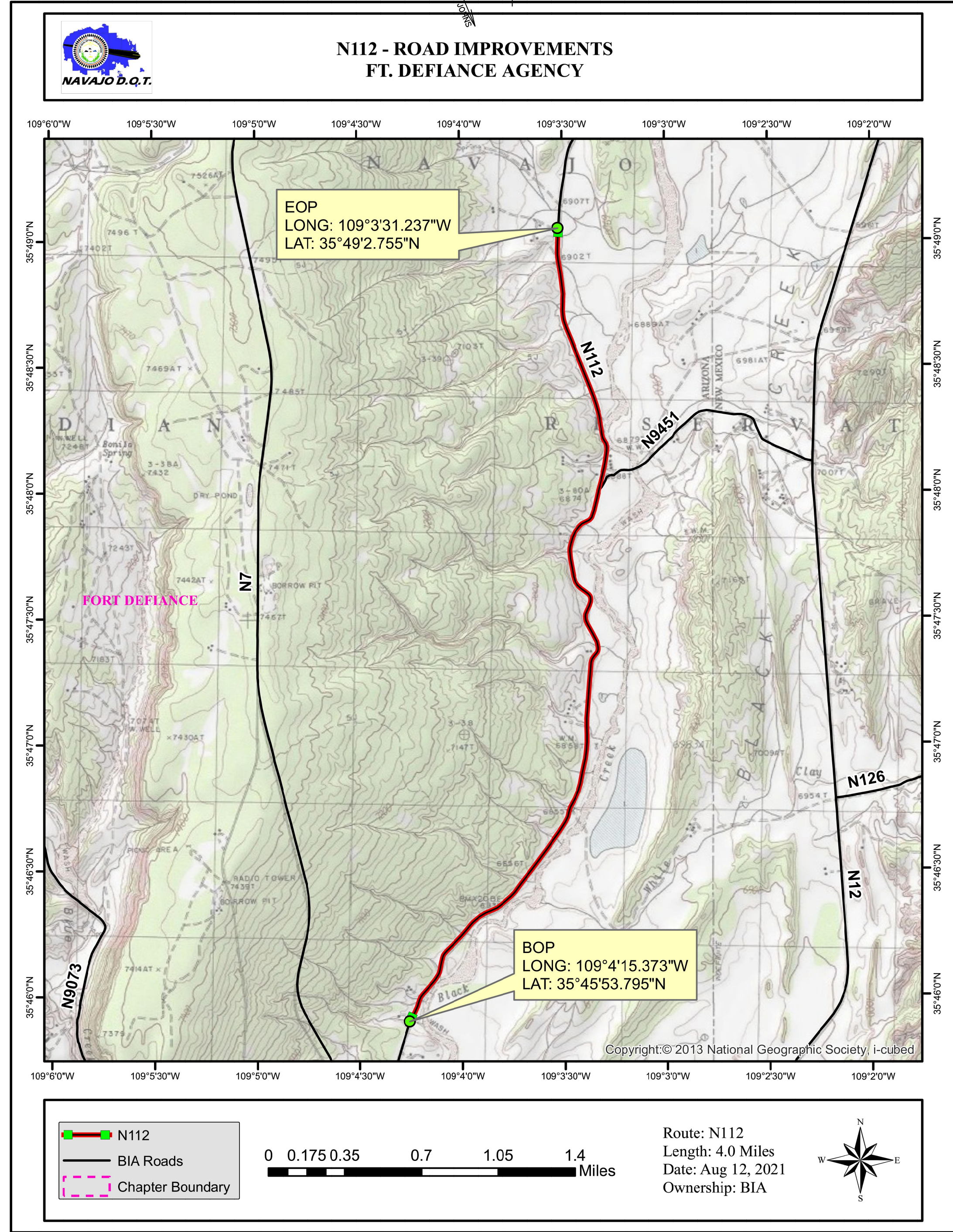
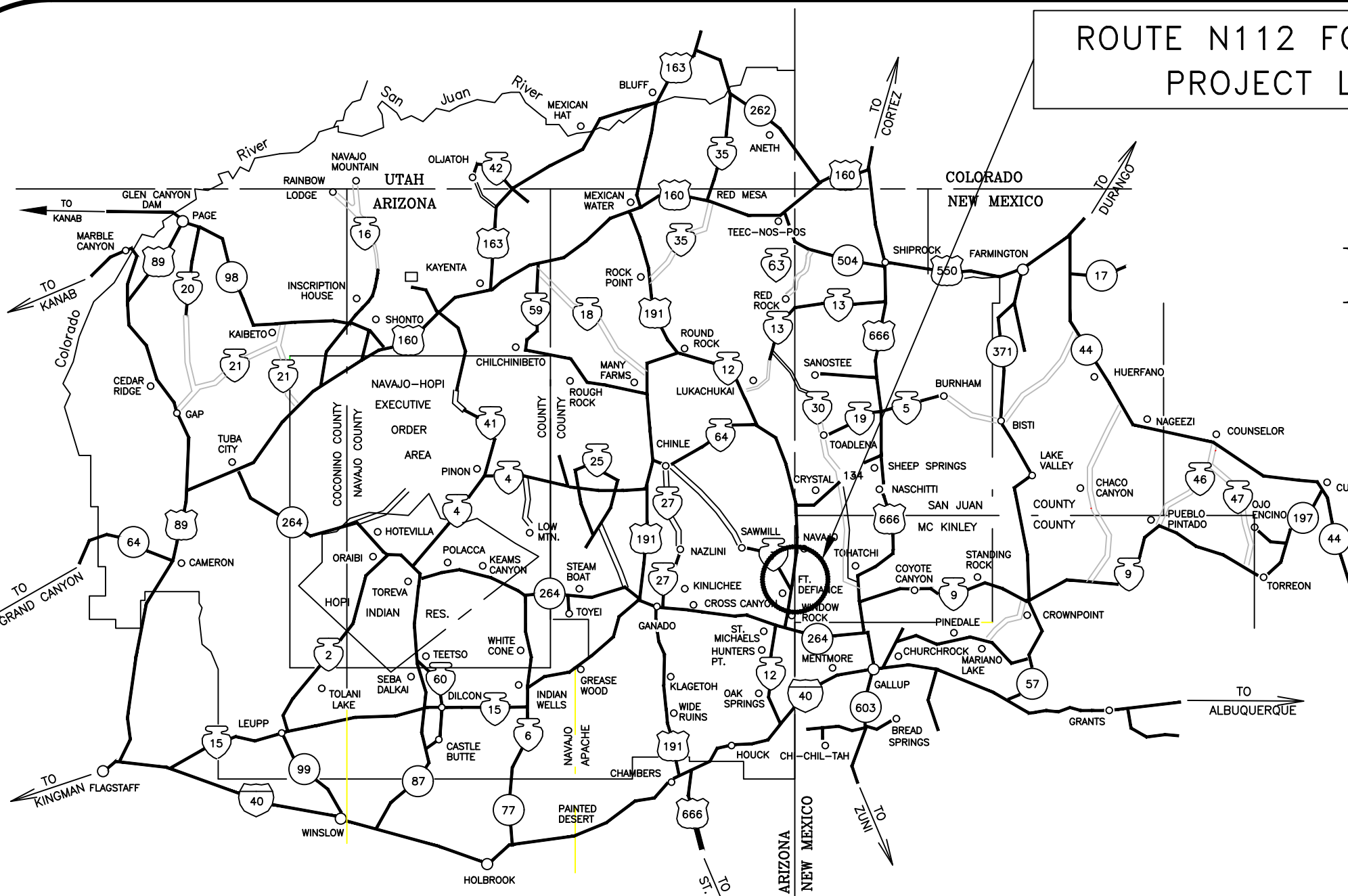


