

SECTION – 8

SPORTMOD DIVISION - 2021

Open to two-wheel drive American automobiles provided they comply with, and adhere to, specifications as outlined for this division.

NOTICE

ALL EQUIPMENT IS SUBJECT TO THE APPROVAL OF BEAVER DAM OFFICIALS. NO EQUIPMENT WILL BE CONSIDERED AS HAVING BEEN APPROVED BY REASON OF HAVING PASSED THRU INSPECTION UNNOTICED. EFFORTS TO TAKE ADVANTAGE OF “LOOP HOLES” IN THESE RULES WILL NOT BE TOLERATED. ALL RACE CARS ARE SUBJECT TO INSPECTION BY TRACK OFFICIALS AT ANYTIME.

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events and by participating in these events, all participants are deemed to have complied with these rules. No expressed or implied warranty of safety shall result from publications of, or compliance with, these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official. The race director shall be empowered to permit reasonable and appropriate deviation from any of the specifications herein or impose any further restrictions that in his opinion do not alter the minimum acceptable requirements. No expressed or implied warranty of safety shall result from such alteration of specifications. Any interpretation or deviation of these rules is left to the discretion of the officials. Their decision is final.

Numbers are required on the door or quarter panel and must be at least 20" tall and 4" wide. A number is required on the roof, readable from the right side, and must be at least 30" tall. Numbers must also be on the front and back of car for ease of lineups and be at least 4" tall. **NO METALLIC OR HOLOGRAPHIC NUMBERS WILL BE ALLOWED.** No roman numerals allowed. Numbers need to be of contrasting color to the main color scheme. If there are double numbers a letter will be assigned to you by the track to go with your number. If a letter is required with the car number, the letter must be at least fifty percent (50%) of the height of the number. **IT IS THE RESPONSIBILITY OF THE DRIVER TO ENSURE THAT THE SCORERS CAN READ THE CAR NUMBER. IF YOUR CAR NUMBER IS NOT READABLE FROM THE SCORING TOWER, THE CAR WILL NOT BE SCORED UNTIL THE NUMBER IS CORRECTED.**

Beaver Dam reserves the right to assign car numbers, and to assign or restrict the display of graphics and advertising on race cars. Offensive graphics or slogans are not permitted. All Competitors agree to accept Beaver Dam's decision in this matter.

Where required, participating sponsor's emblems, or decals will be placed in the position designated by Beaver Dam Officials. Cars that do not display all required sponsors emblems, or decals, will receive less prize money.

All cars in competition are required to have a working Westhold transponder (silver side facing the track). The front of the transponder must be placed at the rear of the engine mounting midpalate. The transponder pouch should be placed horizontally, with a clear view to the ground. If the transponder pouch is mounted to round tube frame components with nylon ties, the transponder pouch should be secured so that it cannot spin from its horizontal position. Failure to comply with transponder requirements will result in a penalty. Penalties will be imposed in accordance with **SECTION 4 - PENALTIES**. No on/off switches permitted on transponder power supply.

8.1 SAFETY EQUIPMENT

A full face, helmet and face shield Snell-rated SA2010, SA2015, SA2020 or SFI 41.1/or newer and more highly rated for auto racing helmet required. Helmet and face shield must be worn at all times car is on the track and must accompany vehicle at time of inspection. Roll bar padding required in driver compartment (Fire retardant recommended). SFI-approved full fire suit, fire retardant gloves, and shoes required. Neck brace optional. Drivers not wearing a neck collar will be required to wear a fire-retardant head sock. Right and left seat head supports required if using head restraint system or neck collar. Recommended: Fire retardant head sock and underwear (Mandatory for driver under the age of 18); head and neck restraints; collapsible steering shaft. Driver-side window net required, minimum 16-inch by 20-inch ribbon or mesh style and must be mounted to roll cage so latch is at top front of window. The window net must be up and latched at all times the car is on the track. Window net must be positioned in a manner to protect debris from entering the cockpit area. Maximum 4" high, visor attached to window net. Minimum three-inch (two-inch with head restraint system) wide SFI-approved five-point safety belt assembly, or Beaver Dam approved safety restraint system, required (Y-type shoulder harness not allowed), must be mounted securely to roll cage, and be date stamped 2019, or 2020, or 2021. Head and neck restraint system recommended. Shoulder restraints must mount to main roll cage, not permitted on rear frame. Kill switch required within easy reach of driver and must be clearly marked 'OFF' and 'ON'.

A Raceceiver model SW1600 or equivalent, tuned to channel 250 (453.1125) is mandatory. Raceceiver unit must lock on one channel only, no scanner units. Failure to comply with Raceceiver requirements will result in a penalty. Penalties will be imposed in accordance with **SECTION 4 – PENALTIES**.

