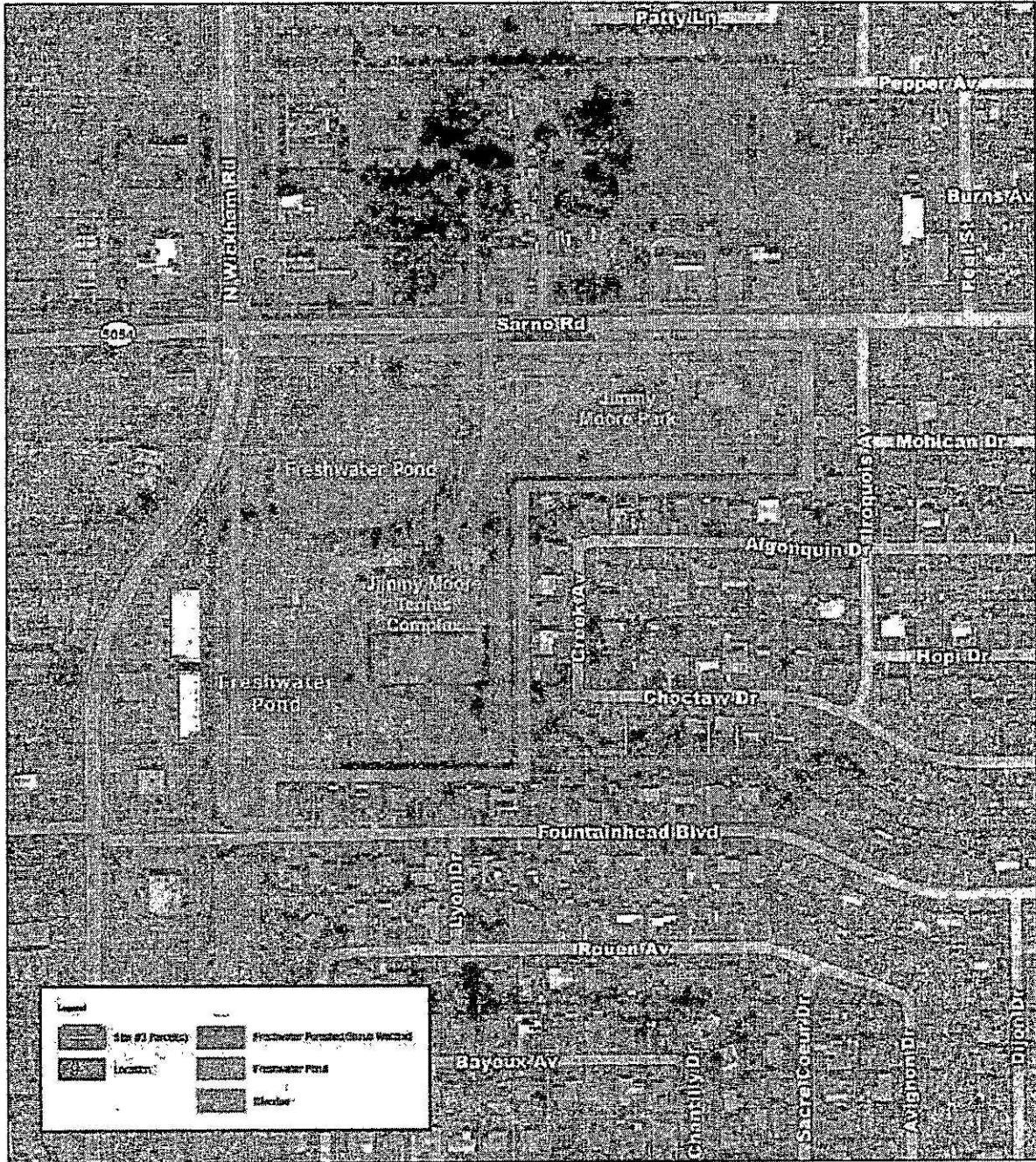
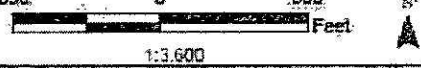
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Exhibit E - Site #3 Location & Wetlands Map

Site #3
 Parcel ID 27-37-19-00-501
 Tax Account 2720641



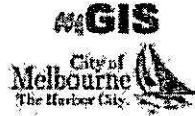
GIS Portal: <https://maps.mibfl.org/arcgis>
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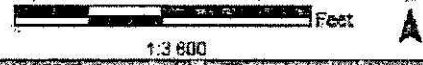
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Exhibit F - Site #4 Location & Wetlands Map