ROUTE N112 FORT DEFIANCE
PROJECT LOCATION

Fort Defiance N112 Road Improvements Blade and Gravel 3.06 Miles of Existing Road NN FET Funding

Sheet Number	Sheet Index
1	Cover Sheet
2	General Construction Notes
3	Quantity Sheet
4	Overall Site Plan
5	Road Section Detail
6	Temporary Traffic Control Plan

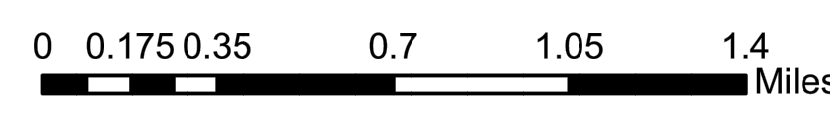


LEGEND

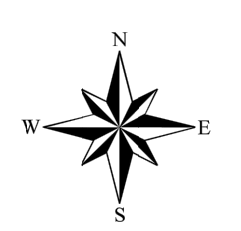
- STATE LINE
- RESERVATION LINE
- COUNTY LINE
- TOWNSHIP or RANGE LINE
- SECTION LINE
- HIGHWAY RIGHT-OF-WAY LINE
- UNFENCED PROPERTY
- SECTION CORNER AND 1/4 CORNER
- POWER LINE AND POLES
- TELEPHONE LINE AND POLES
- GUY WIRE OR ANCHOR
- TRAFFIC SIGN
- DELINEATORS
- BARBED WIRE FENCE
- CHAIN LINK FENCE
- CATTLE GUARD
- CULVERTS
- CONCRETE BOX CULVERTS
- GROUND LINE - EARTH
- GROUND LINE - ROCK
- EXISTING ROAD
- SIDE ROAD TURNOUT
- TREES and SHRUBS
- CHANNEL or DITCH
- DIKE or DITCH BLOCK
- RIP-RAP
- RAILROAD TRACK
- GAS LINE
- DWELLING
- RIGHT-OF-WAY MONUMENT

NAVAJO DIVISION OF TRANSPORTATION
P.O. BOX 4620 WINDOW ROCK, AZ 86515

Project: Pinon N8030 Culvert Installations
Scale: NTS
Sheet: 1 of 6
Revised: October 19, 2021



Route: N112
Length: 4.0 Miles
Date: Aug 12, 2021
Ownership: BIA



GENERAL NOTES:

1. ALL WORKMANSHIP AND MATERIALS SHALL CONFORM TO THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF ROADS AND BRIDGES ON FEDERAL HIGHWAY PROJECTS, FP-14
2. ALL PERMANENT AND TEMPORARY ROADSIDE SIGNS AND PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS" (CURRENT EDITION) AND IN ACCORDANCE WITH THE DETAILS ON THESE PLANS. PLACEMENT OF "STOP" BAR, PERMANENT TRAFFIC SIGNS AND PAVEMENT MARKINGS SHALL BE FIELD ADJUSTED AS DIRECTED BY THE CONTRACTING OFFICIAL(CO), AT NO ADDITIONAL COST TO THE NAVAJO DIVISION OF TRANSPORTATION.
3. ALL CONSTRUCTION PRACTICES AND PROCEDURES WILL COMPLY WITH THE PERTINENT PROVISIONS OF THE OCCUPATIONAL SAFETY AND HEALTH STANDARDS (TITLE 29, CODE OF FEDERAL REGULATIONS) PUBLISHED BY THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION, DEPARTMENT OF LABOR.
4. THE CONTRACTOR WILL KEEP EXISTING PROPERTIES/RESIDENCES AND INTERSECTING ROADS ACCESSIBLE AT ALL TIMES CONTRACTOR WILL ALSO HAVE TO PLACE SIGNS AT ALL DRIVEWAYS AND TURNOUTS, FOLLOWING APPROVED TRAFFIC CONTROL PLAN.
5. THE TEMPORARY TRAFFIC CONTROL DETAILS SHOWN REFLECTS GENERAL REQUIREMENTS FOR THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR PREPARING AND SUBMITTING A TRAFFIC CONTROL PLAN IN ACCORDANCE WITH THESE DETAILS, TAKING INTO ACCOUNT THE CONTRACTOR'S CONSTRUCTION SEQUENCING PLAN, MUTCD AND THE SUPPLEMENTAL SPECIFICATIONS FOR SECTION 635 - TEMPORARY TRAFFIC CONTROL. THE CONTRACTOR SHALL ALSO SUBMIT A COPY OF HIS TRAFFIC CONTROL PLAN TO THE NAVAJO DIVISION OF TRANSPORTATION (2) - WEEKS PRIOR TO START OF CONSTRUCTION.
6. THE DESIGN FEATURES INCLUDING HORIZONTAL AND VERTICAL ALIGNMENTS, TYPICAL SECTIONS, AND OTHER DESIGN DETAILS SHOWN SHALL NOT BE ALTERED OR MODIFIED IN ANYWAY DURING CONSTRUCTION WITHOUT THE EXPRESSED WRITTEN DIRECTION AND WRITTEN APPROVAL OF THE NAVAJO DIVISION OF TRANSPORTATION (NDOT) DIVISION MANAGER THROUGH THE CO, UNLESS OTHERWISE NOTED IN THESE PLANS OR SPECIFICATIONS, DRAINAGE STRUCTURES AND TURNOUTS SHALL BE INSTALLED AS SHOWN WITH ONLY MINOR CORRECTIONS IN LOCATION, SKEW, AND/OR INVERT ELEVATIONS AS NEEDED TO FIT FIELD CONDITIONS. TURNOUTS MAY NOT BE SHIFTED MORE THAN 17 FEET FROM THE LOCATIONS SHOWN ON THE PLANS WITHOUT THE WRITTEN APPROVAL OF THE NDOT DIVISION MANAGER THROUGH THE CONTRACTING OFFICER.
7. THE CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY AND EXPENSE FOR DISPOSAL AND/OR CONSTRUCTION DERIS IN ACCORDANCE WITH SECTIONS 107 AND 203 OF THE FR-14 AS WELL AS ANY AND ALL PERMIT REQUIREMENTS. THIS WORK SHALL BE INCIDENTAL OBLIGATIONS OF THE CONTRACTOR.
8. THE BIDDER SHALL READ AND MAKE CAREFUL EXAMINATION OF THE PLANS, SPECIFICATIONS, QUANTITIES, MATERIAL, SURVEYING REQUIREMENTS, AND VISIT THE SITE OF THE PROPOSED CONSTRUCTION TO BECOME FAMILIAR WITH THE SITE CONDITIONS AND LIMITATIONS BEFORE MAKING A PROPOSAL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL ERRORS RESULTING FROM THE FAILURE TO MAKE SUCH AN EXAMINATION. ANY INFORMATION DERIVED FROM THE MAPS, PLANS, SPECIFICATIONS, PROFILES, DRAWINGS OR THE ENGINEER, SHALL NOT RELIEVE THE CONTRACTOR FROM ANY RISK OR FROM FULFILLING THE TERMS OF THE CONTRACT. THERE ARE SEVERAL AREAS WITH LIMITED WORKING ROOM WITHIN THE PROJECT RIGHT-OF-WAY, AND/OR WITH EXISTING FEATURES WITHIN OR NEAR THE PROJECT RIGH-OF-WAY, THAT WILL REQUIRE "SPECIAL" CONSTRUCTION PROCEDURES.
9. THE CONTRACTOR IS REQUIRED TO SUBMIT A REVISED PIPE LIST TO THE NDOT DIVISION MANAGER THROUGH THE CO. BASED ON THE FIELD STAKING IN ACCORDANCE WITH SECTION 152 OF THE CONTRACT SUPPLEMENTAL SPECIFICATION, THE APPROVAL OF ANY AND ALL REVISED PIPE LISTS WITH ACCOMPANYING DRAWINGS IS RENDERED AS A SERVICE ONLY AND IS NOT CONSIDERED A GUARANTEE OF MEASUREMENTS, QUANTITIES, INSTALLATION PROCEDURES, AND/OR DIMENSIONS, NOR SHALL IT BE CONSIDERED AS RELIEVING THE CONTRACTOR FROM COMPLYING WITH THE CONTRACT SPECIFICATIONS AND DESIGN PLANS. THE CONTRACTOR IS HEREBY NOTIFIED THAT UNDER NO CIRCUMSTANCE SHALL ANY DRAINAGE STRUCTURE(S) BE INSTALLED BELOW THE NATURAL FLOW LINE OF THE WASH, CHANNEL, ARROYO OR DITCH LINE.