8.2 FRAME

OEM complete, 1964 or newer perimeter, American made rear wheel drive passenger frame only. No sports car frames. No Jeep, Bronco etc. or four-wheel drive frames. No front wheel drive frames. Frames must be full and complete, cannot be widened or narrowed, and must be able to support roll cage on both sides. Exceptions are: weight jack in original center line of spring tower allowed; frame may be cut to a maximum of 36-inches forward from center of rear end housing; horns may be removed in front of steering box and notched a maximum of one inch at bottom for tie rod clearance; cross member may be notched and boxed radiator and or steering clearance; maximum 7-inch wide opening in side of spring tower for spring removal. Maximum 2-inch wide x 4-inch frame stiffener may be welded to the outside of the left frame rail. Left top frame rail may be removed inside driver's cockpit area. Minimum wheel base 108-inches, maximum 112-inches. Maximum overall width (front or rear) shall not exceed 78-inches from outside of widest point of tire to outside widest point of tire. For car utilizing stock rear suspension design, no further forward than 1-inch behind factory seam frame may be replaced with 2-inch x 3-inch rectangular steel tubing x 0.095-inch minimum wall thickness. No part of frame or body may be lower than 4-inches from ground except for cross member and lower rear frame underslung.

8.3 ROLL CAGE

Roll cage must consist of continuous hoop design, minimum 1.75-inch O.D. tubing with a minimum wall thickness 0.095-inch for main cage. Cage must be frame mounted in at least 6 locations. Low carbon or mild steel recommended. Cage must be electric welded. No brazing or soldering allowed. Roll cage must consist of a configuration of front rear and top hoops, which are connected by tubing on sides, or side hoops. Driver's head must not protrude outside cage with helmet on in normal racing position. Top halo must be divided with diagonal or lateral cross bar. Foot protection bar is required. Main cage may not go further forward than rear of engine block. All bars forward of the cage must be lower than the hood.

8.4 DOOR BARS

All driver door bars and uprights must be a minimum of 1.50-inch O.D. and 0.083-inch wall thickness. Minimum of three driver's side door bars parallel to the ground and perpendicular to the driver. Passenger side door bars required at least one horizontal or angled minimum 1.25-inch O.D. x 0.083-inch wall thickness and one top horizontal door bar 1.50-inch O.D. x 0.083-inch thickness. Steel doorplate required on driver's side. 18 gauge or 0.049-inch minimum thickness welded to outside of the door bars. Must cover the area from the top door bar to the bottom door bar and from the rear hoop down post to minimum 5-inches in front of leading edge of driver's seat. Doorplate must be visible for inspection.

8.5 BODY

See diagram. No composite or plastic body panels allowed except roof rock guard, hood scoop and nose piece. Body and interior deck must be the same width front to rear and parallel to the OEM frame. Aluminum nose panel must be flat with fins not to exceed 2.25" high, Plastic nosepiece MD3 PART #020-410 allowed. Cooling holes allowed. Nose panel may be no lower than two inches outside of front frame horns. Nose panel must remain within confines of front bumper and be mounted in a manner approved by Raceway officials. Plastic valance permitted. May not extend more than 4" below the bottom of the frame horns. Must be same width front to rear. May not be higher than front, top of hood. Engine compartment must remain open (no side

panels). Hood must be level or sloped down at front and be enclosed at rear. No reverse hood rake allowed. Hood must be no more than 2" above interior deck at rear. Hood must cover radiator. No panels in front of right door to engine compartment. No inner panels. No car covers at any time, except for rear tail cover while car is in personal pit area. Must have front windshield and rear window support posts. Driver and passenger windows must have 12-inch minimum opening width and height measured at the center of the window, measured between the lowest point at the top of the window whether roll cage or roof and highest point of bottom of window either roll cage, door panel or interior. May use polycarbonate full windshield. Roof must be fiberglass, steel or aluminum full size rounded down in all directions. No dished roofs allowed. Driver roof hatch allowed. Maximum 1.5-inch rolled down rock guard allowed on roof front. Maximum four-inch roof sides allowed. Maximum one-inch ridge down sides of roof. Maximum one-inch rear roof stiffener (must face down). Roof must be mounted within 1/2" of roll cage. Window side (sail) panels must resemble the diagram configuration in all aspects and may not extend ahead of back of seat. Solid sail panels only. Single plane one-piece rear spoiler optional, may be minimum one inch to 5-inches maximum in material height and maximum 66-inch in width. No additional fins, lips, wicker bills, or air controlling devices permitted. Spoiler may have one stiffener that must be 1-inch, or more, down from top edge. Maximum 3 spoiler braces allowed, forward facing, perpendicular to spoiler. Spoiler braces must match diagram in all aspects. Outside of tires must be widest part of car, viewed from front, rear and above. Body may not extend past front of rear bumper. No reflective body panels. **All body diagram dimensions must be met, no tolerance.**