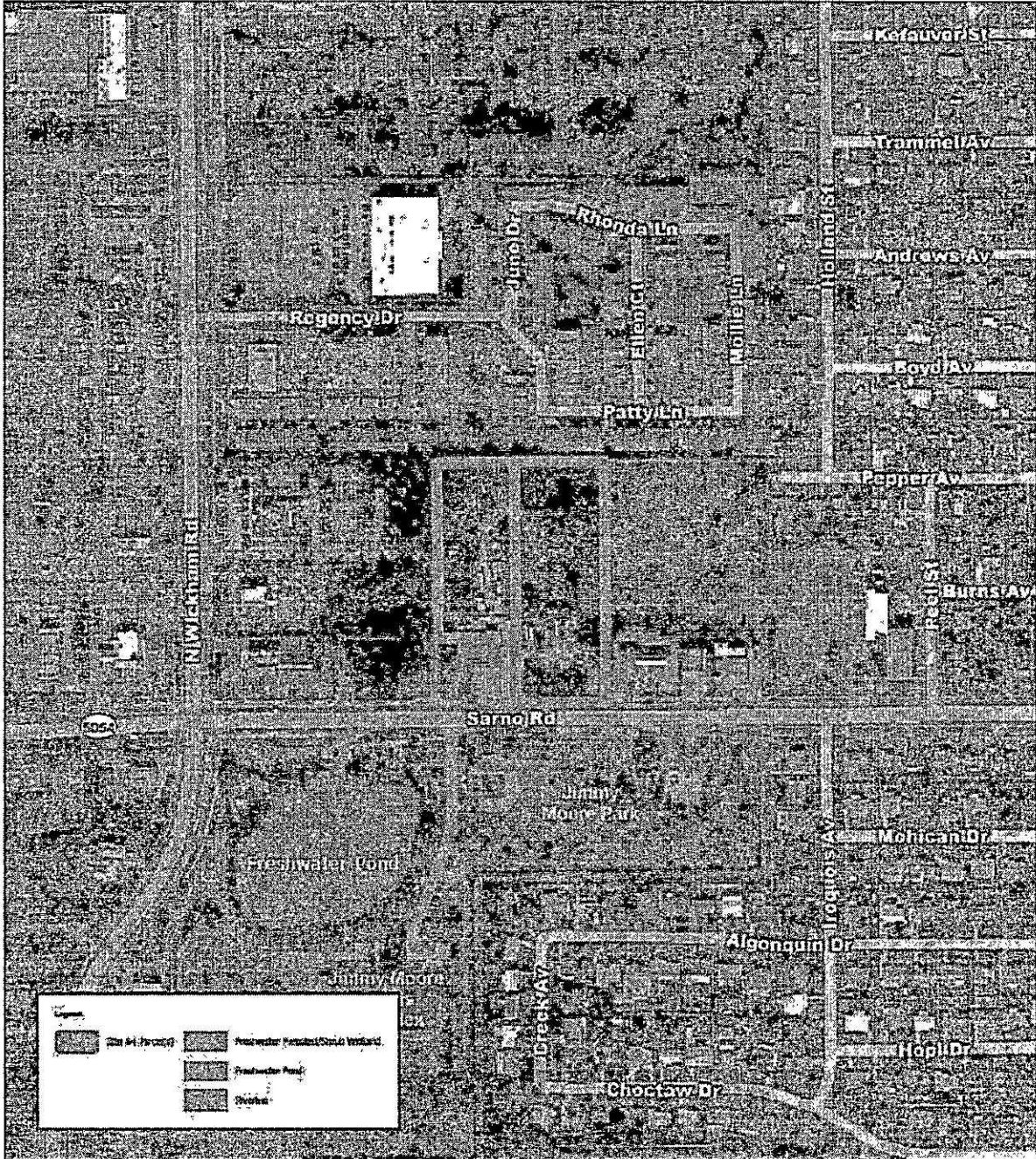
Site #4
 Parcel IDs 27-37-19-00-258
 & 27-37-19-00-258.1
 Tax Accounts 2720403 & 2720404



GIS Portal: <https://maps.mibf.org/arcgis>
 ArcGIS Online: <https://mgis.maps.arcgis.com>