10. NO WORK SHALL BE PERFORMED OR GROUND DISTURBED OUTSIDE THE DESIGNATED CONSTRUCTION LIMITS IN ACCORDANCE WITHOUT WRITTEN APPROVAL BY THE NDOT MANAGER UNLESS OTHERWISE SHOWN AND LABELED ON THESE PLANS AS "CONSTRUCTION ZONE".
11. THE DETAILS SHOWN ON THE STORM WATER POLLUTION AND EROSION/SEDIMENT CONTROL DETAILS ARE GENERAL REQUIREMENTS TO BE USED BY THE CONTRACTOR IN PREPARING A STORM WATER POLLUTION PREVENTION PLAN ALONG WITH THE REQUIREMENTS IN SECTION 629 OF THE STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGE CONSTRUCTION FP-14. SWPPP IS ONLY REQUIRED AT THE DRAINAGE PIPE REPLACEMENT LOCATIONS. THE CONTRACTOR IS REQUIRED TO SUBMIT A COURTESY COPY OF THE APPROVED SWPPP TO THE NAVAJO NATION WATER QUALITY EPA OFFICE.
12. THE QUANTITIES SHOWN ARE FOR ESTIMATING PURPOSES ONLY AND TO COMPARE AND CANVAS BIDS. ACTUAL PAY QUANTITIES WILL BE DETERMINED IN THE FIELD FOR AUTHORIZED CHANGES THAT EFFECT THE QUANTITIES. ANY OVER-RUN OR UNDER-RUN OF QUANTITIES SHALL BE SUBJECT TO FEDERAL ACQUISITION REQUIREMENT 52.211-18, VARIATION IN ESTIMATED QUANTITY.
13. IMMEDIATELY PRIOR TO PLACING EMBANKMENT, AGGREGATE BASE AND/OR RECYCLED MATERIAL. THE TOP 6 INCHES OF THE ORIGINAL GROUND, OR FINISHED SUBGRADE (INCLUDING TURNOUTS) SHALL BE CHECKED FOR COMPACTION AND GRADE. IF COMPACTION DOES NOT MEET THE MINIMUM SPECIFIED COMPACTION AND TOLERANCE REQUIREMENTS, THE ORIGINAL GROUND AND/OR SUBGRADE SHALL BE RE-WATERED AND/OR SCARIFIED AS NEEDED AND RE-COMPACTED TO THE REQUIRED DENSITY AND TOLERANCE. AT THE CONTRACTOR'S EXPENSE. IN NO CASE SHALL ANY EMBANKMENT OR SURFACING MATERIAL BE PLACED ON FROZEN, MUDDY OR UNSTABLE NATURAL GROUND OR SUBGRADE. THIS WORK SHALL BE CONSIDERED AN INCIDENTAL OBLIGATION TO THE CONTRACTOR.
14. THE CONTRACTOR SHALL REMOVE, CLEAN, AND STOCKPILE ALL SALVAGEABLE EXISTING CULVERTS, ASPHALT, GUARDRAIL, CATTLE GUARDS, FENCING MATERIALS, ETC., AS CALLED FOR ON THESE PLANS.
15. THE CONTRACTOR SHALL BE REQUIRED TO REPAIR ALL DENTED, BENT OR OTHERWISE DAMAGED PIPE EDGES FOR THE SECTION AS CALLED FOR REPAIR WORK. THIS WORK SHALL MEET THE APPROVAL OF THE CO, AND IS EXPECTED TO INCLUDE STRAIGHTENING OF DENTED/BENT CULVERT EDGES, WELDING OF CUTS/TEARS IN THE EXISTING CULVERTS, TRIMMING BACK THE EXISTING CULVERT TO CREATE AN END SUITABLE FOR AN EXTENSION COLLAR, ETC. NO SEPARATE PAYMENT FOR THIS WORK WILL BE MADE.
16. THE ROADWAY TYPICAL SECTION SHOWN IS A BASIC TEMPLATE TO WHICH THE PROJECT IS TO BE STAKED AND BUILT HOWEVER, THERE WILL BE LOCATIONS WHERE, DUE TO EXISTING GROUND SCHEDULE CONDITIONS, TURNOUTS, CULVERTS OR OTHER OTHER STRUCTURES, ETC. THE SHOWN TYPICAL SLOPES CANNOT BE CONSTRUCTION. IN THIS CASE, THE NDOT DIVISION MANAGER, THROUGH THE CO, SHALL BE CONSULTED FOR CHANGES IN THE TYPICAL SECTIONS, DESIGN SLOPES, AND/OR OTHER ADJUSTMENTS BEFORE PROCEEDING WITH THE WORK UNLESS NOTED OTHERWISE ON THE PLANS. IN NO CASE SHALL THE CUT AND FILL BACK SLOPES BE BUILT STEEPER THAN THE MAXIMUM ALLOWED IN THE ROADWAY TYPICAL SECTION AS SHOWN.
17. THE CONTRACTOR SHALL SAW CUT (FULL DEPTH) THE EXISTING ASPHALT PAVEMENT (INCLUDING TURNOUTS) WHERE NEW ASPHALT IS TO BE TIED INTO OLD ASPHALT PAVEMENT AT THE LOCATIONS NOTED ON THE PLANS. THE CONTRACTOR SHALL MATCH THE NEW ASPHALTIC CONCRETE PAVEMENT SURFACE TO EXISTING PAVEMENT SECTION AT THE TIE-IN POINTS AND TO PROVIDE FOR A SMOOTH TRANSITION AS DIRECTED BY THE CO. ALL SAWED PAVEMENT EDGES TO RECEIVE ASPHALT TACK COAT.
18. ROADWAY SHALL BE OPENED TO FULL TWO WAY TRAFFIC AT THE END OF WORK DAY AND WEEKENDS. SEE TRAFFIC CONTROL SHEETS FOR ADDITIONAL REQUIREMENTS.
19. THE FOLLOWING SHALL BE SUBMITTED AND APPROVED BY THE NDOT PRIOR TO ISSUANCE OF NTP: A QUALITY CONTROL PLAN B. TRAFFIC CONTROL PLAN C. STORMWATER POLLUTION PREVENTION PLAN D. SAFETY PLAN E. CONSTRUCTION PLAN & SCHEDULE.
20. PROTECTION OF SURVEY CONTROL & MONUMENTS: CONTRACTOR SHALL REPLACE IF DAMAGED OR REMOVED. NO ADDITIONAL COMPENSATION SHALL BE MADE.