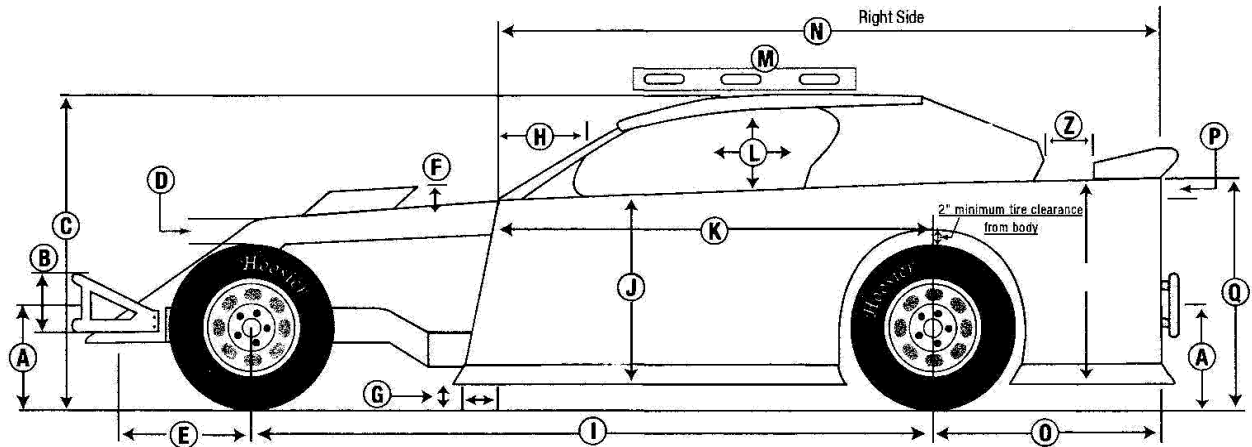
Sport Mod Body Dimensions

- | | |
|--|--|
| A 20" max. 16" min. (ground to center of bumpers, front and rear) | O 48" max. 34" min. |
| B --- 6.5" min (center to center) | P Interior slope is 6" max. front to rear and flat across. |
| C 54" max. roll cage, 56" max roof 42" min. | Q 38" max. 28" min. |
| D 6" max. Hood drop (sides), sealed off from driver's compartment and max. 6" rake. | R. Min. 24" nose must be flat and flush with sides. |
| E 36" max. | S 66" max. 24" min. must cover radiator. |
| F 6" max 4" min. Door may extend maximum 6" Past block at bottom on both sides. | T 52" max. 44" min. |
| H 19" max. must be same on both sides | U 56" max. 47" min. |
| I 112" max. 108" min. | V All tires must be outside body and nerf bar except |
| J 31" max. 27" min. | V1 rear tires max. 1" inside body- both sides |
| K 72" max. or not past back of block, right side. side may extend forward to cover foot box. | W 66" max. 53" min. Same front to back. |
| L 18" max. 12" min. opening, both sides. | X 43" max. 24" min. |
| M With level, must have no more than 2" clearance at rear of roof, and 5" at top front. | Z 3" min. |
| N 120" max. 106" min. | |

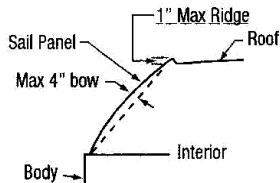
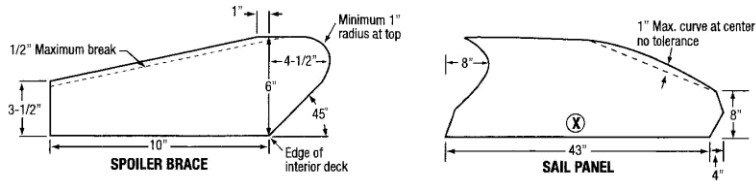
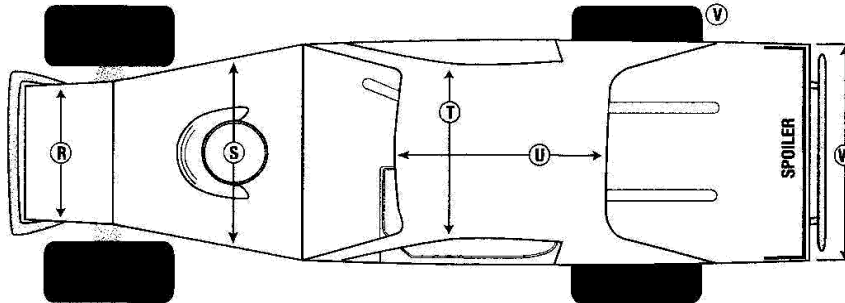
REAR FILLER PANEL: 8" min./90-degree angle. Must be solid, attached to deck, extended to quarter panels

SPOILER BRACES: Maximum 3 total. Bottom edge of spoiler brace must be flush with rear edge of interior deck

SAIL PANELS: Must slope down from rear edge of roof and be within confines of these dimensions and same on both sides. Must mount within to outside of and be no wider than interior deck 2" of outside edge off interior deck. If flat at front half of interior, you have used up 50% of your 6" so from behind driver to rear you may only have 3". Top of interior must be flush with top of door and quarter panels. Optional escape hatch from driver to passenger door allowed.



All body measurements are max. unless otherwise specified. Dimensions have zero tolerance.



8.6 DRIVER COMPARTMENT

Must have minimum 3 windshield bars in front of driver. Polycarbonate or aluminum cowl in front of driver, no wider than cockpit, no further back than steering wheel. Minimum 0.125-inch thick aluminum or 0.060-inch steel, complete floor pan required. Aluminum high back purpose-built aluminum racing seat required. Use of a full containment style seat highly recommended. Must be bolted in using minimum of four 3/8-inch diameter bolts. A flat steel washer no less than 1½-inches in diameter must be installed between the head of the bolt and seat. Must mount inside left frame rail and ahead of rear tires. Bottom of seat may not be lower than bottom of frame rails. Drivers compartment must be sealed off from track, driveline, fuel cell, canisters and pumps. Oil coolers may not be above interior. Accumulators or accusumps are not permitted in the cockpit. No mirrors or reflective devices. No cockpit adjustable devices of any kind permitted other than brake adjusters.