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Legend	
	Site #4 Parcel(s)
	Freshwater Protected/Stream Wetland
	Freshwater Pond
	Other

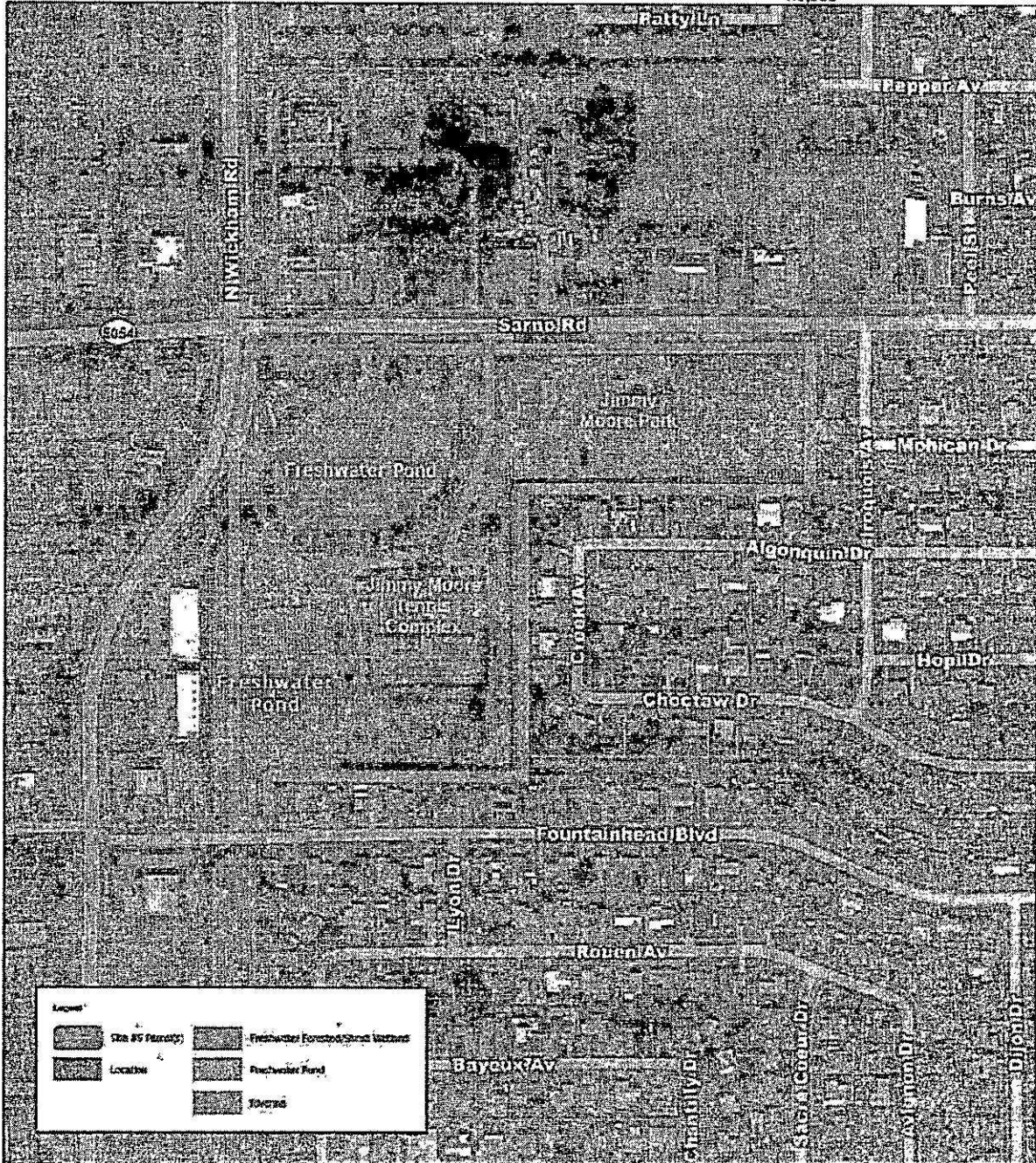
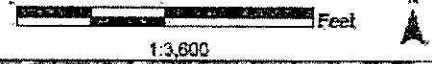
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Exhibit G - Site #5 Location & Wetlands Map

Site #5
 Parcel ID 27-37-19-00-501
 Tax Account 2720641



GIS Portal: <https://maps.mlb7.org/arcgis>
 ArcGIS Online: <https://mgis.maps.arcgis.com>



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