NAVAJO DIVISION OF TRANSPORTATION

P.O. BOX 4620 WINDOW ROCK, AZ 86515

Project: Fort Defiance N112

Scale: NTS

Sheet: 2 of 6

Rev Date: October 19, 2021

QUANTITY SHEET
Fort Defiance N112 Grade and Gravel
Fort Defiance, Apache County, Arizona

SUMMARY OF QUANTITIES			
FP-14 SPEC.	ITEM DESCRIPTION	UNIT	QUANTITY
SECT. 212	BLADE EXISTING ROAD - 20-FEET WIDE X 2 DITCHES @ 12-FEET WIDE X 3.06 MILES	SY	79,000
SECT. 301	PLACE GRAVEL AND COMPACT - 4 INCH DEPTH x 20-FEET WIDE x 3.06 MILES	TONS	7,558

NOTE: MOBILIZATION, COMPACTION WATER AND TRAFFIC CONTROL ARE INCIDENTAL TO EACH BID ITEM

NAVAJO DIVISION OF TRANSPORTATION
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Project: Fort Defiance N112 Blade and Gravel
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OVERALL SITE PLAN
Fort Defiance N112 Grade and Gravel
Fort Defiance, Apache County, Arizona

← TO NAVAJO NM



END PROJECT STA 10+00

TO FORT DEFIANCE AZ →

SCOPE OF WORK:

1. PROJECT TO FOLLOW EXISTING CENTELINE OF EXISTING ROAD ONLY
2. BLADE & SHAPE EXISTING ROAD TO DETAIL SHEET STA 171+63 TO STA 10+00
3. PLACE 4-INCHES OF GRAVEL STA 171+63 TO STA 10+00
4. UTILITY LINES SHALL BE LOCATED AND AVOIDED, NO EXCAVATION