8.7 FRONT SUSPENSION

All components must be unaltered OEM steel in OEM mounting locations and be replaceable by OEM parts. Exceptions are: tube style upper A-frame with steel or aluminum cross shaft. Upper A-frame mounts may be moved. Only OEM or OEM stamped steel replacement lower A-frames may be used. Nylon, rubber, polyurethane or steel lower A-frame bushings permitted. No offset or bearing style bushings permitted. Shock mounts may be welded or bolted to lower A-frame. Lower A-frame must be right and left of same design and dimensions. Lower A-frame mounts must be in OEM location. OEM or OEM replacement, all steel, upper and lower ball joints only. No screw in style lower ball joints. Sway bar, if utilized, must be unaltered OEM. No rubber bump stops, suspension limiting chains, cables, or tethers, or suspension stops allowed.

8.8 STEERING

No rack and pinion steering gear permitted. All components must be OEM, steel, unaltered, in OEM locations. Exceptions are outer tie rod end and adjustment sleeve may be replaced by minimum 0.625-inch steel heim end and steel tube. Spindle may be ground for brake caliper clearance only. OEM or OEM replacement Pinto spindles permitted and aftermarket spindle speedway motors part #90134501-L and 90134501-R with original cast markings visible. Multi-piece IMCA approved Speedway Motors spindle allowed. Spindle must of the same design, left and right. Bolt on spindle savers permitted. Steel steering shafts and knuckles only. Must have minimum two knuckles or have collapsible shaft design. Steering wheel, quickener, and steering wheel disconnect may be aluminum. The center of the steering wheel must be padded. Cockpit steering location may be altered but must be kept on left side. Idler arm, pitman arm and center link must match frame. No sway bar of any type permitted.

8.9 SHOCKS

One steel nonadjustable unaltered shock per wheel. Shock must be used to control wheel. Welded mount shocks only. No shocks allowed on screw jacks. No coil over, air, bulb style, threaded body, through rod or remote reservoir shocks allowed. No Schrader or bladder type valves allowed. Shocks must completely collapse upon inspection at any time. Maximum diameter 2 1/8" O.D. Shock body, Front half of shocks may be shielded. Maximum 7" stroke on any front shock and maximum 9" stroke on any rear shock. Shock may not be used to pin or preload any spring. One or all shocks may be claimed for \$50 each. Beaver Dam reserves the right to claim any shock for \$50. Claim must follow claim procedure in **Section 3.9**.

8.10 SPRINGS

One closed end steel coil or multi leaf (rear) spring per wheel. Minimum 4.5-inches O.D. Must be non-progressive style. Front coil spring free height must be 9 1/2", 1/2" tolerance. Rear coil spring minimum free height 11" maximum free height 13", 1/2" tolerance. No air bags, inner liners, spring rubbers, or torsion bars.

8.11 REAR SUSPENSION

All components must be magnetic steel. No covers allowed. All mounts must be welded solid or bolted solid. No floater or birdcages. Coil springs must be mounted vertically and be mounted as to cover over the centerline of the rear end housing. No coil over eliminators allowed. Rear shocks cannot be mounted on control arms. Rear shocks must be mounted to solid bracket attached to the rear end housing, below bottom of the axle tube, and to solid mount on upper frame. Must be located behind the rear end housing. All rear control arms and panhard bars must be straight. No suspension stops of any style permitted except the following: Solid safety chains permitted if securely mounted from frame to axle housing. These may not mount to any type of floating device. No springs or rubber mounts permitted. Safety chains or tethers must have slack during inspection. Rear suspension must utilize one of these three designs.

8.11.1 OEM stock design.

Requirements: Rear cross member control arm mounts must be in stock location on frame. All components must be unaltered OEM stock. Control arms must match the frame being used. Only steel, rubber, or nylon bushings may be used. Springs must remain in the stock location. Lower spring mount must be welded to rear end housing. Steel upper screw jack bolt and bucket.

8.11.2 After market three-link design. Requirements: 16-inches minimum, 24-inch maximum, center-to-center length lower control arms. Must utilize one upper control arm, solid tube design only. Upper must be mounted in the top center of rear end housing. Upper rear arm mount must be solid welded to housing. Upper mount must remain within 1-inch of centerline of driveshaft. Upper screw jack bolts and buckets must be magnetic steel. No floating components of any kind allowed. Panhard must mount behind rear end housing. Must be minimum 23-inches long measured center to center.

8.11.3 Multi leaf spring. Requirements: Must be magnetic steel multi leaf springs. No additional suspension components allowed except one shock per wheel. Adjustable aluminum lowering blocks permitted.

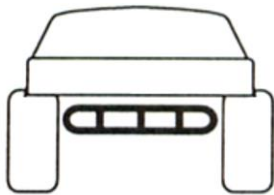
8.12 REAR END

Any steel OEM passenger car or truck housing allowed. Cambered rear end prohibited. Maximum allowable camber on rear end 0 degrees. Must be centered in chassis. All components must be magnetic steel except for lowering blocks (in leaf spring suspension) drive flanges, u-joint and axle caps. Floater design hubs allowed. No quick change of any design permitted. Inspection hole on housing required. Mini spools, or full spools ~~only~~ permitted. No lightweight or altered ring gear, center sections, or yokes permitted. Magnetic steel, solid axles only. One-piece drive flanges only. No heavy weight axle tubes permitted. Max. axle tube wall thickness is .250". No housing braces permitted.

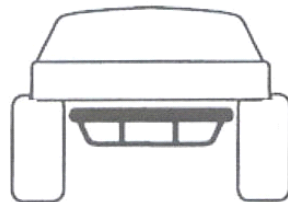
8.13 BUMPERS/RUB-RAILS

Steel front and rear bumpers required at all times. Rear bumper may be a maximum of six inches beyond rear deck. Must be welded solid or bolted securely with minimum 3/8-inch bolts. Bumper design must match diagram. Rear bumper must be square or round minimum 1.25-inch x 0.095-inch minimum wall thickness tubing. Fuel cell protector must be vertical to rear bumper. **Must match one of the design diagrams.** (A) May not extend more than five inches past outside of rear frame rails. (B) If wider than five inches past frame rail, it must be bent 90 degrees forward and the ends of the bumper must be attached to the frame rails. (C) Or bumper be of looped design. Loop must be mounted to frame. All designs must have exposed ends capped. Must have minimum of one vertical upright, minimum 1.250 x 0.065-inch wall thickness tube from bumper to fuel cell guard. Rear bumper may not be wider than rear tires. Two bar front bumper design must be 1.25-inch x 0.065-inch wall thickness minimum. Maximum 0.095-inch wall thickness. Must be mounted frame rail to frame rail. May not extend past frame rails. Bottom loop must be parallel to ground. Top loop must be directly above lower. Minimum spread center to center is 6½-inches.

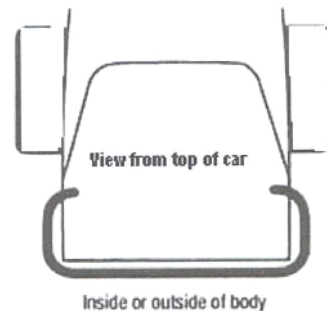
(A)



(B)



(C)



8.14 TIRES/WHEELS

Must use unaltered Hoosier G-60-15 IMCA plated tire Plated tires only. No softening or grooving permitted. Any alterations to factory specifications of tire groove not permitted. Tire grinding or straight siping permitted on the tread face only of the tire. May not extend past factory edge line of tire. No recaps permitted. Wheels must be 3-inch or 4-inch backspace only. No wheel adapter, spacers or bleeder valves permitted. Steel bead lock with steel bolts, on outside, on right rear only. Aluminum inner mud plug may be used on left rear. Aluminum, plastic disc or foam mud plug on right rear only. Aluminum and plastic disc mud plugs must have car number. Wheel cover must utilize magnetic steel bolt on style mounting hardware only. No dzus button mounting permitted. Mounting hardware must be integral to the wheel or bead lock or be welded to the wheel. Wheel width: minimum 8-inches; maximum 8.750-inches. All wheels must be 15-inches diameter with a minimum shell thickness of 0.095-inch minimum and a minimum weight of 19 lbs. One-inch magnetic steel lug nuts required. A MINIMUM TIRE PRESSURE OF 12 LBS WILL BE ENFORCED ON THE RIGHT REAR. Tire rubber samples may be taken for inspection at any time. Sampling and testing will be in accordance with **Section 3.6 Inspections**.

8.14.1 TIRE DUROMETER LIMIT

Five cars will be selected, at random, prior to the start of qualifying races. Durometer readings will be taken at three points across the rear tires (outside, middle, inside) of each car. The readings will be averaged together. The minimum allowable durometer reading of any tire prior to the qualifying races, B Main race, and A Main race will be ninety percent (90%) of the average reading.

Example: Average durometer reading of five selected cars: 50

Minimum allowable reading prior to race: 45

8.15 BRAKES

Must be magnetic steel OEM or approved four-wheel disc or drum style. Must maintain OEM dimensions for disc rotors, calipers, and drums. May not be altered or lightened in any form. No oil bath front hubs. Oversized and undersized piston, stock appearing, calipers not permitted. Bolt pattern may be changed. Larger studs permitted. Rear rotors may be aftermarket, minimum thickness 0.810-inch thick measured new. Vented rotors only. No scalloping, or ceramic coating of rotors. One brake-proportioning valve allowed. Valve must be installed to control proportioning of brakes front to rear only. Brake lines must be visible for inspection. No in-line devices between master cylinder and calipers, or drums. Rear caliper brackets must be welded or bolted solid to rear end housing.