BEGIN PROJECT STA 171+63
AT END OF ASPHALT PAVEMENT

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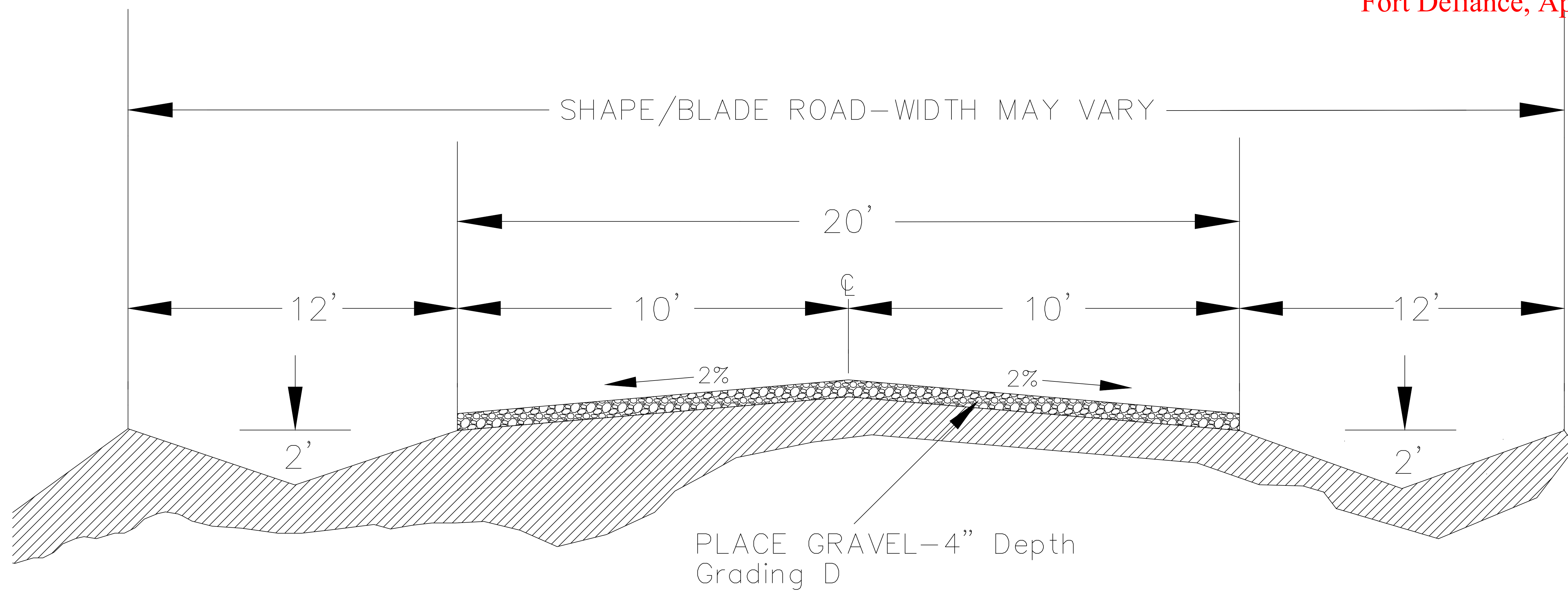
Project: Fort Defiance Blade and Gravel

Scale: 1"=50'

Sheet: 4 of 6

Rev Date: October 19, 2021

TYPICAL ROAD SECTION
Fort Defiance N112 Blading and Gravel
Fort Defiance, Apache County, Arizona



TYPICAL ROAD SECTION—NTS—FORT DEFIANCE N112

*Width of roadway may be adjusted to suit actual field conditions as approved by Navajo DOT.

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Project: Fort Defiance N112
Scale:
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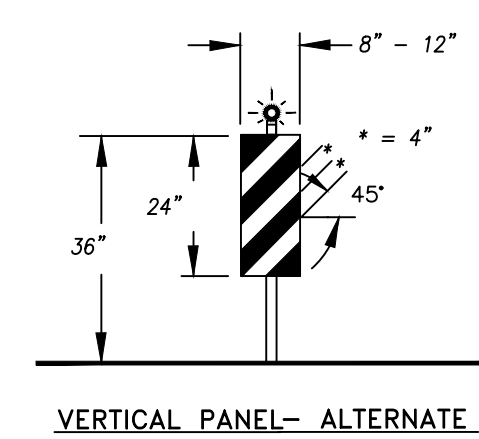
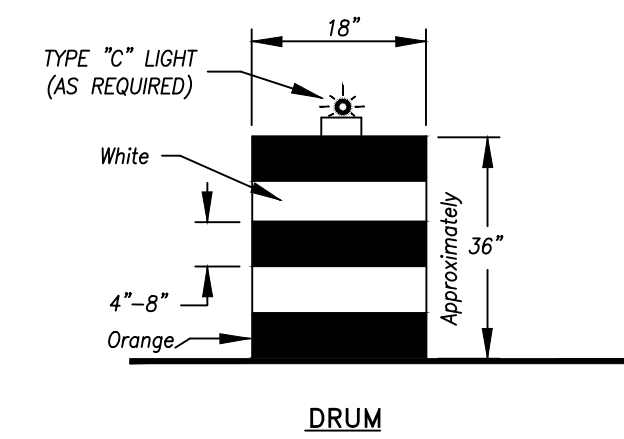
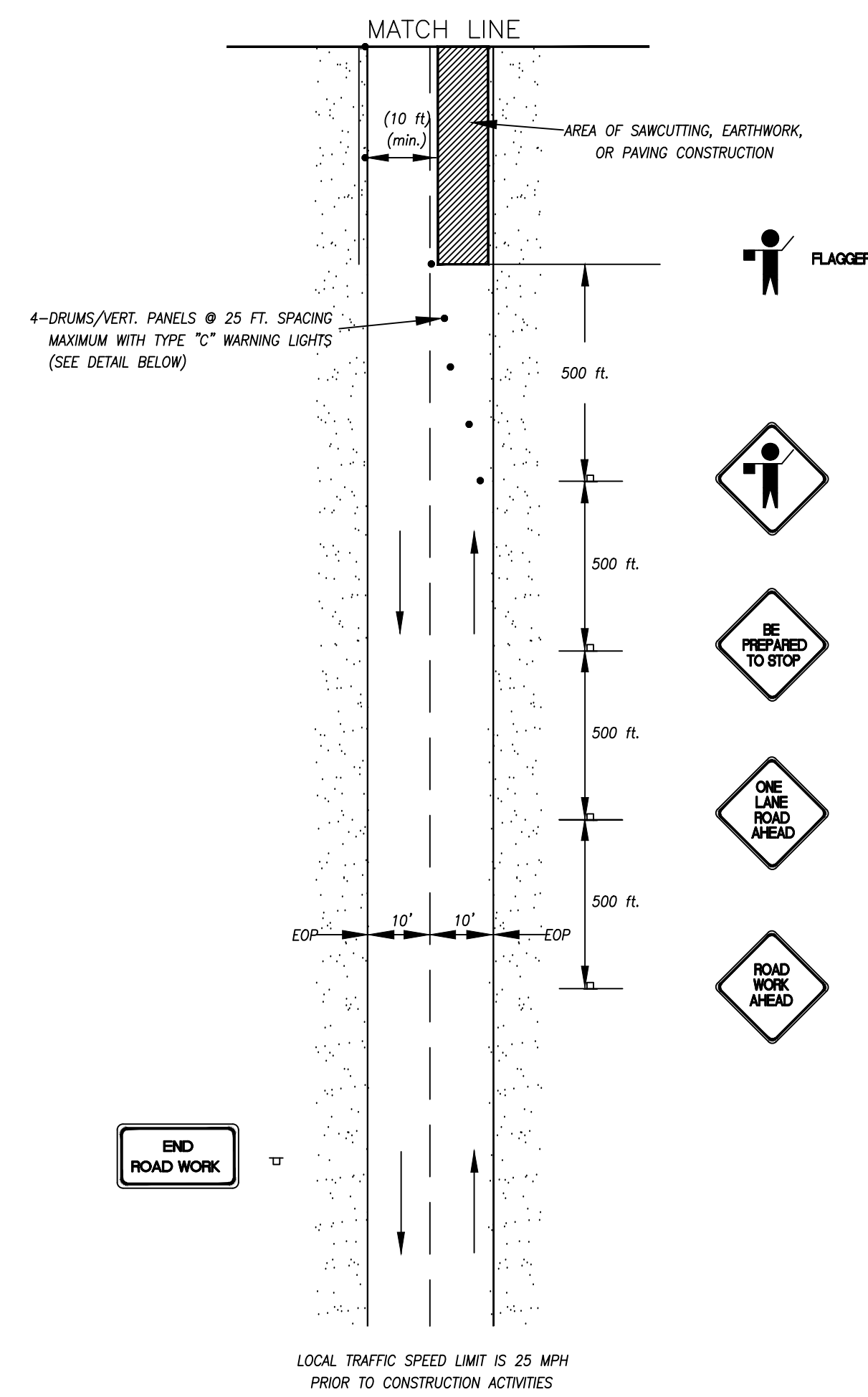
TEMPORARY TRAFFIC CONTROL PLAN