8.16 EXHAUST

Header shall be of a round tube design only. All primary header tubes must enter directly into one collector, at the same point, at the end of the header. No merge collectors. Collector length max. 9". Turn down length max. 10", Overall combination max. length 19". Ceramic coated headers prohibited. Painted headers only permitted. Non stepped, painted headers only. No heat wraps on headers. All header designs must utilize two mufflers, one per side. Mufflers mandatory at all times. All exhaust must go thru mufflers. If a muffler falls off during a race, the car will be disqualified. Track decibel rule is 95 decibels at 100 feet. All cars exceeding 95 decibels will be required to modify system or be penalized. No exhaust sensors, cross-overs, extensions, inserts, or balance tubes. No pan evac systems allowed

8.17 FUEL SYSTEM

Racing fuel cell required. Maximum capacity 32 gallons (12 gallon recommended). Must be in a minimum 20-gauge steel container. Must be securely mounted, behind rear axle, between rear frame rails, minimum 4-inches ahead of rear bumper, minimum 10-inches above ground. Minimum two solid steel straps around entire fuel cell. Straps must be minimum 2-inches x 0.125-inches thick, or one-inch square tubing. All cell mounts must be magnetic steel, securely welded to frame/cage. Fuel cell protection bars must cover rear and extend past sides. No part of cell may extend past protection bar. Fuel cell protector must be vertical to rear bumper. Fuel cell vents including cap must have check valve. Fuel pick up must not be underneath cell. No cool cans. Limit one gas filter per car. Fuel line must be mounted and secured in an acceptable manner. No fuel lines in driver's compartment. Fuel pump must be OEM style in OEM location. No top flow air cleaner housing. Air cleaner stud or housing may not direct air into carburetor. A 1-inch thick, one piece, adapter plate may be used between the intake manifold and carburetor. Maximum 0.100-inch thick carburetor gaskets only. Adjustable throttle bore adapter plate prohibited. Carburetor

may be claim / exchanged, style for style (e.g.: four-barrel claims four-barrel; two-barrel claims two-barrel), following the same procedure as engine claim. Driver claiming carburetor may not claim engine or shocks on same night. Claimer motor permitted carburetors are **1**: Holley 500cfm 0-4412 unaltered except for Dorton specs only (part no. 0-80583-1) and 0-4412SA. On all carburetors, float bowl must face forward. Sealed GM 88958602 and GM 19258602 motors allowed one Holley four barrel. If carburetor spacer is used on crate, must use Speedway Motors part #545-64940 or Moroso part #64940 carburetor spacer. All components (float bowls and main body) must be Holley manufactured. Metering block and base plate may be billet aluminum, non-Holley. Aerosol carburetors prohibited. Beaver Dam Sealed Motor allowed one Holley 500 cfm 0-4412 or 0-4412SA unaltered except for Dorton specs only (part no. 0-80583-1).

8.18 FUEL

Gasoline only. No additives of any kind. Fuel must pass any test subjected by track officials. Fuel samples may be required at any time car is on speedway property. No E85 permitted. No scent masking agents permitted.

8.19 WEIGHT

Minimum weight of car and driver shall be 2500 lbs. Car shall be weighed post-race. No adding of liquids or lost components. Track scales shall be considered the **official** scale. There is no tolerance on weight. No weights and or loose components in the driver's compartment. No weights outside body, or above deck lid. Weights must be securely mounted to frame or roll cage, painted white, with car number. Weights must be mounted with a minimum of two ½-inch bolts. No hollow bolts, titanium of any kind, gun drilled, magnesium, carbon fiber, tungsten, or any other lightening products.

8.20 BATTERY / STARTER

One twelve-volt battery only. Must be shielded and securely mounted between frame rails. Positive terminal must be covered. Starter must bolt to block, in OEM location and directly engage flywheel / flex plate. Car must have capability of being started without being pushed or pulled. Car must leave staging area, or restart on racetrack, unaided, or go to rear of event.

8.21 GAUGES/ELECTRONICS

No transmitting or listening devices, timing retard controls, or digital gauges, except for Raceceiver. No electronic devices capable of storing or transmitting information except analog tach. No adjustable ignition boxes, or crank triggers. No magnetos. Ford/Chrysler may use HEI distributor. No additional ignition accessories except one rev-limiting chip. Chip shall limit high-end setting. Crate and sealed motor must use HEI distributor with MSD #8728 rev control module with Beaver Dam provided chip installed. Failure to use Beaver Dam chip will result in disqualification. All motors must use MSD #8728 rev control module with rev chip or MSD # 8727CT (MSD#8727CT will be required in 2022) with the correct setting of rpm for motor package required. Crate, 602, powered cars must use 6200 rpm chip or limit setting. All others must use 7000 rpm chip or limit setting. Rev control module must be out of drivers reach and visible for inspection without the removal of any body panels. Location of rev control module subject to approval of Beaver Dam Officials. Rev-limiting chip must function as designed. No remote coils or accessories. All wiring must be visible for inspection. Only permitted gauges are analog style, oil pressure, fuel pressure, brake pressure, water temperature and tachometer. Analog

memory recall tachometer permitted. OEM type alternator with internal regulator permitted. No traction control devices of any type or style. Upon request, any ignition component must be surrendered to track officials for inspection and certification by original manufacturer. Securely mounted camera's permitted. Camera's may not be positioned to record any suspension movement. Competitors are responsible for any liability from publishing any videos.

8.22 TRANSMISSION/ DRIVESHAFT

All forward and reverse gears must be operational. Transmission must contain a neutral position. With engine running, driver must be able to engage and disengage all gears in the transmission. Only permitted transmissions are OEM production models used in a passenger car or truck. No aftermarket, in-out boxes, or quick-change type transmissions allowed. Functioning shift levers must be in the OEM location. One OEM style, full size, steel or aluminum flywheel, or OEM style, full size, steel flex plate allowed. Flex plate or flywheel must be bolted directly to the end of the crankshaft.

8.22.1 Automatic:

Must be unaltered two or three-speed OEM production case with a functioning OEM appearing pump. Original aluminum bell housing may be replaced with a steel or aluminum explosion proof bell housing. If utilizing the stock aluminum bell housing, a steel scatter shield, 3-inches wide x 0.125-inches minimum thickness covering 270 degrees around the top of the flex plate area must be used. The only external lines permitted are for the transmission oil cooler.

8.22.2 Manual:

Must be unaltered three or four speed OEM production case and utilize a 7.25-inch or larger clutch. May be single or multi disc design. Steel or aluminum permitted, must be bolted to the flex plate / flywheel. All components must rotate, consistent with engine RPM when transmission is in any gear. Must utilize steel explosion proof bell housing with only one hole for throw out bearing lever or hydraulic hose. Bell housing must cover the upper 270 degrees of the flywheel. Hydraulic clutch pedal permitted with manual transmission only.

8.22.3 Driveshaft:

Minimum 2-inches diameter. Must be painted white, magnetic steel, and magnetic steel yoke and joints. 360-degree drive shaft loop required. Minimum of 0.250-inch x 2-inch steel flat or 1-inch x 0.095-inch round or square tubing located 6-inches behind the front end of the drive shaft.

8.23 ENGINE COMPARTMENT

Rear of engine block (bell housing mounting surface) must be minimum 72-inches forward of centerline of rear end housing. Engine offset may not exceed 2-inches from centerline of front cross member with engine mounted level. Minimum crankshaft centerline to ground is 11-inches. V belts style steel or aluminum pulleys only. Steel, copper, brass, or aluminum radiator must mount in front of engine. No vacuum pumps or pan evacuation systems.

8.24 ENGINE SPECIFICATIONS

All cars must clearly display, on both sides, front roof post which engine they are competing with. Must be contrasting in color from body, minimum 2-inches tall and display "CLAIM", "CRATE", or "SEALED". Markers not acceptable.

8.24.1 GM CRATE MOTOR #88958602 or #19258602:

Must be unaltered with original seals intact or be rebuilt to factory specifications and resealed after inspection by Beaver Dam technical officials. All rebuilt engines must be rebuilt for factory specifications and may not be altered in a manner to improve performance. Must use correct distributor, rev-limiter module with a functioning 6200-rpm rev chip or MSD # 8727CT module set to 6200 rpm, rocker arms, pushrods, and valve springs. No alterations permitted except for the following. Installation of aftermarket oil pan - Champ pan part #cp57lrb and Champ pick up part #cp1012sb permitted. Penalty for tampering is covered in the penalty section of rulebook. Driver using this engine not subject to claim and may not claim another driver's engine. Use of this engine is still subject to track inspection. Carburetor claim / exchange procedures apply.

8.24.2 CLAIMER ENGINE:

All claimer engines must be able to be used in a passenger car/ truck without alterations. External engine casting, mounting holes may not be altered.

Block: Must be OEM steel. Must be factory production GM, FORD, or Chrysler. No Ford SVO or Chrysler W components allowed. Only GM block numbers permitted are 10105123, 10066034, 3892657, 3914660, 3914678, 3932388, 3932386, 3956618, 3970000, 3970006, 3970010, 3970014, 10066033, 10066036, 10243880, 14010207, 14010209, 14010287, 14016376, 14016379, 10054727, 14088528, 10488548, 14088552, 14093638, 14101148. Stroke must match block. Overbore maximum 0.060-inches. Maximum cubic inch 361-GM, 363-Ford, 370-Chrysler. Maximum compression on all claimer engines shall be no greater than 9.0 to 1. Flat top or dish style pistons only. No gas ported pistons. OEM type steel crankshaft and rods matching OEM specs (weight, stroke, journal diameter) must be used. Crankshaft cannot be lightened (no aeroing, bullnose, knife edge, undercutting or drilling of second or third rod throws allowed). Conventional flat tappet camshaft and lifters only. No altering of lifter bores. Must utilize stock firing order (GM: 1-8-4-3-6-5-7-2). Oil restrictors may be used. Wet sump-oiling systems only. One-inch inspection plug in pan, with no obstructions to crank and rods, highly recommended.

Cylinder heads: OEM steel production heads only. The only permitted GM heads are #14079267, 3986336, 3986339, 3986339x, 3986388, 3932441, 376445, 3928454, 3932454, 3876487, 3973487, 3973487x, and 3973493, 3951598, 468642, 330862, 333882, 3998920, 3998991, 3998993, 3998997, 3970126. Maximum valve size on these heads is 2.02-inch intake, and 1.60-inch exhaust. May use Engine Quest SR #CH3501. This head must remain as produced. Production valve size may not be altered. No porting, polishing, or unapproved alterations to any head. Guide plates permitted. Screw in shoulder studs permitted Maximum size 3/8-inches. Poly locks and steel roller tip rockers, or full roller rockers permitted. Cars using the full roller rocker must run 6800 rpm rev limiter or limiting module. Magnetic steel pushrods, max. diameter of 5/16", and magnetic steel retainers required. No stud girdles. No shaft rockers. 1.250-inch O.D. valve springs. (+ or - .015 tolerance) No beehive valve springs.