GENERAL NOTES

1. ALL TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (for Streets and Highways) (MUTCD) MANUAL (2009 EDITION AND AMENDMENTS) AND THE SUPPLEMENTAL SPECIFICATIONS FOR THIS PROJECT.
2. ALL TRAFFIC CONTROL DETAILS SHOWN ARE ONLY A GUIDE. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR PREPARING AND IMPLEMENTING HIS/HER TRAFFIC CONTROL PLAN (TCP) IN ACCORDANCE WITH THESE DETAILS AND THE MUTCD MANUAL. ANY ADDITIONAL TRAFFIC CONTROL DEVICES CALLED FOR ON THE CONTRACTOR'S TCP WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL.
3. ALL TRAFFIC CONTROL SIGNS SHALL BE PLACED AT THE PROJECT LIMITS AND REMAIN IN PLACE THROUGHOUT THE DURATION OF THE PROJECT.
4. FLAGGERS SHALL BE STATIONED LEFT & RIGHT WHEN EQUIPMENT IS CROSSING OR WORKING WITHIN EXISTING ROADWAY PRISM OR AT DETOURS.
5. IN AREAS OF EXISTING ROADWAY WIDENING, THE CONTRACTOR SHALL ENSURE THAT NO PAVEMENT DROP-OFF ARE LEFT EXPOSED DURING NON-WORKING HOURS. THE CONTRACTOR SHALL INITIATE CORRECTIVE MEANS TO ACHIEVE A MINIMUM OF 3:1 SLOPE ON ANY SAWCUT VERTICAL FACES. ALL EXISTING TRAFFIC LANES SHALL BE OPEN DURING NON-WORKING HOURS.
6. AT THE END OF EACH WORK DAY, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE A DRIVING SURFACE FREE OF OBSTRUCTIONS. ACCESS TO ALL ADJOINING PROPERTIES AND ROUTES SHALL BE MAINTAINED AT ALL TIMES DAY AND NIGHT.
7. ALL TRAFFIC CONTROL DEVICES (EXCEPT AT DETOUR ROAD LOCATIONS) SUCH AS CONSTRUCTION SIGNS, DRUMS, BARRICADES, ETC., SHALL BE MOVED TO A LOCATION AT LEAST THIRTY (30) FEET FROM THE EDGE OF THE SHOULDER WHEN CONSTRUCTION IS NOT IN PROGRESS.
8. DURING CONSTRUCTION OPERATIONS, TRAFFIC SHALL BE MOVED THROUGH THE WORK ZONE USING PILOT CARS (AS REQUIRED). APPLICABLE SIGNS AND OTHER ITEMS (TWO-WAY RADIO CONTACT) RELATED TO THE USE OF THE PILOT CARS SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION.
9. THE CONTRACTOR HAS THE OPTION TO EITHER USE DRUMS OR VERTICAL PANELS; BUT SHALL NOT USE A COMBINATION OF BOTH. NO TRAFFIC CONES SHALL BE ALLOWED.
10. THE CONTRACTOR HAS THE OPTION TO UTILIZE DETOUR ROADS IN ACCORDANCE WITH THE MUTCD MANUAL IN CONJUNCTION WITH OR IN LIEU OF A PHASED CONSTRUCTION PLAN AND IN PROPER ACCORDANCE. THE COST OF ANY DETOUR ROADS (INCLUDING WEEKLY MAINTENANCE) SHALL BE INCIDENTAL. THE CONTRACTOR IS RESPONSIBLE FOR ALL NECESSARY PERMITS AND CLEARANCES FOR ANY DETOUR ROADS. THE ARCHAEOLOGICAL, BIOLOGICAL CLEARANCES MUST BE IN PLACE PRIOR TO USE OF DETOUR ROADS.