Intake: Unaltered OEM, steel, low rise, production two- or four-barrel manifold permitted. Optional aluminum IMCA stamped Weiland manifold permitted. Chevrolet #7547-1, Ford 302 # 7515, 351 Windsor -8023, Cleveland -7516, Chrysler #7545. Edelbrock: GM#2701, #2716; Ford

#7181, #7183; Chrysler #2915. Cooling lines allowed on aluminum intakes. Must be unaltered. Accessories; OEM style steel or aluminum water pump only. Unaltered OEM type harmonic balancer.

8.24.3: BEAVER DAM SEALED MOTOR:

Must utilize all specs from claimer motor. Must utilize the HEI distributor with the MSD #8728 rev control module with a functioning 7000-rpm chip installed or MSD #8727 CT with 7000 rpm setting. Motor must be sealed in accordance with track tech staff procedures. Cost for inspection and or sealing is \$50 per motor plus mileage to inspection site.

8.25 ENGINE CLAIM - SWAP RULES

(A) \$550 (U.S.) cash claim on engine, flywheel and balancing plates plus the engine from the claimer's car used in the A Main race. \$50 of this goes to track for overseeing removal of the engine.

(B) Claim does not include: 1. clutch, 2. pressure plate, 3. bellhousing, 4. headers, 5. carburetor, 6. starter, 7. motor mounts, 8. oil/temp. sending units, 9. carburetor spacer, 10. fan and pulleys, 11. clutch ball, 12. clutch arms, 13. throw out bearing, 14. dip stick, 15. water pump, 16. fuel pump, rod and plate, 17. distributor. 18. plug wires, 19. water outlet and restrictor, 20. breathers, 21. external oil restrictors (must furnish plugs).

(C) First place finisher will report to the winner's circle. Position 2-4 must report directly to claim area. The winner will report directly to the claiming area directly after the winner's circle and victory lap. The first four positions are subject to engine claim by any other driver finishing fifth on back in feature that has not been lapped by the fourth-place driver. Failure to report directly to claim area will result in disqualification and loss of money and points for first infraction (engine is still claimable); will be treated as engine claim refusal for second infraction. Driver allowed one claim ONLY per event, regardless of outcome of that claim. In case of multiple claims on same engine, a drawing will be held to determine which driver gets the engine. Should the first eligible driver withdraw his claim, his claim card is marked, as a claim and the next qualified driver will be allowed the engine claim. The claim is not charged to those drivers not awarded the claim.

(D) Should any driver voluntarily withdraw a legal claim, he/she will be charged with a claim and it will result in a \$200 fine.

(E) Driver making claim must drive his racecar immediately after finish of feature, under its own power, directly to claiming area. Only involved drivers and officials allowed in claiming area.

(F) Claims must be made to official immediately after end of feature race and claimed engine must be completely removed within one hour from time claim is made and driver agrees to sell claimed engine.

(G) Claims must be made to official, and driver must present \$ 550 cash.

(H) Refusal to sell forfeits all cash and contingency winnings for feature, any trophies earned in feature, plus ALL points in all claim divisions for the season. Any driver refusing to sell engine, when claimed within rules, regardless of his reinstatement with, loses his right to claim any driver's engine for 12 calendar months from date of reinstatement. Claim is not charged to claiming driver in case of a refusal.

(I) First refusal to sell will result in loss of ALL points in all claim divisions for the season and will also result in driver being suspended from Sport MoD Division for one race from date of refusal and until \$1,000 cash fine is paid to the Beaver Dam Discretionary Fund. Fine must be paid prior to driver returning to competition. Second refusal to sell will result in driver being suspended from events for 30 days from date of refusal and until \$2,000 cash fine is paid to the Beaver Dam Discretionary Fund. Third refusal results in permanent suspension from the Sport MoD Division.

(J) Once an engine claim has been made and accepted, the engine is not to be started, under any circumstances. The car is to be pushed to the area where removal will take place. Any driver starting a claimed engine will be immediately disqualified from that event and will be suspended from all events until such time as a \$100 fine is paid to the Beaver Dam Discretionary Fund. All claimed engines must be removed from car at the track.

(K) If during the engine removal process, the claimed engine is found to be illegal, the claimed driver will be disqualified. Claimer has the option to accept or decline claimed engine. If the claimer declines, the money will be returned to the claimer.

(L) Claim is not in effect on opening night and driver must have been at track on opening night to be eligible to claim second night of season. Following that, no driver may claim unless he/she has competed at that track the two previous consecutive sanctioned events.

(M) No one driver may claim any other one driver more than once during current calendar year.

(N) A disqualification will not affect a legal claim. The engine will be pulled and transferred prior to any penalties being assessed.

(N) Drivers are not claimable the first night at the track.

(O) Unsportsmanlike conduct during the claim procedure, or while pulling the engine, will result in an immediate minimum \$100 fine. The driver will be suspended from all events until such time as the fine is paid to the Beaver Dam Discretionary Fund.