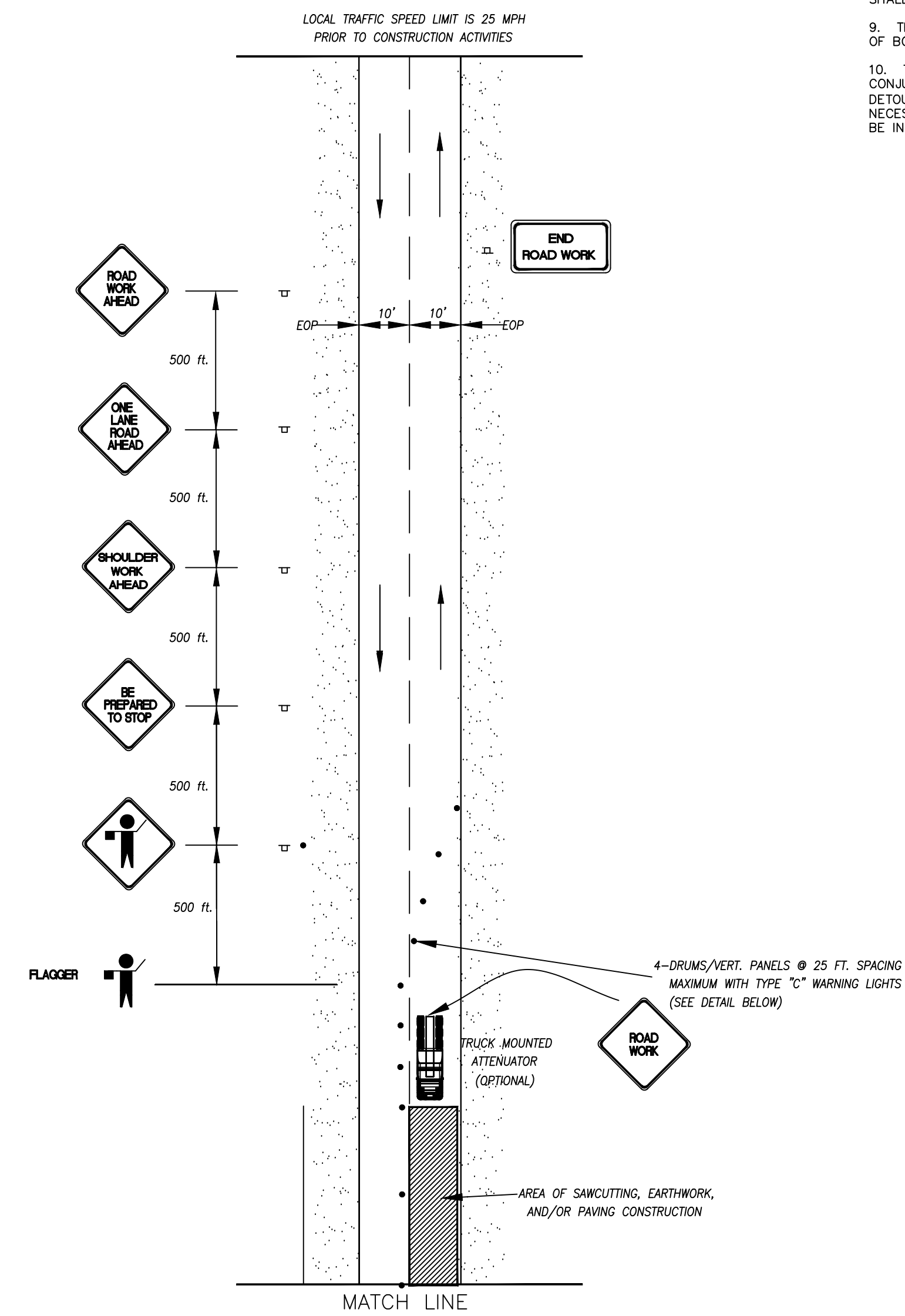
TYPICAL TRAFFIC FLOW ALTERATION WITH ENCROACHMENT FOR SAWCUTTING, EARTHWORK, AND PAVING CONSTRUCTION PHASES

NTS



TYPICAL TRAFFIC FLOW ALTERATION WITH ENCROACHMENT FOR SAWCUTTING, EARTHWORK, AND PAVING CONSTRUCTION PHASES

NTS



NAVAJO DIVISION OF TRANSPORTATION

P.O. BOX 4620 WINDOW ROCK, AZ 86515

Project: Fort Defiance N112

Scale: NTS

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Date: October 7, 2021